

Proposed Projects for the Sales Tax Committee's Consideration

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The Revised Sales Tax Committee Proposed Project List

Project Theme Matrix

Project Number	Project Name	Geographic Location	Regional Mobility/Transportation	Sense of Community	Environmental/Water Quality	Connectivity	Economic Vitality	Ecotourism/Parks	Vertical Infrastructure	Gateways	Project Total
1	Capital Circle Southwest	3	TBD by the Committee		X	X	X	X		X	-
2	Airport Gateway: Westside Student Corridor Enhancements and Flood Relief	4	21,000,000		7,000,000	1,700,000	X			X	29,700,000
3	Airport Gateway: Springhill Road Gateway	3	X	X		X	X	X		31,000,000	31,000,000
4	Southside Gateway Enrichment	2,3	18,500,000	X		600,000	X			10,600,000	29,700,000
5	Lake Bradford Gateway	3,4	X	X		X	X			37,271,738	37,271,738
6	North Monroe Gateway	4	X	X		X	X	X		5,000,000	5,000,000
7	Build Bike Route System	1,2,3,4	X			15,000,000	X	X			15,000,000
8	Complete Sidewalk Network	1,2,3,4	X	X		101,737,238	X				101,737,238
9	Implement Greenways Master Plan	1,2,3,4	X	X	X	X	X	18,497,000			18,497,000
10	Northwest Connector Corridor	4	50,000,000	500,000	X	1,600,000	X	1,500,000			53,600,000
11	Lake Lafayette and St. Marks Regional Linear Park	1,2		X	2,200,000	400,000	X	18,200,000			20,800,000
12	Pine Flats Trail	3		X	10,300,000	X	X	10,900,000			21,200,000
13	Black Swamp Restoration, Regional Pond and Cascades to Munson Slough Greenway Trail Developments	3	X		23,200,000	X	X	10,400,000			33,600,000
14	Market District Activity Center Connectivity	1	X	9,400,000		X	X	X		X	9,400,000
15	Midtown Placemaking	1,4	X	22,000,000		X	X			X	22,000,000
16	Goodwood Playground and Community Garden	1		X			X	200,000			200,000
17	College Avenue Placemaking	4		7,000,000		X	X	X			7,000,000
18	Downtown Outdoor Pedestrian Mall	4		X		X	X	X	6,000,000		6,000,000
19	Florida A&M Entry Points	3,4		2,000,000			X		2,000,000	16,000,000	20,000,000
20	Downtown Parking Garage	4					X		25,000,000		25,000,000
21	Florida Center of Performing Arts and Education	4		X			X		30,000,000		30,000,000
22	Monroe-Adams Corridor Placemaking	3	X	7,000,000		X	X			X	7,000,000
23	Orange/Meridian Placemaking: <i>Beautification of the Stormwater Pond at S. Monroe & Orange Ave.</i>	3	X	X	3,100,000		X	X	1,000,000		4,100,000
24	Beautification and Improvements to the Fairgrounds	2,3		12,000,000			X		X	X	12,000,000
25	Tennessee Street Beautification and Pedestrian Safety Enhancement*	4	TBD by the Committee	X		X	X			X	-
26	Northeast Connector Corridor	4	15,800,000		X	700,000	X	19,800,000			36,300,000
27	Orange Avenue Widening from Adams Street to Capital Circle Southwest	3	78,200,000		X	X	X			X	78,200,000
28	Welaunee Critical Area Plan Regional Infrastructure within the Canopy Project - Welaunee Boulevard and the Dove Pond Regional Stormwater Facility	1	13,500,000		5,000,000	X	X	X		X	18,500,000
29	StarMetro	1,2,3,4	12,250,000				X		X		12,250,000
30	Weems Road Flood Control	1	7,800,000		X	X	X	X			7,800,000
31	Intersection Safety and Improvements	1,2,3,4	5,800,000			X	X				5,800,000
32	Tram Road	2,3	41,900,000			X	X	X		X	41,900,000
33	Extension of Jackson Bluff Road	3,4	26,000,000			X	X				26,000,000

The Revised Sales Tax Committee Proposed Project List
Project Theme Matrix

Project Number	Project Name	Geographic Location	Regional Mobility/ Transportation	Sense of Community	Environmental/ Water Quality	Connectivity	Economic Vitality	Ecotourism/ Parks	Vertical Infrastructure	Gateways	Project Total
34	Service Road (Capital Circle NW)	4	41,800,000			X	X				41,800,000
35	Phase II: Water Quality Program	1,2,3,4			50,000,000		X				50,000,000
36	Killearn Estates Freshwater Restoration Project	1			10,000,000		X				10,000,000
37	Lake Lafayette Basin Improvements and Floodplain Protection and Eastgate Neighborhood Park/Greenspace	1	X	X	35,800,000		X	500,000			36,300,000
38	Downtown Stormwater Improvements	3,4			25,000,000		X				25,000,000
39	Alternative Sewer Solutions: Creating Comprehensive Wastewater Management for Leon County Unincorporated Area (Nitrogen Reduction and Infill Development)	1,2,3,4			67,000,000		X				67,000,000
40	Woodville Water Quality	2			25,800,000		X				25,800,000
41	Oak Ridge Sewer Project	2,3			30,600,000		X				30,600,000
42	Centerville Trace Water Resources	1			5,000,000		X				5,000,000
43A	Alternatives to Central Sewer in Harbinwood Estates: Decentralized Cluster System	4			14,100,000		X				14,100,000
43B	Alternatives to Central Sewer in Harbinwood Estates: Advanced Wastewater Treatment Plant	4			29,300,000		X				29,300,000
44	Lake Jackson Preservation and Mobility Enhancements	4	1,000,000		12,100,000	1,400,000	X	15,900,000			30,400,000
45	Sewer Hookups Incentive Program	1,2,3,4			33,100,000		X				33,100,000
46A	Operating Costs for Parks Built with Blueprint 2000 Funds: <u>County Portion</u>	1,2,3,4		X	X		X	16,500,000			16,500,000
46B	Operating Costs for Parks Built with Blueprint 2000 Funds: <u>City Portion</u>	1,2,3,4		X	X		X	10,514,300			10,514,300
Grand Total			\$ 333,550,000	\$ 59,900,000	\$ 388,600,000	\$ 123,137,238	\$ -	\$ 122,911,300	\$ 64,000,000	\$ 99,871,738	\$ 1,191,970,276

Past Project Matrix

At the March 28, 2013 Sales Tax Committee meeting, the Committee removed several projects from consideration. These projects can be found in Appendix A. Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, staff merged and consolidated several projects. Each project presented here within has a blue cover sheet that indicates the previous project(s) that were merged to create the current project.

The Project Matrix provided underneath this bluesheet uses the old project numbers to identify which projects have been removed by the committee as well as those merged or consolidated by staff based on Committee direction.

3/28/2013 Sales Tax Committee Proposed Project List Project Theme Matrix

Light Gray = Staff Edits Gray = Community Removed Blue = Blueprint 2000 Project Yellow = City of Tallahassee Project Red = Community Project Purple = Leon County Project										
Project Number	Project Name	Entity	Regional Mobility/ Transportation	Sense of Community	Water Quality	Connectivity	Economic Vitality	Ecotourism/ Parks	Vertical Infrastructure	Gateways
1	Airport Gateway: Connector from Capitol Circle SW to Lake Bradford		X	X		X	X			X
	<i>Project #1 was overlapping with BP2000 Project #13 and City Project #17. This project was removed.</i>									
2	Black Swamp Restoration, Regional Pond and Cascades to Munson Slough Greenway Trail Developments	BP2000	X		X	X	X	X		
3	Extension of Jackson Bluff Road		X			X	X			
	<i>Project #3 is being recommended for removal due to the fact that it is inconsistent with the Comprehensive Plan and is no longer a priority in the Regional Mobility Plan.</i>									
4	Greenway Connections Timberland Ravine to Klapp-Phipps Overstreet Park		X			X	X	X		
	<i>Project #4 was overlapping with City Project #17, Community Project #40, #53. This project was removed.</i>									
5	Lake Lafayette Basin Improvements and Floodplain Protection	BP2000	X		X		X	X		
	<i>Project #21 was combined with Project #5 in order to make it more holistic.</i>	BP2000								
6	Lake Lafayette Stormwater Improvements and Trailhead Developments New Title: Weems Road Flood Control	BP2000	X		X	X	X	X		
	<i>Project #6 had overlapping components with City Project #17 and County Project #52. This project was revised to address Weems Road Flood Control.</i>	BP2000								
7	Ochlocknee River Basins: Stormwater Improvements, Groundwater Protection, and Greenway Acquisition		X		X	X	X	X		
	<i>Project #7 was overlapping with County Project #56. This project was removed.</i>									
8	Phase II: Eastern Leon County Groundwater and Floodplain Protection		X		X	X	X	X		

*Recommended by more than one constituent and/or entity

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Project Number	Project Name	Entity	Regional Mobility/Transportation	Sense of Community	Water Quality	Connectivity	Economic Vitality	Ecotourism/Parks	Vertical Infrastructure	Gateways
	<i>Project #8 was overlapping with County Project #52 and previous land acquisitions by the State and Blueprint 2000. This project was removed.</i>									
9	Phase II: Water Quality Program*	BP2000			X		X			
10	Service Road (Capital Circle NW)	BP2000	X			X	X			
	<i>Project #10 is being recommended for removal due to the fact that it is inconsistent with the Comprehensive Plan and is no longer a priority in the Regional Mobility Plan.</i>									
	Airport Gateway: move County Project #62 here									
11	Airport Gateway: Springhill Road Gateway	BP2000	X	X		X	X	X		X
	move the Project #18 (which is combined with Project #71)									
	Move Project #27									
	Move Project #61 here									
12	Tram Road	BP2000	X			X	X	X		X
13	Capital Circle Southwest*	BP2000	X		X	X	X	X		X
14	Canopy Roads Legacy Projects		X	X			X	X		X
15	Connecting the Community: Build Bike Route System		X	X		X	X		X	
16	Connecting the Community: Complete Sidewalk Network		X	X		X	X		X	
	<i>Projects #41, 42, 58, 69 was combined with this project in order to create one project for all Completing the sidewalk network.</i>									
17	Connecting the Community: Implement Greenways Master Plan		X	X	X	X	X	X	X	
	<i>Move Project #52, 59, 56 here over this project.</i>									

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18	Lake Bradford Gateway		X	X		X	X	X	X	X
	<i>Project #71 was comined with Project #18.</i>									
19	Downtown Outdoor Pedestrian Mall			X			X	X	X	
20	Downtown Stormwater Improvements				X		X		X	
21	EastGate Neighborhood Park/Green Space			X			X	X		
	<i>Combine with Blueprint Project #5</i>									
22	DeSoto Winter Encampment			X		X	X	X	X	
23	College Avenue Placemaking		X	X		X	X		X	X
	<i>Combine Project #70</i>									
24	Goodwood Playground and Community Garden			X			X	X	X	
25	Midtown Placemaking		X	X		X	X		X	X
26	Monroe-Adams Corridor Placemaking*		X	X		X	X		X	X
27	North Monroe Gateway*		X	X		X	X		X	X
28	Killlearn Estates Freshwater Restoration Project	City			X		X			
29	Meadow Ridge Drive Park			X			X	X		
30	Parallel Road to Tennessee St.		X				X			
31	StarMetro: Bus Stop Enhancements (benches and covered seating)		X				X			

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32	StarMetro: Build Regional Transfer Station		X	X		X	X		X	
33	StarMetro: Build Super Stops		X	X		X	X		X	X
	StarMetro: Projects #31, 33, 34, 35 combined into one StarMetro Project									
34	StarMetro: Expand Facilities and Fleet		X			X	X			
35	StarMetro: Redevelop C.K. Steele Plaza		X	X		X	X		X	
36	Canopy PUD Infrastructure (Welanuee Blvd)	City	X			X	X			
37	Tennessee Street Beautification and Pedestrian Safety Enhancement*	City	X	X		X	X			X
38	Underground Utilities		X	X			X	X		X
39	Sewer Hookups Incentive Program	County Admin			X		X			
	Group all Water Quality Projects Together: Project 43, 63, 57, 47, 44, 51									
40	Market District Activity Center Connectivity* (Note: this project was proposed by multiple entities: City, County, and Community)	City	X	X		X	X		X	X
	Combine County Project into this Project									
41	Sidewalks: Primary (1 side) vs. Secondary (both sides)		X			X	X			
	Combine with Project #16									
42	Additional Sidewalks and Mobility Improvements: COMBINE WITH OTHER SIDEWALKS		X			X	X			
43	Alternative Sewer Solutions*: Creating Comprehensive Wastewater Management for Leon County Unincorporated Area (Nitrogen Reduction and Infill Development)	County			X		X			

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Project Number	Project Name	Entity	Regional Mobility/ Transportation	Sense of Community	Water Quality	Connectivity	Economic Vitality	Ecotourism/ Parks	Vertical Infrastructure	Gateways
44	Alternatives to Central Sewer in Harbinwood Estates	County			X		X			
45	Beautification and Improvements to the Fairgrounds	Planning		X			X		X	X
46	Beautification of the Stormwater Pond at S. Monroe & Orange Ave.	County (working with Starmetro and City Stormwater)		X			X	X		
	<i>Group placemaking projects together 23, 24, 25, 26, 45, 46 67</i>									
47	Centerville Trace Water Resources				X		X			
48	County Infrastructure Improvements to Public Safety Facilities: Courthouse and Jail						X		X	
49	County Road Resurfacing		X				X			
50	Intersection and Safety Improvements		X			X	X			
51	Lake Jackson Preservation and Mobility Enhancements		X		X	X	X	X		
52	Lake Lafayette and St. Marks Regional Linear Park		X	X	X	X	X	X		X
53	Meridian Road Greenway					X	X	X		
	<i>Combined with Project #17.</i>									
54	Northeast Connector Corridor		X		X	X	X	X		
55	Northeast Park			X			X	X		
56	Northwest Connector Corridor		X		X	X	X	X		
57	Oak Ridge Sewer Project	County			X		X			

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58	Ox Bottom Manner Sidewalks: COMBINE WITH OTHER SIDEWALKS					X	X			
59	Pine Flats Trail		X		X	X	X	X		
60	Private Dirt Road Paving Program		X		X		X			
61	Southside Gateway Enrichment		X			X	X			X
62	Westside Student Corridor Enhancements and Flood Relief		X	X	X	X	X			
63	Woodville Water Quality*				X		X			
64	Orange Avenue Widening from Wahnish Way to Capital Circle Southwest: divide into segments (Wahnish Way to Springhill and then Springhill to CCSW)	County	X		X	X	X			
65	Sports Complex			X			X		X	
66	Florida Center of Performing Arts and Education			X			X		X	
67	Florida A&M Gateways-Entry Points	City		X			X		X	X
68	Florida State University: Downtown Parking Garage	City					X		X	
69	Florida State University: Pedestrian Mobility Enhancements (sidewalks):		X	X		X	X			
70	Florida State University and Tallahassee Downtown Improvement Authority: College Avenue Placemaking		X	X	X	X	X			
	<i>Combined with Project #23.</i>									
71	Florida State University: Lake Bradford Gateway Extension to Orange Avenue				X		X			X
	<i>Project #71 was combined with Project #18.</i>									

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Project Number	Project Name	Entity	Regional Mobility/ Transportation	Sense of Community	Water Quality	Connectivity	Economic Vitality	Ecotourism/ Parks	Vertical Infrastructure	Gateways
72	Goodwood Land Purchase for Community Parks and Gardens			X			X	X		
73	Operating Costs for Parks Built with Blueprint 2000 Funds	BP2000		X			X	X		

*Recommended by more than one constituent and/or entity

Revenue Projections

Sales Tax Revenue Projections

The revenue projections presented below are based on one penny of sales tax dollars generating \$37.8 million per year.

Scenario 1

Annual	County	City	Blueprint			Total
			Economic Vitality <i>(Imagine Tallahassee)</i> up to 15%	Infrastructure 65%	BP Total Percentage 80%	
1	3,780,000	3,780,000	5,670,000	24,570,000	30,240,000	37,800,000

Years	County	City	Blueprint			Total
			Economic Vitality <i>(Imagine Tallahassee)</i> up to 15%	Infrastructure 65%	BP Total Percentage 80%	
15	56,700,000	56,700,000	85,050,000	368,550,000	453,600,000	567,000,000
20	75,600,000	75,600,000	113,400,000	491,400,000	604,800,000	756,000,000
25	94,500,000	94,500,000	141,750,000	614,250,000	756,000,000	945,000,000
30	113,400,000	113,400,000	170,100,000	737,100,000	907,200,000	1,134,000,000
35	132,300,000	132,300,000	198,450,000	859,950,000	1,058,400,000	1,323,000,000
40	151,200,000	151,200,000	226,800,000	982,800,000	1,209,600,000	1,512,000,000

Sales Tax Revenue Projections

The revenue projections presented below are based on one penny of sales tax dollars generating \$37.8 million per year.

Scenario 2

Annual	County	City	Blueprint			Total
			Economic Vitality <i>(Imagine Tallahassee)</i> up to 15%	Infrastructure 55%	BP Total Percentage 70%	
1	5,670,000	5,670,000	5,670,000	20,790,000	26,460,000	37,800,000

Years	County	City	Blueprint			Total
			Economic Vitality <i>(Imagine Tallahassee)</i> up to 15%	Infrastructure 55%	BP Total Percentage 70%	
15	85,050,000	85,050,000	85,050,000	311,850,000	396,900,000	567,000,000
20	113,400,000	113,400,000	113,400,000	415,800,000	529,200,000	756,000,000
25	141,750,000	141,750,000	141,750,000	519,750,000	661,500,000	945,000,000
30	170,100,000	170,100,000	170,100,000	623,700,000	793,800,000	1,134,000,000
35	198,450,000	198,450,000	198,450,000	727,650,000	926,100,000	1,323,000,000
40	226,800,000	226,800,000	226,800,000	831,600,000	1,058,400,000	1,512,000,000

Sales Tax Revenue Projections

The revenue projections presented below are based on one penny of sales tax dollars generating \$37.8 million per year.

Scenario 3

Annual	County	City	Blueprint			Total
			Economic Vitality <i>(Imagine Tallahassee)</i> up to 15%	Infrastructure 45%	BP Total Percentage 60%	
1	7,560,000	7,560,000	5,670,000	17,010,000	22,680,000	37,800,000

Years	County	City	Blueprint			Total
			Economic Vitality <i>(Imagine Tallahassee)</i> up to 15%	Infrastructure 45%	BP Total Percentage 60%	
15	113,400,000	113,400,000	85,050,000	255,150,000	340,200,000	567,000,000
20	151,200,000	151,200,000	113,400,000	340,200,000	453,600,000	756,000,000
25	189,000,000	189,000,000	141,750,000	425,250,000	567,000,000	945,000,000
30	226,800,000	226,800,000	170,100,000	510,300,000	680,400,000	1,134,000,000
35	264,600,000	264,600,000	198,450,000	595,350,000	793,800,000	1,323,000,000
40	302,400,000	302,400,000	226,800,000	680,400,000	907,200,000	1,512,000,000

Sales Tax Revenue Projections

The revenue projections presented below are based on one penny of sales tax dollars generating \$37.8 million per year.

Scenario 4

Annual	County	City	Blueprint			Total
			Economic Vitality <i>(Imagine Tallahassee)</i> up to 15%	Infrastructure 35%	BP Total Percentage 50%	
1	9,450,000	9,450,000	5,670,000	13,230,000	18,900,000	37,800,000

Years	County	City	Blueprint			Total
			Economic Vitality <i>(Imagine Tallahassee)</i> up to 15%	Infrastructure 35%	BP Total Percentage 50%	
15	141,750,000	141,750,000	85,050,000	198,450,000	283,500,000	567,000,000
20	189,000,000	189,000,000	113,400,000	264,600,000	378,000,000	756,000,000
25	236,250,000	236,250,000	141,750,000	330,750,000	472,500,000	945,000,000
30	283,500,000	283,500,000	170,100,000	396,900,000	567,000,000	1,134,000,000
35	330,750,000	330,750,000	198,450,000	463,050,000	661,500,000	1,323,000,000
40	378,000,000	378,000,000	226,800,000	529,200,000	756,000,000	1,512,000,000

SALES TAX PROJECTS

Project Number: 1

Project Name: Capital Circle Southwest

Total Project Cost: TBD

Total Project Cost Notes

The total estimated cost of this project is \$114,421,000 and includes \$53,421,000 for construction and \$61,000,000 for right of way costs. The projected costs include the ROW and amenities customary to all other segments of Capital Circle. While the FDOT is seeking funding of the roadway improvements, they have indicated a need for local funding partnership for improvements beyond the typical FDOT roadway design. The committee will need to recommend a partnership amount for this project. As an example, Capital Circle Northwest (currently under construction), has a total cost of \$117,000,000. The local share contributed to this project is approximately \$45,000,000.

Executive Project Summary

The Capital Circle SW roadway expansion project from Orange Avenue to Crawfordville Road was included in the original Tier I Blueprint 2000 projects. However, due to current sales tax funding restrictions, this project will not be able to be completed within the current sales tax authorization. This project is necessary to complete the overall transportation objectives originally envisioned with the Capital Circle enhancements in the original Blueprint 2000 program. The project includes water quality enhancements and infrastructure for multiple transportation modes, improving access in the area. The project stimulates economic development in the short term by providing jobs in construction and associated professional services, and in the long term by improving access to the Airport, Innovation Park, Tallahassee Museum, and undeveloped properties.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Detail Project Description

This project is necessary to complete the overall transportation objectives originally envisioned with the Capital Circle enhancements in the original Blueprint 2000 program. In addition, this project is critical to the full development of the airport and surrounding commercial areas.

Benefits the Environment – One of the most significant features in the project area is the Bradford Brook Chain-of-Lakes, including Lake Bradford, located west and north of Capital Circle Southwest. The lakes currently receive runoff from Capital Circle and stormwater inputs from the West Drainage Ditch. Another significant feature in the area is the Apalachicola National Forest, the largest national forest in Florida. The Capital Circle Southwest project will protect the Lake Bradford Chain-of-Lakes, Apalachicola National Forest, and existing neighborhoods through the incorporation of greenways, landscaping, alternative transportation modes, and stormwater enhancements.

Provides Regional Impact / Geographic Equity – The Capital Circle Southwest project has been planned on a community wide basis taking into account far reaching community goals (enhanced greenways, roadways, stormwater facilities, lake protection, and the inclusion of multiple modes of transportation). Additionally, the project will enhance access to the Airport for local and regional users.

Enhances Mobility – In order to provide transportation alternatives, infrastructure planning for the Capital

Circle Southwest project incorporates pedestrian and bicycle trail linkages and includes sidewalks and bike lanes to provide overall increased access to parks, neighborhoods, the Downtown area, the universities and Tallahassee Community College.

Supports Recreation and Quality of Life – The project includes water quality enhancements and infrastructure for multiple transportation modes, improving access in the area.

Supports Economic Development / Revitalization – The project stimulates economic development in the short term by providing jobs in construction and associated professional services, and in the long term by improving access to the Airport, Innovation Park, Tallahassee Museum, and undeveloped properties.

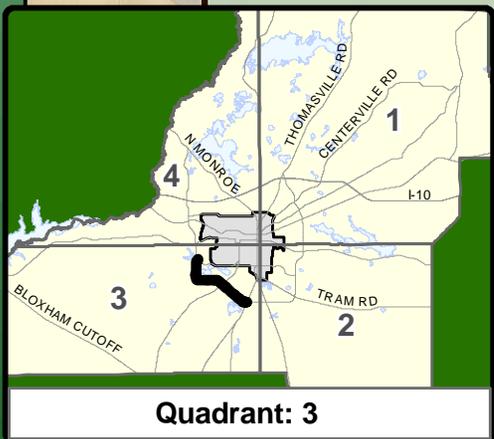
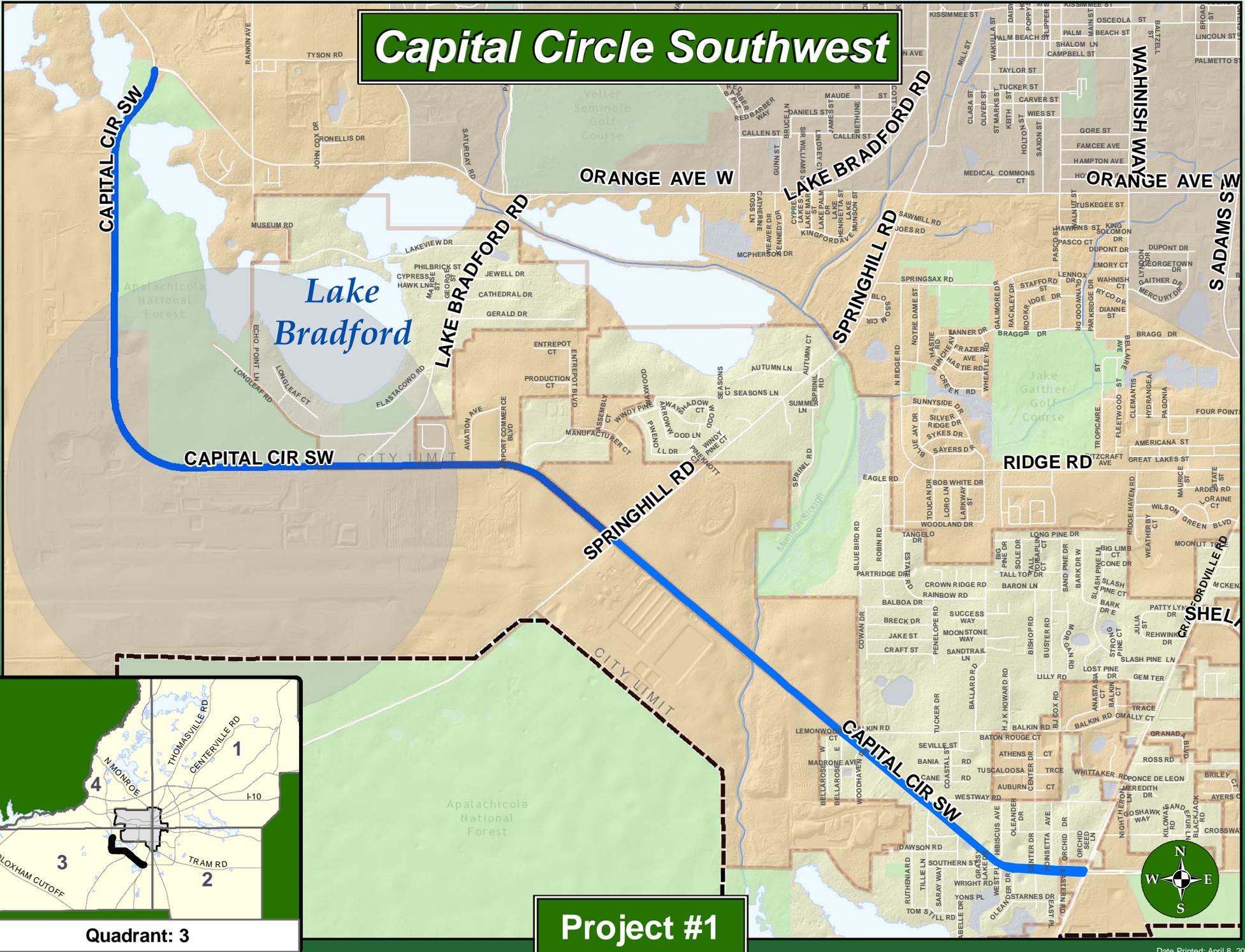
On an Existing Master Plan or Blueprint List – Although there is no dedicated funding for the construction of the Capital Circle Southwest project due to a decline in sales tax revenue, it was included in the original Blueprint 2000 Master Plan as a Tier 1 project.

Leverages Other Funds – It is likely that the Capital Circle Southwest project design and construction will be funded by a partnership between the Florida Department of Transportation and Blueprint 2000.

Cost By Themes

\$114,421,000	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Capital Circle Southwest



Quadrant: 3

Project #1



Previous Project Information for:

Project # 1

Capital Circle Southwest

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Catalyzing Airport Development: Complete Capital Circle Southwest (*Previously Project #13*)

Airport Gateway: Connector from Capital Circle SW to Lake Bradford (*Previously Project #1*)

Complete Capital Circle Southwest (*Previously Project #13*)

Program
5

Catalyzing Airport Development: Complete Capital Circle Southwest

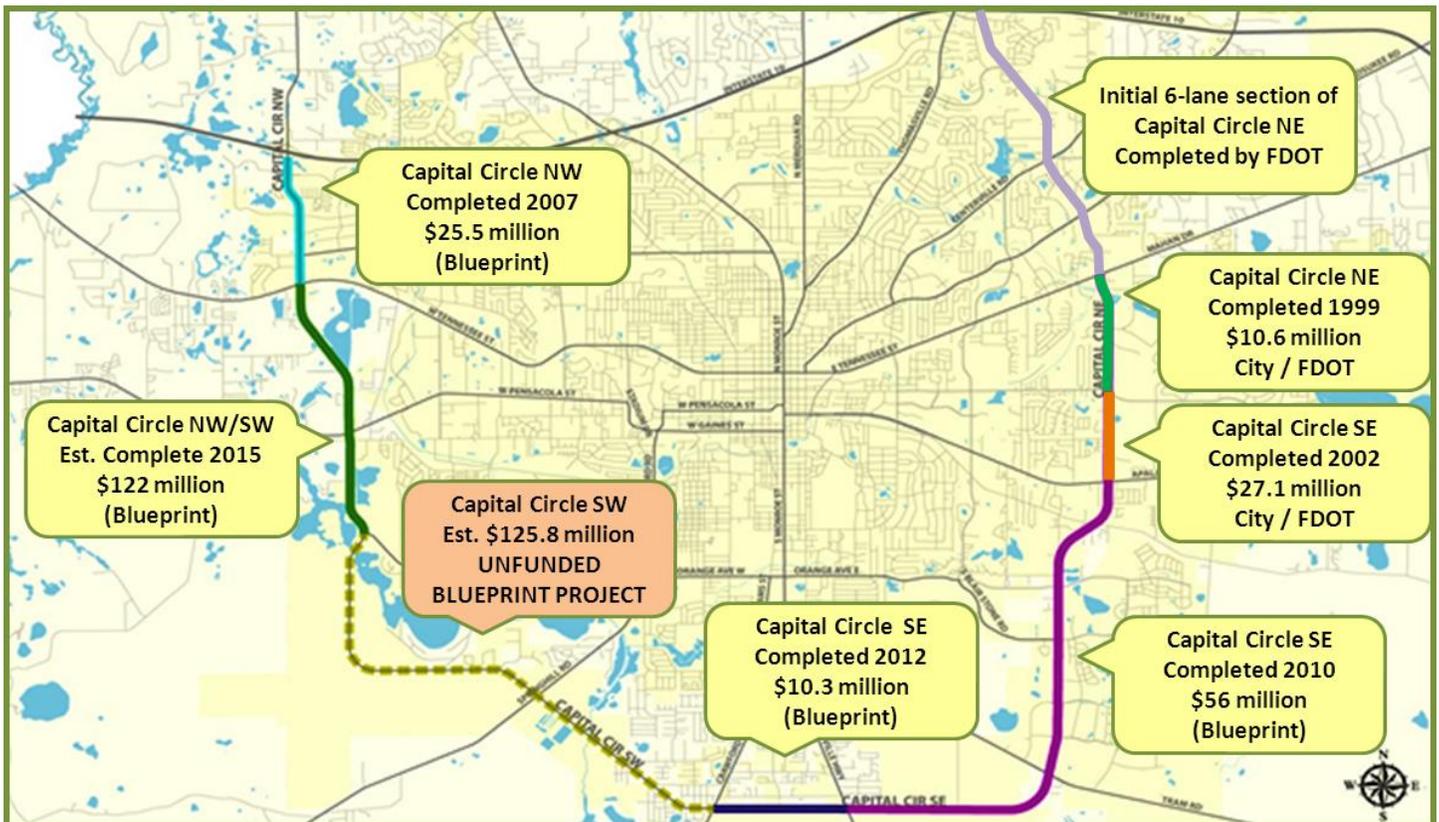
Estimated Cost:
\$125.8 million

WHAT IS IT?

The widening of Capital Circle from Crawfordville Highway to SR 20 was a Tier 1 Project under the current BluePrint 2000 funding list. However, due to current sales tax funding restrictions, this project will not be able to be completed within the current sales tax authorization. This project is necessary to complete the overall transportation objectives originally envisioned with the Capital Circle enhancements in the original BluePrint 2000 program. In addition, this project is critical to the full development of the airport and surrounding commercial areas.

Evaluation Criteria:

Provides Regional Impact / Geographic Equity	✓	Benefits the Environment	✓
Enhances Mobility	✓	Supports Economic Development / Revitalization	✓
Supports Recreation and Quality of Life	✓	On an Existing Master Plan or Blueprint List	✓
		Leverages Other Funds	✓



A decline in sales tax revenue means that construction will not be funded for this section by 2019. At left is a proposed cross section for Capital Circle Southwest.

WHY IS IT IMPORTANT?

Benefits the Environment – One of the most significant features in the project area is the Bradford Brook Chain of Lakes, including Lake Bradford, located west and north of Capital Circle Southwest. The lakes currently receive runoff from Capital Circle and stormwater inputs from the West Drainage Ditch. Another significant feature in the area is the Apalachicola National Forest, the largest national forest in Florida. The Capital Circle Southwest project will protect the Lake Bradford Chain-of-Lakes, Apalachicola National Forest, and existing neighborhoods through improvements to the West Ditch, restoration of the Black Swamp, and, in accord with all Blueprint projects, incorporate greenways, landscaping, alternative transportation modes and stormwater enhancements.

Provides Regional Impact / Geographic Equity – The Capital Circle Southwest project has been planned on a community-wide basis taking into account far reaching community goals (enhanced greenways, roadways, stormwater facilities, lake protection, and the inclusion of multiple modes of transportation). Additionally, the project will enhance access to the Airport for local and regional users.

Enhances Mobility – In order to provide transportation alternatives, infrastructure planning for the Capital Circle Southwest project incorporates pedestrian and bicycle trail linkages and includes sidewalks and bike lanes to provide overall increased access to parks, neighborhoods, the Downtown area, the universities and Tallahassee Community College.

Supports Recreation and Quality of Life – The project includes water quality enhancements and infrastructure for multiple transportation modes, improving access in the area.

Supports Economic Development / Revitalization – The project stimulates economic development in the short term by providing jobs in construction and associated professional services, and in the long term by improving access to the Airport, Innovation Park, Tallahassee Museum, and undeveloped properties.

On an Existing Master Plan or Blueprint List – Although there no dedicated funding for the construction of the Capital Circle Southwest project due to a decline in sales tax revenue, it was included in the original Blueprint 2000 Master Plan as a Tier 1 project.

Leverages Other Funds – It is likely that the Capital Circle Southwest project design and construction will be funded by a partnership between the Florida Department of Transportation and Blueprint 2000.



Delivery of the first jets are expected in early 2014, which will coincide with the construction of the Honda Jet facilities. The other components of the project are still in design as Flightline seeks to identify a suitable developer to construct the improvements.



Compass Pointe exemplifies economic development in proximity of the Tallahassee Regional Airport. It includes leasable space, a HondaJet Sales and Service center and storage hanger, and a hotel.

Blueprint 2000

Proposed Sales Tax Projects

Project Name: Airport Gateway: Connector from Capital Circle SW to Lake Bradford
(Blueprint 2000 Map 2C)

Project Themes:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

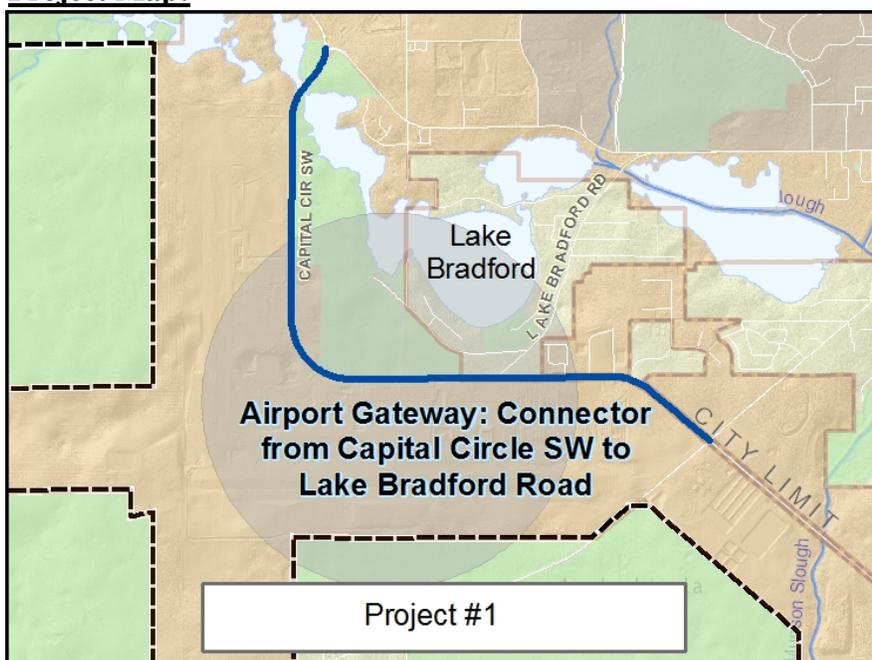
Project Description:

The Capital Circle SW roadway project from Orange Avenue to Springhill Road represents an expansion of the existing roadbed alignment. The roadway project consists of a four lane, controlled access, or six lane widening and includes a multiuse trail and bikeway which will eventually join the St. Marks Trail Extension. Connections to Tallahassee Regional Airport and Innovation Park will be designed encourage economic development and to meet future traffic conditions related to significant growth.

The Bradford Chain of Lakes is one of the few remaining pristine like systems in Leon County and must be preserved. The primary environmental purpose for the roadway realignment relates to the stormwater impacts to the Chain of Lakes associated with a widening of roadways. Water quality within the Cascades Chain of Lakes could be adversely affected over time by the use of septic tanks located outside the Urban Service Area west of the Cascades Chain of Lakes system. Additional study is proposed to assess the threat to water quality in the lakes and fund possible solutions.

Two greenway connections are proposed, a recreational path along Capital Circle from the City's Golden Aster Park to Munson Slough and the St. Marks Trail, and a habitat-oriented greenway comprising undeveloped floodplain east of Lake Bradford. Additionally, the project seeks to develop an attractive trail interior to Capital Circle, SW linking the Tallahassee Museum of History and Natural Science and Forest Service Lands to the City's Golden Aster Park.

Project Map:



Blueprint 2000 Proposed Sales Tax Projects

Project Name: Complete Capital Circle Southwest (Blueprint 2000 Map 2C)

Project Themes:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

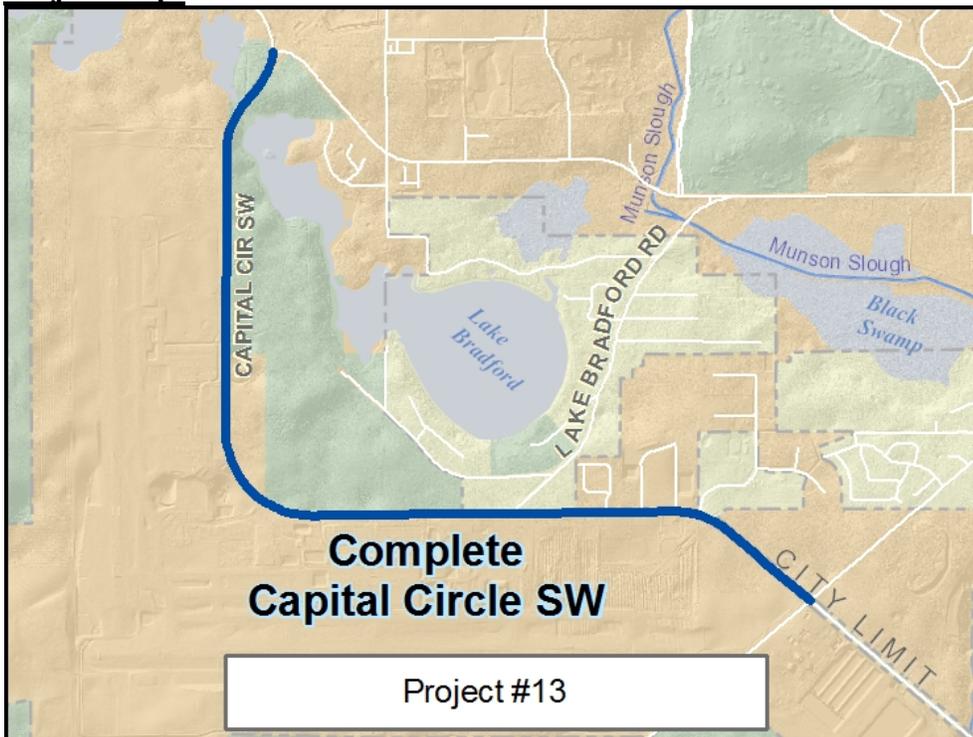
Project Description:

The Capital Circle SW roadway expansion project from Springhill Road to Crawfordville Highway project consists of a four lane, controlled access, or six lanes widening along the existing roadbed. The project includes a multiuse trail and bikeway which will eventually join the St. Marks Trail Extension. Connections to Tallahassee Regional Airport and Innovation Park will be designed encourage economic development and to meet future traffic conditions related to significant growth.

Water quality enhancement is to be accomplished via management of the impacts of road widening on stormwater runoff.

The Pine Flats Greenway beginning at the St. Marks Trail (north of Orange Avenue) and ending at the 8 Mile Pond Park along the watercourse (Munson Slough) is proposed as a part of this project. This greenway will link more than 10 proposed and existing trails.

Project Map:



SALES TAX PROJECTS

Project Number: 2

Project Name: Airport Gateway: Westside Student Corridor Enhancements and Flood Relief

Total Project Cost: \$29.7 million

Total Project Cost Notes

This project cost includes right of way costs for the widening of Pensacola from Capital Circle Southwest (CCSW) to Appleyard Drive.

Executive Project Summary

The Westside Student Corridor Enhancements and Flood Relief project creates a southwest gateway for the community by linking the improved Capital Circle corridor to the urban core while widening Highway 20 from Capital Circle to Appleyard to four lanes. This project encompasses a holistic approach to transportation capacity improvements, regional mobility, connectivity to existing/proposed amenities (including Blueprint 2000 projects), and increases stormwater capacity to address flooding concerns. The project supports the multi-modal transportation.

The project relieves stormwater runoff in the Gum Creek/West Drainage Ditch that contributes to flooding issues in the Tallahassee Community College area and the Gum Road Target Planning Area. In addition, the project constructs sidewalks on Gum, Aeon Church, and Dome Level Roads creating safe and convenient mobility access for the residents of area.

Another mobility enhancement aspect of this project is the implementation of the western segment of the Education Quadrant Greenways. The construction of the TCC/FSU/FAMU Connector Trail links all three institutions of higher learning to the facilities at Innovation Park.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Detail Project Description

The project proposed improvements include the following:

- Work with community partners during the update of the PD&E study for this corridor.
- Widen Pensacola Street to four lanes and construct land landscape medians from Capital Circle to Appleyard. This project supports the mobility plan by providing multimodal links in a high capacity student corridor. This project widens State Road 20 from two lanes to four lanes. Currently, the two lane section of State Road 20 between Blountstown Highway and Appleyard Drive restricts traffic flow on this east-west roadway. The adopted level of service (L.O.S.) for this category of roadway is E. The current roadway is operating at L.O.S. F for eastbound traffic and L.O.S. D for westbound traffic. Widening this roadway to four lanes elevates the road into compliance with the comprehensive plan level of service.
- Construct bike lanes and sidewalks including sidewalk on Gum, Aeon Church, and Dome Level Roads.
- Construct a gateway feature by constructing a decorative bridge which will allow water flow to continue toward existing public lands downstream. The widening of State Road 20 will create a southwest gateway for the community linking the upgraded Capital Circle corridor to the urban core.

- Conduct a comprehensive stormwater study to include ecosystem enhancements for water quality and flood attenuation. A comprehensive stormwater study would include ecosystem enhancements for water quality and flood attenuation reaching to Lake Bradford, as called for by the Blueprint Citizens Advisory Committee while reviewing Capital Circle Southwest.

- Establish connectivity to Innovation Park along the Education Quadrant Greenways by constructing the TCC/FSU/FAMU Connector Trail which links all three institutions of higher learning. This project supports regional mobility by constructing bike lanes, sidewalks, and enhances public transportation for students of Tallahassee Community College, Florida State University, and Florida A & M University. The project provides significant pedestrian linkages for established residential areas to the Blueprint Broadmoor Pond, CCSW and all their associated interconnections.

Cost By Themes

\$21 million	Regional Mobility/Transportation
	Sense of Community
\$7 million	Environmental/Water Quality
\$1.7 million	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Airport Gateway: Westside Student Corridor Enhancements and Flood Relief

Flood Benefits Area

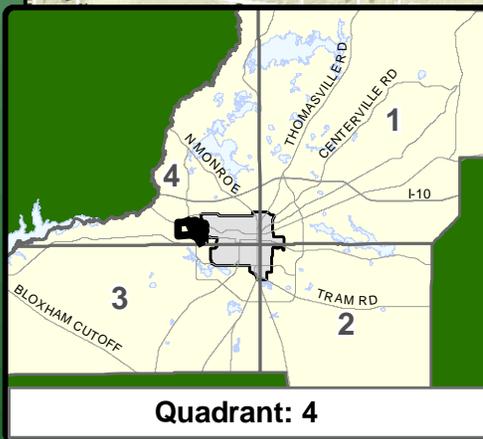
**Tallahassee
Community
College**

Widen to four lanes with
sidewalks, bike lanes and
landscape medians

**TCC/FSU/FAMU
Connector**



Gateway Feature Example
Debbie Lightsey
Nature Park



Project #2



Previous Project Information for:

Project # 2

Airport Gateway: Westside Student Corridor Enhancements and Flood Relief

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Westside Student Corridor Enhancements and Flood Relief
(*Previously Project #62*)

Proposed Project for the Sales Tax Extension

Project #4: Westside Student Corridor Enhancements and Flood Relief

Estimated Project Cost: \$29 million

Project Criteria:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input checked="" type="checkbox"/> Water Quality Enhancements and Green Infrastructure |
| <input checked="" type="checkbox"/> Regional Mobility | <input checked="" type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input checked="" type="checkbox"/> Greenway Master Plan | <input checked="" type="checkbox"/> Transportation Capacity Improvements |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input checked="" type="checkbox"/> Complements BP2000 Project | |

Project Description:

The Westside Student Corridor Enhancements and Flood Relief project encompasses a holistic approach to transportation capacity improvements, regional mobility, connectivity to existing/proposed amenities (including Blueprint 2000 projects), and increases stormwater capacity to address flooding concerns. The project creates a southwest gateway for the community by linking the improved Capital Circle corridor to the urban core while widening Highway 20 from Capital Circle to Appleyard. The project supports the multi-modal transportation and enhanced access to public transit. The project relieves stormwater runoff in the Gum Creek/West Drainage Ditch that contributes to flooding issues in the Tallahassee Community College area and the Gum Road Target Planning Area. In addition, the project constructs sidewalks on Gum, Aeonon Church, and Dome Level Roads creating safe and convenient mobility access for the residents of area. Another mobility enhancement aspect of this project is the implementation of the western segment of the Education Quadrant Greenways. The construction of the TCC/FSU/FAMU Connector Trail links all three institutions of higher learning to the facilities at Innovation Park.

The project proposed improvements include the following:

- Widen to four lanes and construct land landscape medians from Capital Circle to Appleyard;
- Public Transportation enhancements;
- Construct bike lanes and sidewalks including sidewalk on Gum, Aeonon Church, and Dome Level Roads;
- Construct a gateway feature by constructing a decorative bridge which will allow water flow to continue toward existing public lands downstream;
- Conduct a comprehensive stormwater study to include ecosystem enhancements for water quality and flood attenuation;
- Establish connectivity to Innovation Park along the Education Quadrant Greenways by constructing the TCC/FSU/FAMU Connector Trail links all three institutions of higher learning.

Project Criteria Detail:

Comprehensive Plan: This project supports the Mobility and Stormwater Management elements of the Comprehensive Plan. The areas that the project complies with are listed below.

- Mobility Element:
 - Policy 1.1.6: A functional transportation network shall be coordinated and maintained with the Florida State University, Florida A&M University, and Tallahassee Community College master plans to link those educational institutions and provide access to transit and surrounding supporting land uses.
 - Policy 1.2.2: Safe and convenient facilities for pedestrians, cyclists and transit users shall be evaluated for all new road and road widening projects. Specifically, all road projects, including resurfacing projects, shall be

Project #4: Westside Student Corridor Enhancements and Flood Relief

Page 2

- evaluated for the addition of bicycle lanes or paved shoulders, and transit shelters where they did not previously exist
- Policy 1.2.7: Require that all new or rebuilt multi-lane (four or six-lane) arterial and major collector streets be constructed with grassed and/or landscaped medians where sufficient right-of-way can be obtained, unless limited by environmental constraints.
 - Policy 1.3.4: Design, construct, and maintain transportation facilities to prevent flooding, minimize pollution, and maintain natural stormwater quantity, timing, rate, and direction of flow characteristics consistent with the adopted Stormwater Level of Service Standard.
 - Stormwater Management Goal 1: Provide a stormwater management system which protects the health, welfare, and safety of the general public by reducing damage and inconvenience from flooding and protects surface water and groundwater quality.
 - Parks & Recreation
 - Policy 1.1.5: Maintain a Greenways Master Plan that integrates pedestrian and bicycle mobility into a linear park and open space system that connects local, regional, and state facilities, with specific emphasis on connections within Downtown and energy efficiency districts.
 - Policy 1.5.3: The County shall help fund and develop a Greenways Trail System that provides the public opportunities to access a safe and convenient trail system in the unincorporated County. This system should incorporate public lands, right of way, easements on private lands, and open space designated lands to achieve this goal. The trail system will connect to the maximum extent possible existing and future residential areas to employment, education, and activity centers.

Regional Mobility Plan: This project supports the mobility plan by providing multimodal links in a high capacity student corridor.

Greenway Master Plan: This project implements the western segment of the Education Quadrant Greenways connecting TCC to the FSU/FAMU facilities at Innovation Park through the construction of the TCC/FSU/FAMU Connector Trail.

Connectivity: This project supports regional mobility by constructing bike lanes, sidewalks, and enhances public transportation for students of Tallahassee Community College, Florida State University, and Florida A & M University. The project provides significant pedestrian linkages for established residential areas to the Blueprint Broadmoor Pond, CCSW and all their associated interconnections. In addition, the widening of State Road 20 will create a southwest gateway for the community linking the upgraded Capital Circle corridor to the urban core.

Complements Blueprint 2000 Project(s): This project complements the following Blueprint 2000 Projects: Broadmoor Pond, Martha Wellman Park, Debbie Lightsey Park, Capital Circle Northwest/Southwest, and the Capital Circle trail system.

Water Quality Enhancements and Green Infrastructure: This project conducts a comprehensive stormwater study and includes ecosystem enhancements for water quality and flood attenuation reaching to Lake Bradford. This study was called for by the Blueprint Citizens Advisory Committee while reviewing the proposed Capital Circle Southwest.

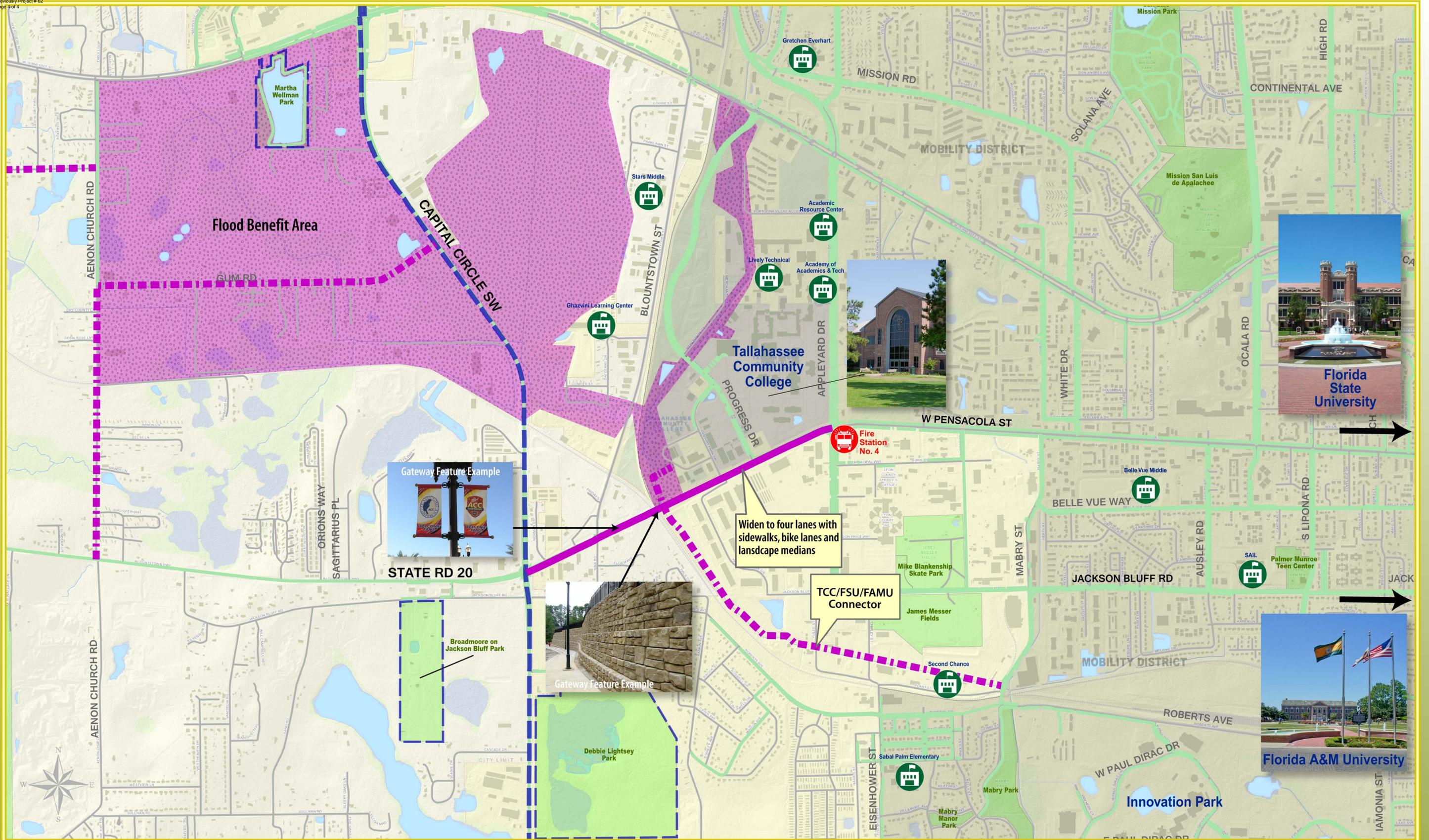
Stormwater/Sewer Capacity Improvements: This project relieves the constriction in the Gum Creek/West Drainage Ditch by improving the bridge section to allow flow to continue toward existing public lands downstream. A comprehensive stormwater study would include ecosystem enhancements for water quality and flood attenuation reaching to Lake Bradford, as called for by the Blueprint Citizens Advisory Committee while reviewing Capital Circle Southwest.

Project #4: Westside Student Corridor Enhancements and Flood Relief

Page 3

Transportation Capacity Improvements: This project widens State Road 20 from two lanes to four lanes. Currently, the two lane section of State Road 20 between Blountstown Highway and Appleyard Drive restricts traffic flow on this east-west roadway. The adopted level of service (L.O.S.) for this category of roadway is E. The current roadway is operating at L.O.S. F for eastbound traffic and L.O.S. D for westbound traffic. The four lane section is projected to operate at L.O.S. E for westbound and L.O.S. C for eastbound. Widening this roadway to four lanes elevates the road into compliance with the comprehensive plan level of service. To date the Florida Department of Transportation has spent approximately \$1 million to complete the Highway 20 PD&E study. However, the study may need to be updated as it was completed several years ago. Final Design, permitting and right of way acquisition work will need to be completed before construction can begin on this transportation corridor.

Core Infrastructure: This project is considered a core infrastructure project due to the support multi-modal use in the student corridor which is not currently supported as well as stormwater/sewer capacity improvements as detailed above.



Westside Student Corridor Enhancements and Flood Relief

Legend Existing Sidewalks, Bike Routes Park Trails Highway 20 Corridor Proposed Sidewalk/Trail Improvements Flood Benefit Area Blueprint 2000 Projects Parks		Westside Student Corridor Enhancements and Flood Relief Project Criteria: Comprehensive Plan Regional Mobility Greenway Master Plan Connectivity Complements BP2000 Project Water Quality Enhancements and Green Infrastructure Stormwater/Sewer Capacity Improvements Transportation Capacity Improvements Core Infrastructure	

This product has been compiled from the most accurate source data from Leon County and the City of Tallahassee. However, this product is for reference purposes only and is not to be construed as a legal document or survey instrument. Any reliance on the information contained herein is at the user's own risk. Leon County and the City of Tallahassee assume no responsibility for any use of the information contained herein or any loss resulting therefrom.

SALES TAX PROJECTS

Project Number: 3
Project Name: Airport Gateway: Springhill Road Gateway
Total Project Cost: \$31,000,000

Total Project Cost Notes
(ROW \$10,800,000) (Construction \$20,200,000)

Executive Project Summary

This project proposes to widen Springhill Road to four lanes, provide roadway beautification, sidewalks, bike lanes, and screening of lands adjacent to roadway. Springhill Road is envisioned as a “gateway” road into the downtown area from the Airport.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Detail Project Description

This project proposes to widen Springhill Road to four lanes, provide roadway beautification, sidewalks, bike lanes, and screening of lands adjacent to roadway. Springhill Road is envisioned as a “gateway” road into the downtown area from the Airport.

Cost By Themes

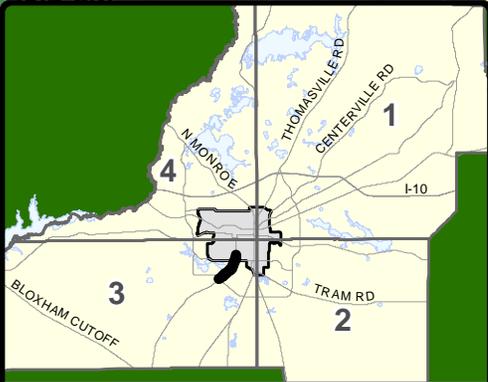
	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
\$31,000,000	Gateways

Airport Gateway: Springhill Road Gateway



Lake Bradford

Project #3



Quadrant: 3

Previous Project Information for:

Project #3

Airport Gateway: Springhill Road Gateway

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Springhill Road to Indianhead Acres: Right of Way, Construction, Stormwater Improvements, Greenway and Trail Developments (*Previously Project #11*)

Blueprint 2000

Proposed Sales Tax Projects

Project Name: Springhill Road to Indianhead Acres: Right of Way, Construction, Stormwater Improvements; Greenway and Trail Developments (Blueprint 2000 Map 2C)

Project Themes:

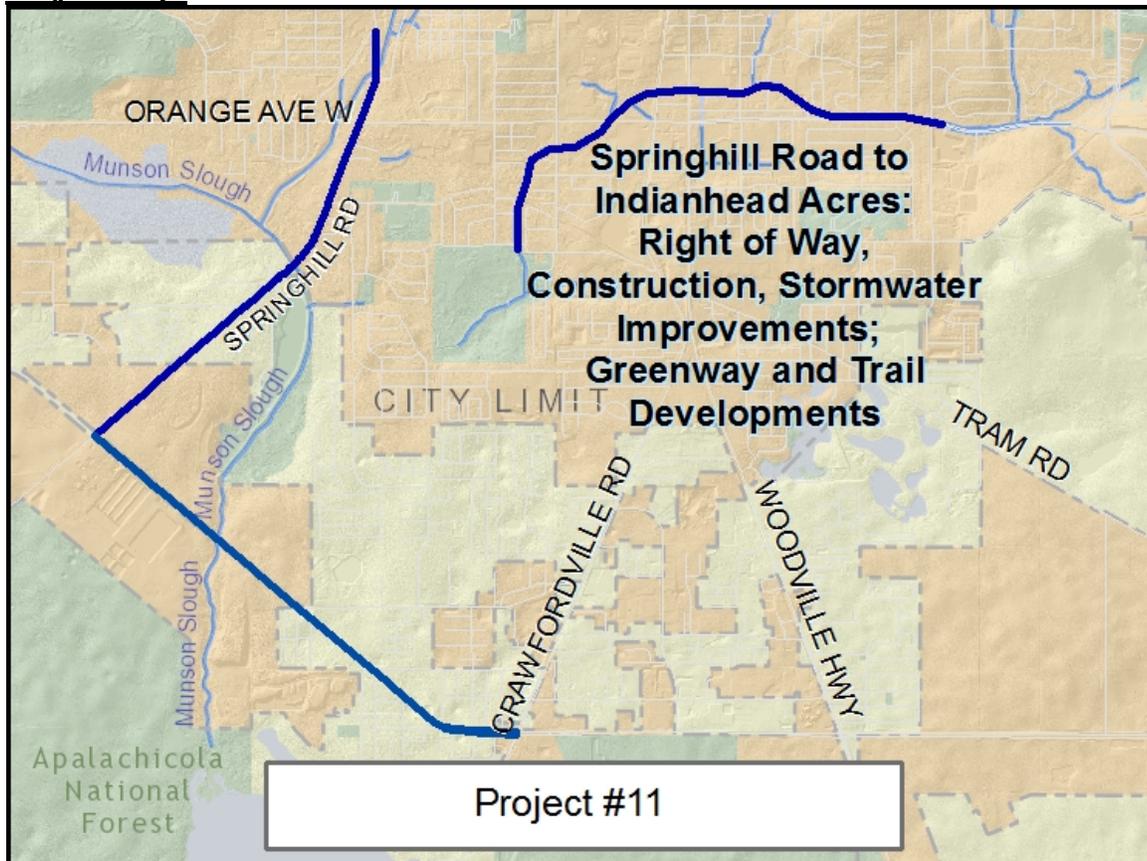
- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Project Description:

This project proposes to widen Springhill Road to four lanes, provide roadway beautification, sidewalks, bike lanes, and screening of lands adjacent to roadway. Springhill Road is envisioned as a “gateway” road into the downtown area from the Airport.

Water quality enhancement is to be accomplished via improved management of Munson Slough and the impacts to stormwater through roadway widening. Additionally, this project includes acquisition of active, developed properties in the surrounding floodplain areas. A joint stormwater treatment pond is proposed along the East Branch (Indianhead, Paul Russell, and Pine Ridge) to control flooding and improve water quality.

Project Map:



SALES TAX PROJECTS

Project Number: 4
Project Name: Southside Gateway Enrichment
Total Project Cost: \$29.7 million

Total Project Cost Notes

This project cost includes right of way costs.
\$18.5 million for Woodville Highway upgrade to 4-lane roadway
\$10.6 million for Crawfordville/Woodville Highway Gateway feature

The primary expense for this project is associated with the transportation improvements, with the balance directed to landscaping along Crawfordville Highway and sidewalks along Shelfer Road.

Executive Project Summary

The Southside Gateway Enrichment project enhances regional mobility, increases the viability of this southern commercial gateway corridor, and supports the goal of economic revitalization to the southside of Tallahassee.

This roadway also has a significant regional impact as it is a crucial north/south evacuation route from the coast. The project enhances transportation capacity by widening Woodville Highway (from Capital Circle to Gaile Avenue) to four lanes. The project includes widening the medians for landscaping, pedestrian refuge, and safe turning movements, and increases opportunities for public transit. In addition, the interconnection of Woodville and Crawfordville Highways, via a looped roadway, enhances the operation of the corridors and provides a large greenspace to establish a gateway into the southside of Tallahassee. The project replaces a grey landscape of asphalt and concrete along Crawfordville Highway with landscaped medians and roadsides and adds greenspace for stormwater ponds improvements.

Finally, the project promotes multimodal connectivity by building bike connections along Ross Road, linking Woodville and Crawfordville Highways, and constructing sidewalks along the eastside of Shelfer Road in order link the high density residential to the surrounding multimodal connections.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Detail Project Description

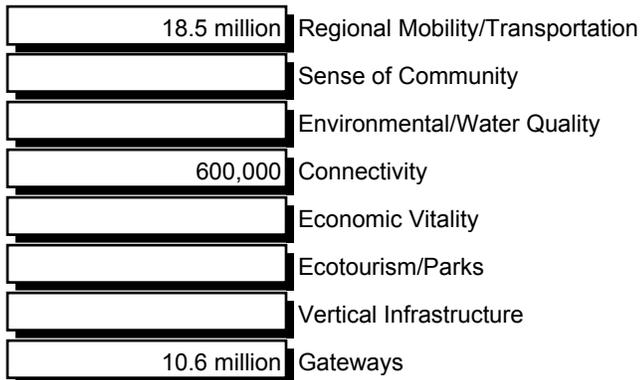
This project proposed improvements include the following:

- Establish a “gateway” feature entering the southside of the County (Woodville /Crawfordville Highways). This project is consistent with the regional mobility plan by providing additional capacity and linkage to complete a major corridor connector from downtown to the south.
- Widen Woodville Highway to four lanes between the improved Capital Circle and Gaile Avenue; The Southside Gateway Enrichment project enhances the roadway capacity by widening Woodville Highway between Capital Circle and Gaile Avenue to four lanes. This roadway also has a significant regional impact as it is a crucial north/south evacuation route from the coast. By increasing the capacity it will help decrease the evacuation times from the coastal areas. To date the Capital Regional Transportation Authority has spent approximately \$2.1 million to complete the Woodville Highway preliminary design and

engineering study. Final Design, permitting and right of way acquisition work will need to be completed before construction can begin on this transportation corridor.

- Construct sidewalks, bike lanes, and wide medians for landscaping, pedestrian refuge and adds sidewalk on eastside of Shelfer Road between Ross and Crawfordville Highway to link high density residential to area amenities. The Southside Gateway Enrichment project promotes multimodal connectivity, enhanced roadway capacity, landscaping, and possible gateway features advancing redevelopment potential of the surrounding area of the southside of Tallahassee.
- Reconstruct Crawfordville and Woodville Highways between Gaile and Tram to create an extended roundabout and greenspace for enhanced connectivity between these two main north south routes.
- Reconstruct a portion of the St. Marks Trail. The St. Marks Trail will be preserved and its usage enhanced with increased connectivity to existing sidewalk and trail networks.
- Provide greenspace allowing for water infiltration and stormwater ponds. This project replaces a grey landscape of asphalt and concrete with landscaped medians and roadsides. Additional greenspace for water infiltration and stormwater ponds enhances the livability of roadway segment. The new stormwater treatment facilities will improve the water quality leaving the roadway corridors.
- Landscape the stormwater ponds and roadway along Crawfordville Highway.
- Establish bike connections on Ross Road to connect Woodville and Crawfordville Highways.

Cost By Themes



Southside Gateway Enrichment

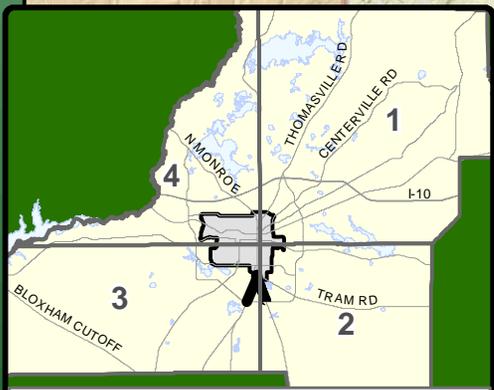
Widen to four lanes with sidewalks, bike lanes, St. Marks Trail landscaped medians
Evacuation route - capacity enhancement



Stormwater Pond Improvements

Sidewalks

Landscaping Improvements



Quadrants: 2 and 3

Project #4



Previous Project Information for:

Project #4

Southside Gateway Enrichment

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Southside Gateway Enrichment (*Previously Project #61*)

Proposed Project for the Sales Tax Extension

Project #6: Southside Gateway Enrichment

Estimated Project Cost: \$29.7 million

Project Criteria:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input checked="" type="checkbox"/> Water Quality Enhancements & Green Infrastructure |
| <input checked="" type="checkbox"/> Regional Mobility | <input checked="" type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input checked="" type="checkbox"/> Greenway Master Plan | <input checked="" type="checkbox"/> Transportation Capacity Improvements |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input checked="" type="checkbox"/> Complements BP2000 Project | |

Project Description:

The Southside Gateway Enrichment project enhances regional mobility and connectivity, increases the viability of this southern commercial gateway corridor, and supports the goal of economic revitalization to the southside of Tallahassee. This roadway also has a significant regional impact as it is a crucial north/south evacuation route from the coast. The project enhances transportation capacity by widening Woodville Highway (from Capital Circle to Gaile Avenue) to four lanes. The project includes widening the medians for landscaping, pedestrian refuge, and safe turning movements, and increases opportunities for public transit. In addition the interconnection of Woodville and Crawfordville Highways, via a looped roadway, enhances the operation of the corridors and provides a large greenspace to establish a gateway into the southside of Tallahassee. The project replaces a grey landscape of asphalt and concrete along Crawfordville Highway with landscaped medians and roadsides and adds greenspace for stormwater ponds improvements. Finally, the project promotes multimodal connectivity by building bike connections along Ross Road, linking Woodville and Crawfordville Highways, and constructing sidewalks along the eastside of Shelfer Road in order link the high density residential to the surrounding multimodal connections.

This project proposed improvements include the following:

- Widen Woodville Highway to four lanes between the improved Capital Circle and Gaile Avenue;
- Establish a “gateway” feature entering the southside of the County (Woodville /Crawfordville Highways);
- Construct sidewalks, bike lanes, and wide medians for landscaping, pedestrian refuge;
- Reconstruct Crawfordville and Woodville Highways between Gaile and Tram to create an extended roundabout and greenspace for enhanced connectivity between these two main north south routes;
- Add sidewalk on eastside of Shelfer Road between Ross and Crawfordville Highway to link high density residential to area amenities;
- Enhance opportunities for public transportation;
- Reconstruct a portion of the St. Marks Trail;
- Provide greenspace allowing for water infiltration and stormwater ponds;
- Landscape the stormwater ponds and roadway along Crawfordville Highway;
- Establish bike connections on Ross Road to connect Woodville and Crawfordville Highways.

Project Criteria Detail:

Comprehensive Plan: This project supports the Mobility Element Policies of the Comprehensive Plan by identifying and programming of new road projects or substantial improvements to existing roads shall be consistent with the Future Land Use Element of the Comprehensive Plan and specifically the

Project #6: Southside Gateway Enrichment

Page 2

Urban Service Area strategy to promote urban infill and discourage urban sprawl. The areas that the project complies with are listed below.

- Mobility:
 - Policy 1.1.8 Development projects shall contribute to providing a safe, convenient, comfortable, and aesthetically pleasing transportation environment that promotes walking, cycling, and transit use.
 - Objective 1.4 Connectivity & Access Management: Reduce vehicle trip demand, increase access, and safety for cyclists and pedestrians, and preserve the integrity of the transportation system with effective connectivity and access management programs.
 - Policy 1.2.2: Safe and convenient facilities for pedestrians, cyclists and transit users shall be evaluated for all new road and road widening projects. Specifically, all road projects, including resurfacing projects, shall be evaluated for the addition of bicycle lanes or paved shoulders, and transit shelters where they did not previously exist
 - Policy 1.2.7: Require that all new or rebuilt multi-lane (four or six-lane) arterial and major collector streets be constructed with grassed and/or landscaped medians where sufficient right-of-way can be obtained, unless limited by environmental constraints.
 - Policy 1.3.4: Design, construct, and maintain transportation facilities to prevent flooding, minimize pollution, and maintain natural stormwater quantity, timing, rate, and direction of flow characteristics consistent with the adopted Stormwater Level of Service Standard.

Regional Mobility Plan: This project is consistent with the regional mobility plan by providing additional capacity and linkage to complete a major corridor connector from downtown to the south.

Greenway Master Plan: This project enhances the St. Marks Trail and provides multimodal connections to Cascade Park and existing trail system. The project connects directly to the Campbell Connector and Karst Pond Greenways.

Connectivity: The Southside Gateway Enrichment project promotes multimodal connectivity, enhanced roadway capacity, landscaping, and possible gateway features advancing redevelopment potential of the surrounding area of the southside of Tallahassee. Constructing the bike lanes along Ross Road will link Woodville and Crawfordville Highways and the sidewalk addition on Shelfer Road links high density residential to the surrounding multimodal connections

Complements Blueprint 2000 Project(s): The Southside Gateway Enrichment project complements the following Blueprint 2000 Projects: Capital Circle Southeast, Capital Circle Southwest, Capital Circle trail system and Capital Cascades Park and trail system.

Water Quality Enhancements and Green Infrastructure: The Southside Gateway Enrichment project replaces a grey landscape of asphalt and concrete with landscaped medians and roadsides. Additional greenspace for water infiltration and stormwater ponds enhances the livability of roadway segment. The new stormwater treatment facilities will improve the water quality leaving the roadway corridors. The St. Marks Trail will be preserved and its usage enhanced with increased connectivity to existing sidewalk and trail networks.

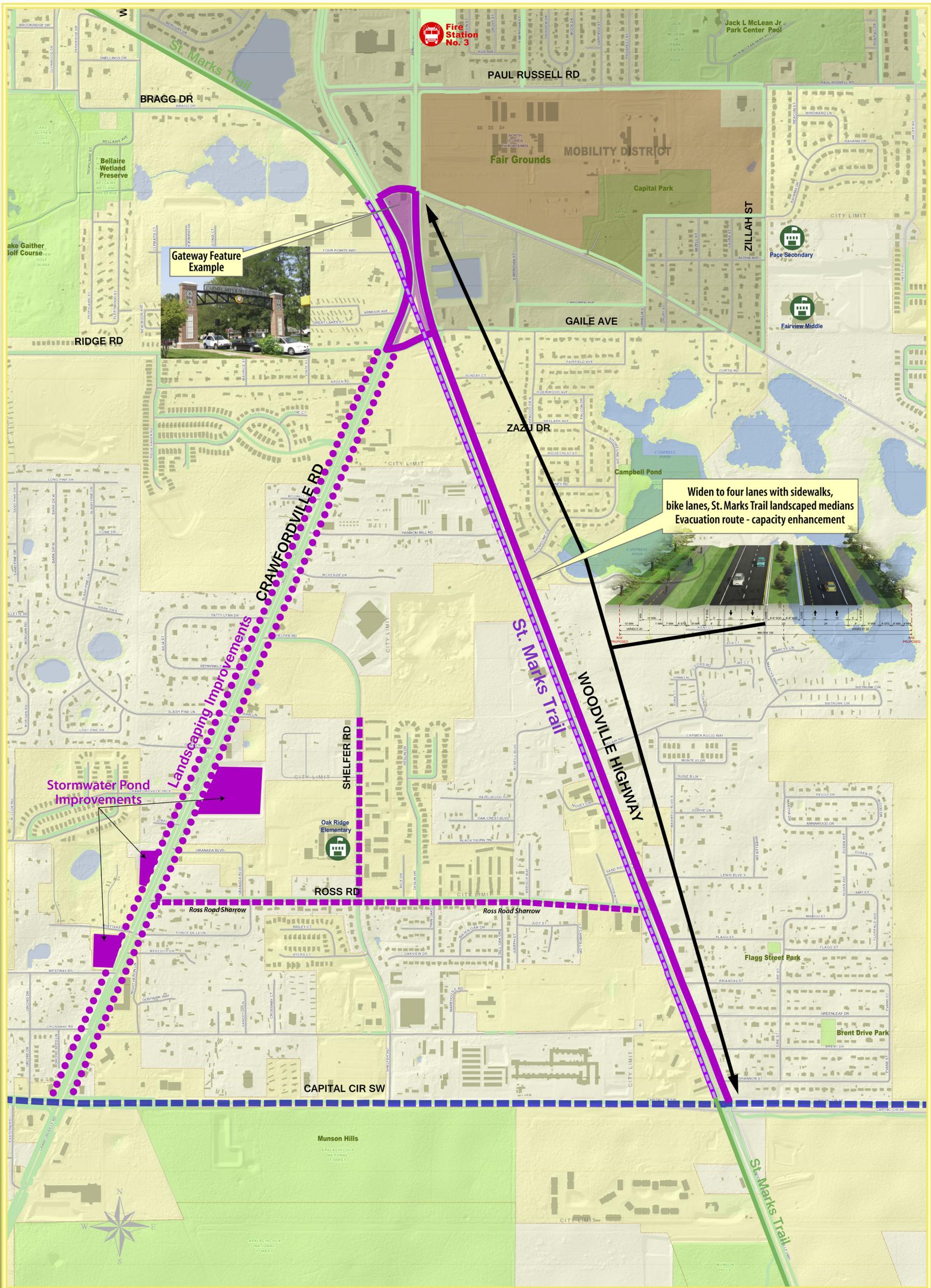
Stormwater/Sewer Capacity Improvements: The existing roadways were constructed without stormwater management facilities, which will be addressed during the proposed reconstruction of the Woodville Highway and the Crawfordville/Woodville greenspace feature.

Project #6: Southside Gateway Enrichment

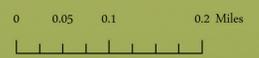
Page 3

Transportation Capacity Improvements: The Southside Gateway Enrichment project enhances the roadway capacity by widening Woodville Highway between Capital Circle and Gaile Avenue to four lanes. This roadway also has a significant regional impact as it is a crucial north/south evacuation route from the coast. By increasing the capacity it will help decrease the evacuation times from the coastal areas. The project includes widening the medians for landscaping, pedestrian refuge, and safe turning movements, enhanced opportunities for transit. In addition the interconnection of Woodville and Crawfordville Highways, via a looped roadway, enhances the operation of the corridors and provides a large greenspace to establish a gateway into the southside of Tallahassee. To date the Capital Regional Transportation Authority has spent approximately \$2.1 million to complete the Woodville Highway preliminary design and engineering study. Final Design, permitting and right of way acquisition work will need to be completed before construction can begin on this transportation corridor.

Core Infrastructure: The Southside Gateway Enrichment project is considered a core infrastructure project due the fact that Woodville Highway has a significant regional impact as it is a crucial north/south evacuation route from the coast. By increasing the capacity it will help decrease the evacuation times from the coastal areas. In addition, this project promotes multimodal connectivity, enhanced roadway capacity, landscaping, and possible gateway features advancing redevelopment potential of the surrounding area of the southside of Leon County.



Southside Gateway Enrichment



Legend

- Existing Sidewalks, Bike Routes
- Park Trails
- Woodville Hwy Southside Gateway
- Proposed Bike/Sidewalk Project
- Landscaping Improvements
- St. Marks Trail (Existing)
- Blueprint 2000 Projects
- Parks
- Fair Grounds

Southside Gateway Enrichment Project Criteria:

- Comprehensive Plan
- Regional Mobility
- Greenway Master Plan
- Connectivity
- Complements BP2000 Project
- Water Quality Enhancements & Green Infrastructure
- Stormwater/Sewer Capacity Improvements
- Transportation Capacity Improvements
- Core Infrastructure



This product has been compiled from the most accurate source data from Leon County and the City of Tallahassee. However, this product is for reference purposes only and is not to be construed as a legal document or survey instrument. Any reliance on the information contained herein is at the user's own risk. Leon County and the City of Tallahassee assume no responsibility for any use of the information contained herein or any loss resulting therefrom.

SALES TAX PROJECTS

Project Number: 5
Project Name: Lake Bradford Road
Total Project Cost: \$37,271,738

Total Project Cost Notes

Conceptual Estimate of Probable Costs

- Lake Bradford Road From Gamble To Csx R/R Overpass (Includes roundabout at Gamble): 0.1 miles, \$1,253,525
- Lake Bradford Road From CSX R/R Overpass To Eppes Drive (Includes roundabout at Eppes): 0.2 miles, \$2,446,300
- Lake Bradford Road From Eppes Drive To Stadium/Gaines (Includes roundabout at Stadium & Airport): 0.37 miles, \$6,273,593
- Eppes Drive From Jackson Bluff Road To Lake Bradford Road: 0.43 miles, \$2,997,195
- Eppes (Hendry) Drive From Stadium Drive To Jackson Bluff Road: 0.14, \$178,985
- Airport Drive From Eppes Drive To Lake Bradford Road: 0.14 miles \$650,610
- Jackson Bluff Road From Eppes Drive To Lake Bradford Road (includes pavement removal): 0.32 miles, \$556,680
- Gaines Street From Stadium To Woodward (includes roundabout at Woodward): 0.52 miles, \$5,794,850
- Lake Bradford Road From Orange Avenue To Gamble Street: 0.47 miles, \$17,120,000

Executive Project Summary

This Gateway project would be a continuation of the investment and redevelopment spurred by the first two phases of the Gaines Street improvements. At present, three major projects, Gaines Street, Lake Bradford Gateway and FAMU Way, are converging in this area. With additional improvements along the Lake Bradford corridor, we can further set the stage for redevelopment and reinvestment in this area as well and provide and an attractive entrance into the City from the Airport.

The purpose of the Gateway is to:

- Create a unique urban gateway to Tallahassee from the Airport
- Allow more organized and consolidated development with installation of stormwater improvements
- Better traffic circulation in and around the Stadium, FSU and the future FAMU Way

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Detail Project Description

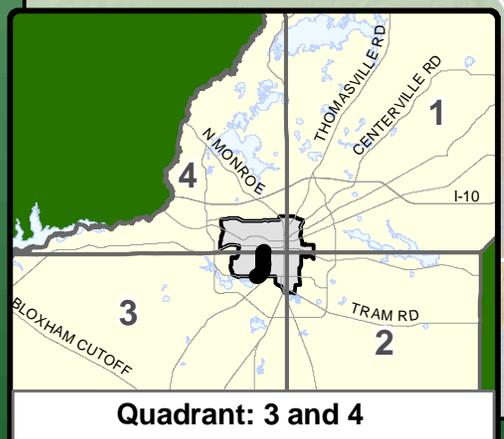
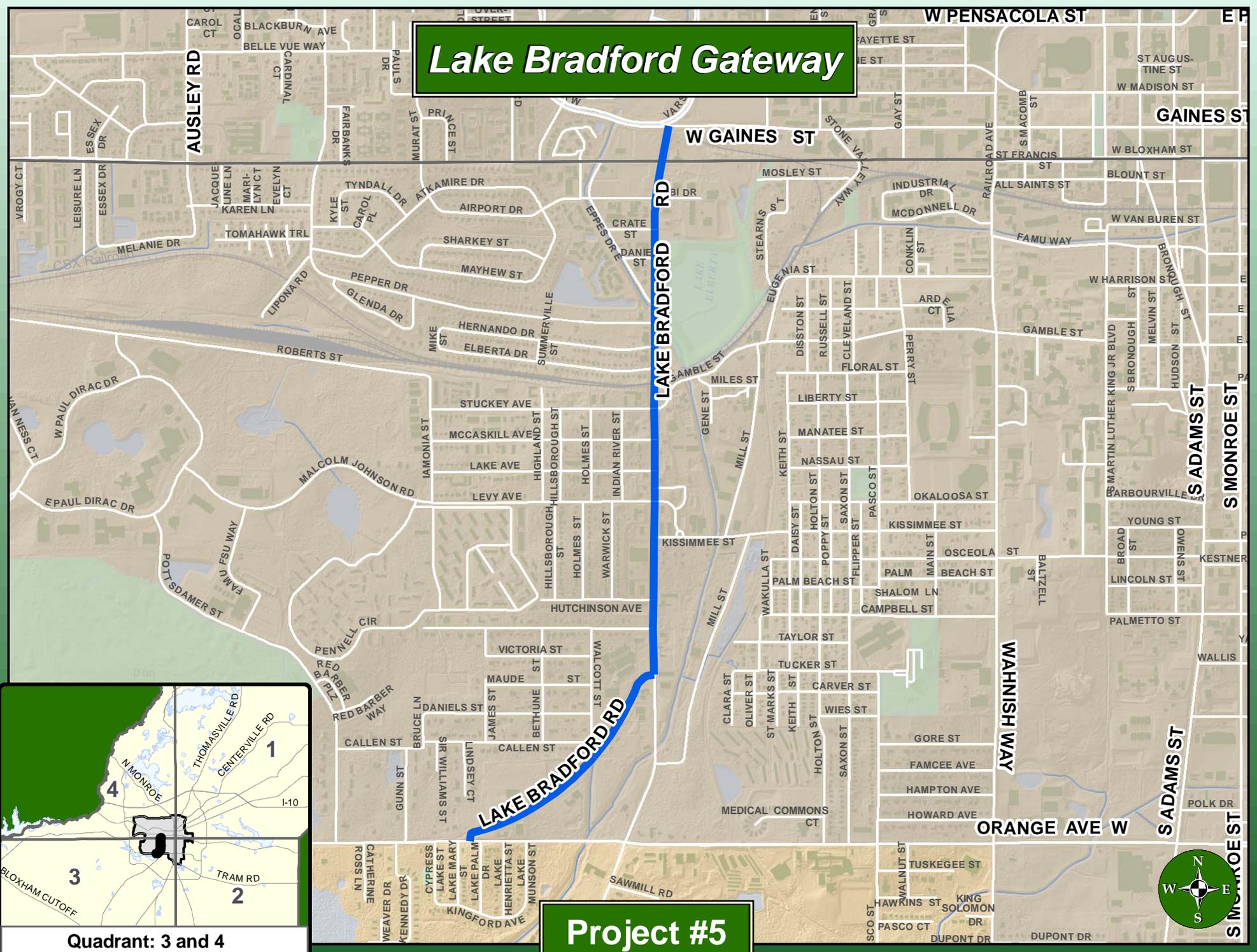
The gateway streetscaping improvements would include roadway reconstruction to allow for bicycle

facilities and wider sidewalks, roundabouts, and other pedestrian safety enhancements. It would also include aesthetic elements such as landscaping and lighting.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
\$37,271,738	Gateways

Lake Bradford Gateway



Quadrant: 3 and 4

Project #5



Previous Project Information for:

Project #5

Lake Bradford Gateway

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Building the Core: Lake Bradford Gateway (*Previously Project #18*)

Florida State University – Lake Bradford Gateway Extension to Orange Avenue. (*Previously Project #71*)

Program
1c

Building the Core: Lake Bradford Gateway

Estimated Cost:
\$37 million

WHAT IS IT?

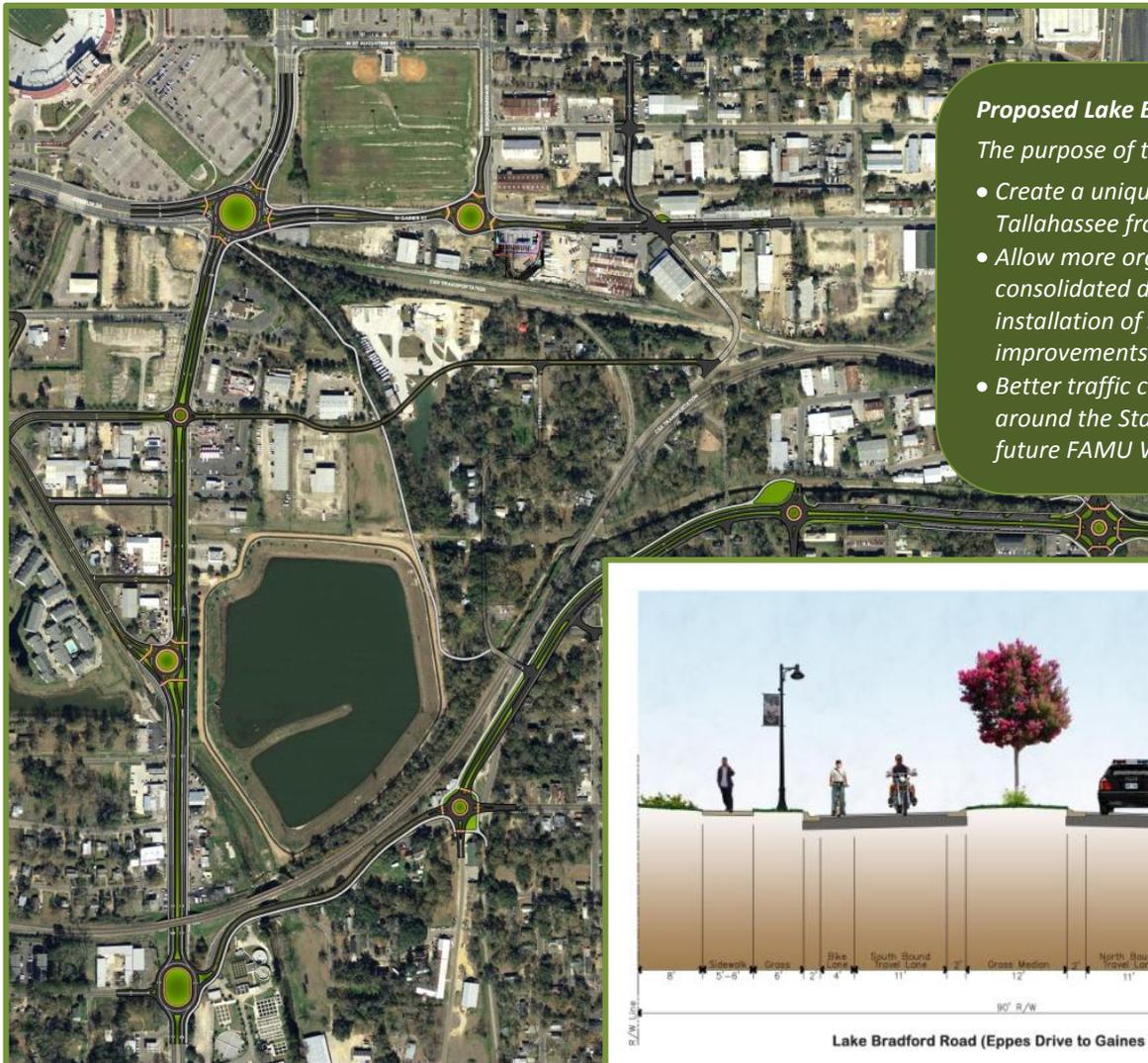
Combining it with other projects within this special district area of the overall Mobility District could create the opportunity for additional redevelopment of the southwestern quadrant of the urban core. This would be a continuation of the redevelopment spurred by the first two phases of the Gaines Street improvements. At present, three major projects, Gaines Street, Lake Bradford Gateway and FAMU Way, are converging in this area. With few additional improvements to the existing concepts, we can set the stage for redevelopment of the area. The project elements include:

- Roundabout at Gaines Street / Stadium
- Roundabout at Gamble
- Landscaping
- Eppes Drive and Hendry Street Connections
- Major stormwater conveyance system from the Stadium to the north end of Lake Elberta

Evaluation Criteria:

Provides Regional Impact / Geographic Equity ✓
Enhances Mobility ✓
Supports Recreation and Quality of Life ✓

Benefits the Environment ✓
Supports Economic Development / Revitalization ✓
On an Existing Master Plan or Blueprint List ✓
Leverages Other Funds ✓



Proposed Lake Bradford Investments

The purpose of the Gateway is to:

- Create a unique urban gateway to Tallahassee from the Airport
- Allow more organized and consolidated development with installation of stormwater improvements
- Better traffic circulation in and around the Stadium, FSU and the future FAMU Way



WHY IS IT IMPORTANT?

Benefits the Environment – By increasing access to the St. Marks Trail, a major recreational and commuter bike facility, and by providing safer, more convenient bicycle and pedestrian access to FSU, FAMU, Downtown and Gaines Street, this project would encourage increased use of alternative transportation modes, which results in reduction in fossil fuel use and increased air quality.

Provides Regional Impact / Geographic Equity – Since this corridor is the main entrance to town from the airport, the entire region stands to benefit from creating a positive first impression on visitors. The mobility and aesthetic enhancements also would benefit the neighborhoods and blighted properties in the area.

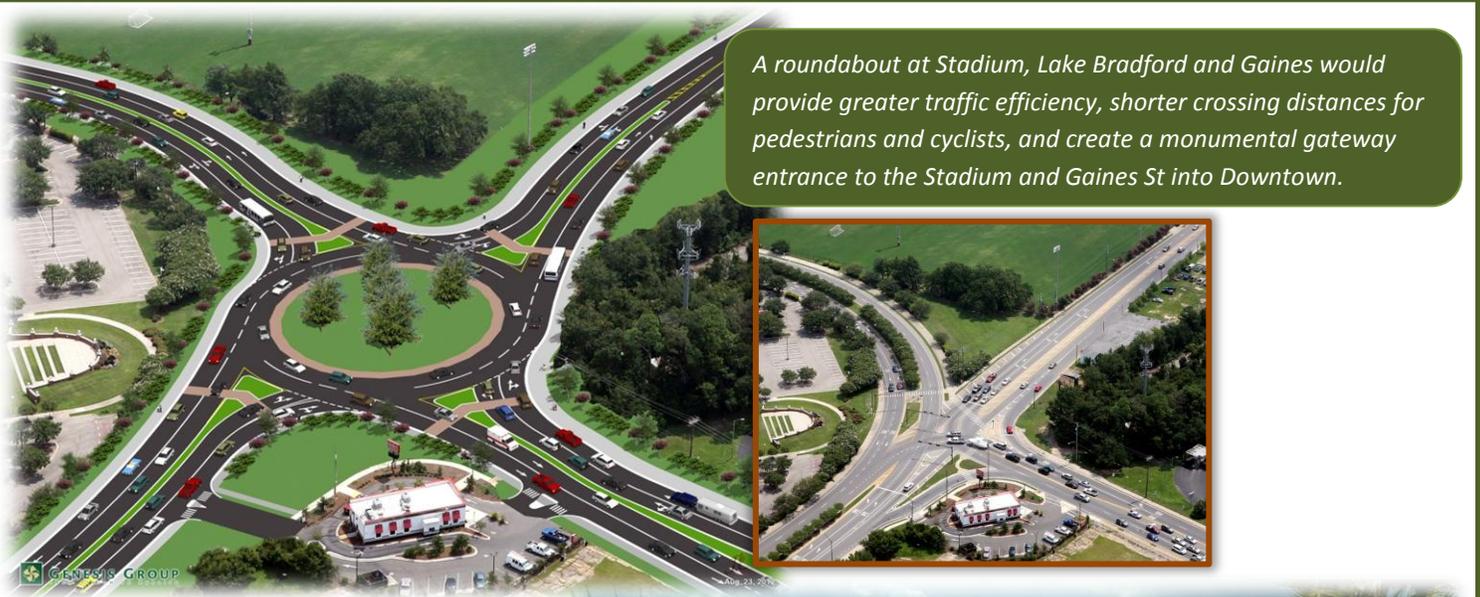
Enhances Mobility – The project would include bike lanes, rebuilt sidewalks, landscaping, roundabouts and signage, which would increase the safety and access for all modes. It would also allow easier movement of north/south traffic at the intersection of Gaines.

Supports Recreation and Quality of Life – Enhancements along this area would support access to the Lake Elberta Park as well as connections to the St. Marks Trail. Landscaping, lighting and transportation investments would increase the mobility and safety for residents, students, and business patrons in the area.

Supports Economic Development / Revitalization – This project would enhance private investments near the area, such as Collegetown just north, and support further high quality urban redevelopment by creating an aesthetically pleasing and safe pedestrian, bicycle and vehicular infrastructure in walking distance to FSU, FAMU, and Gaines Street.

On an Existing Master Plan or Blueprint List – This has been an identified Gateway project for the City for nearly ten years.

Leverages Other Funds – The City has earmarked approximately 3 million dollars for gateway enhancements in this area.



A ground level view of how Lake Bradford Road south of Stadium would appear. The more attractive corridor could facilitate redevelopment.



Proposed Sales Tax Projects by the Community

Project Name: Florida State University – Lake Bradford Gateway Extension to Orange Avenue
(Project #71)

Project Themes:

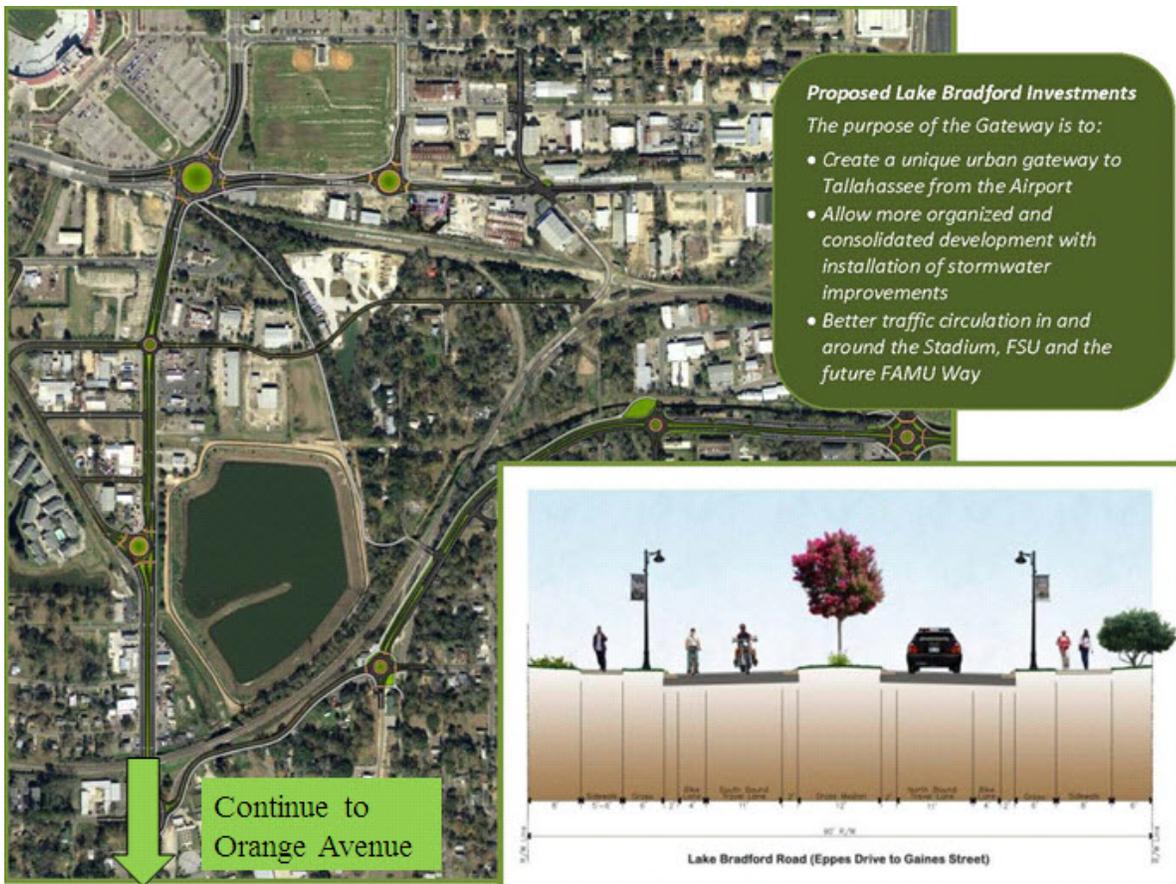
- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Project Description:

On March 14, 2013, the Florida State University presented several projects for the consideration of the Sales Tax Committee. This project is for the extension of Lake Bradford Gateway Extension to Orange Avenue. The project description for this project was developed using the materials by FSU during their presentation to the Committee.

The University is requesting that the Lake Bradford Gateway project be extended to Orange Avenue. By extending this gateway to Orange Avenue, it would provide an entrance to the downtown core and the Main Campus. This would provide for enhanced signage, landscaping, and similar improvements along this gateway corridor.

Project Map:



SALES TAX PROJECTS

Project Number: 6
Project Name: North Monroe Gateway
Total Project Cost: \$5 million
Total Project Cost Notes

Executive Project Summary

Many visitors, residents, and former residents, including those coming to FSU football games, enter Tallahassee via North Monroe Street. However, currently the very automobile-oriented right-of-way and empty or underutilized properties along the corridor, such as the Tallahassee Mall, do not present an attractive nor thriving first impression. Some private reinvestment has occurred on its own, but public investment in the corridor by way of landscaping, medians, and other enhancements, combined with opportunities for private businesses, such as facade enhancement programs, could improve this significant entrance into the City and catalyze further redevelopment of the properties. This project has been spearheaded by the Knight Creative Communities Initiative's "Gateway Tallahassee" citizen group.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

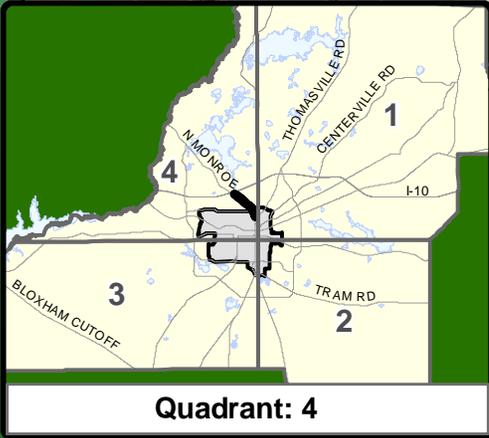
Detail Project Description

The Monroe Gateway begins just off Interstate 10 and is used by residents and visitors to the Tallahassee region. Gateway enhancements, including signage, art, crosswalks and other pedestrian safety enhancements, will create a more walkable corridor and provide a more welcoming experience for all who use the area. The project will improve aesthetics, with the potential to promote reinvestment and improve the quality of life for residents and daily commuters. The North Monroe Gateway Project can transform the city's most heavily traveled entryway into a welcoming entrance that clearly identifies and brands Tallahassee. A portion of the gateway (John Knox to Tharpe Street) is in the Monroe Street Access Management and Lake Ella Implementation Study Area for the Capital Region Transportation Planning Agency. Monroe Street is a state highway, so leveraging of funding could be available through state and federal programs. The Gateway would also complement the Midtown Sense of Place Project, which proposes landscaping along Monroe from Tharpe to the Thomasville Road intersection.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
\$5 million	Gateways

North Monroe Gateway



Quadrant: 4

Project #6

Previous Project Information for:

Project #6

North Monroe Gateway

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Create a Positive First Impression: Revitalize the North Monroe Gateway (*Previously Project #27*)

Program
4e

Create a Positive First Impression: Revitalize the North Monroe Gateway

Estimated Cost:
\$5 million

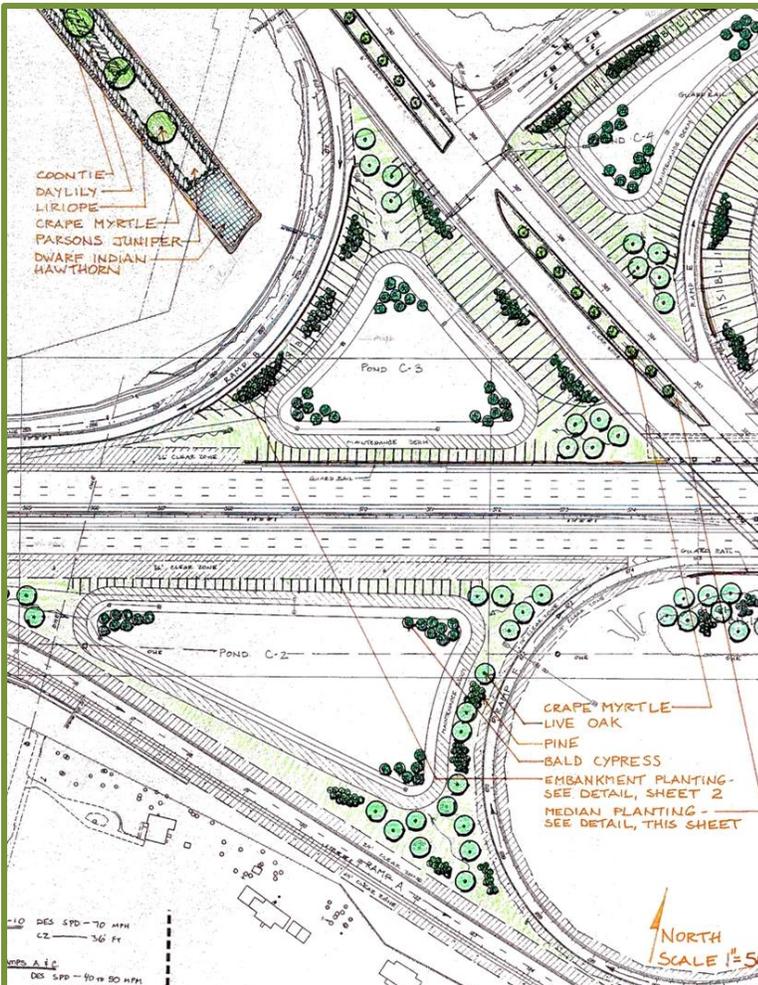
WHAT IS IT?

Many visitors, residents, and former residents, including those coming to FSU football games, enter Tallahassee via North Monroe Street. However, currently the very automobile-oriented right-of-way and empty or underutilized properties along the corridor, such as the Tallahassee Mall, do not present an attractive nor thriving first impression. Some private reinvestment has occurred on its own, but public investment in the corridor by way of landscaping, medians, and other enhancements, combined with opportunities for private businesses, such as facade enhancement programs, could improve this significant entrance into the City and catalyze further redevelopment of the properties. This project has been spearheaded by the Knight Creative Communities Initiative's "Gateway Tallahassee" citizen group.

Evaluation Criteria:

Provides Regional Impact / Geographic Equity ✓
Enhances Mobility ✓
Supports Recreation and Quality of Life ✓

Benefits the Environment ✓
Supports Economic Development / Revitalization ✓
On an Existing Master Plan or Blueprint List ✓
Leverages Other Funds ✓



"Gateways Provide A First Impression for Visitors and Residents."

Gateway Tallahassee's mission is to transform strategic pathways into the city into remarkable points of entry that clearly identify and brand Tallahassee as a vibrant, thriving and welcoming community.

Above, public investment in landscaping has already been made around the redesigned I-10 intersection.

WHY IS IT IMPORTANT?

Provides Regional Impact / Geographic Equity – The Monroe Gateway is right off Interstate 10 and is used by residents and visitors to our city and the Tallahassee area.

Enhances Mobility – Gateway enhancements will create a more walkable corridor and provide a more welcoming experience for drivers and cyclists in the area.

Supports Recreation and Quality of Life – The project will improve the aesthetics of the area and the pedestrian environment, promote reinvestment, and provide other amenities that will improve the quality of life of residents in the community.

Supports Economic Development / Revitalization – Safe and attractive corridors are good for business and the overall community. The North Monroe Gateway Project will transform the city's most heavily traveled entryway into a welcoming entrance that clearly identifies and brands Tallahassee.

On an Existing Master Plan or Blueprint List – A portion of the gateway (John Knox to Tharpe Street) is in the Monroe Street Access Management and Lake Ella Implementation Study Area for the Capital Region Transportation Planning Agency.

Leverages Other Funds – Monroe Street is a state highway. Funding could be available through state and federal programs. The Gateway would also complement the Midtown Sense of Place Project which proposes landscaping along Monroe from Tharpe to the Thomasville Road intersection.

At right, North Monroe Street is difficult for pedestrians and cyclists to navigate. It also has many places, such as medians, which could be improved significantly by investing in landscaping.

Below, examples of possible Gateway elements.



In 2012, the Gateway Tallahassee group has studied the area and brought stakeholders together, coming to the following conclusions:

- *Improvements are needed on North Monroe to **bolster safety and aesthetics.***
- *They are concerned about the effects of **declining property values and the retailers leaving Tallahassee Mall** on the business community.*
- *Tallahassee has the opportunity to **identify North Monroe as a vital gateway** into the city and use it to **brand the city** and its amenities.*
- *They realize improving North Monroe's sense of place will take a **financial infusion and cooperation** among different government agencies.*
- *An association is needed to **advocate** on behalf of North Monroe, especially to better position the area to **capitalize** on any future funding opportunities.*

SALES TAX PROJECTS

Project Number: 7
Project Name: Bike Route System
Total Project Cost: \$15 million

Total Project Cost Notes

The first phase uses signage on existing roadways and trails and could be implemented for an estimated cost of \$5 million. A second phase includes identification of infrastructure improvements that could further enhance bicycle connectivity over the longer term - such as bridges, midblock crossings and off-road trails - with an estimated cost of \$10 million.

Executive Project Summary

In coordination with local cyclists, a bicycle route system using existing roads has been identified. The first step was to design a system for the Mobility District, and then move into the whole City and County to provide a true commuting alternative. This project is closely integrated with the Greenways Master Plan in that on road facilities will provide connectivity between the various greenways and parks.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

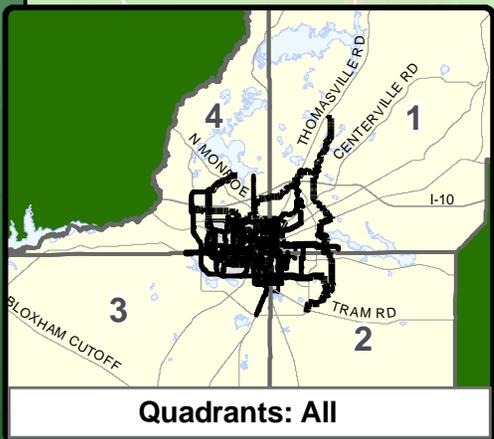
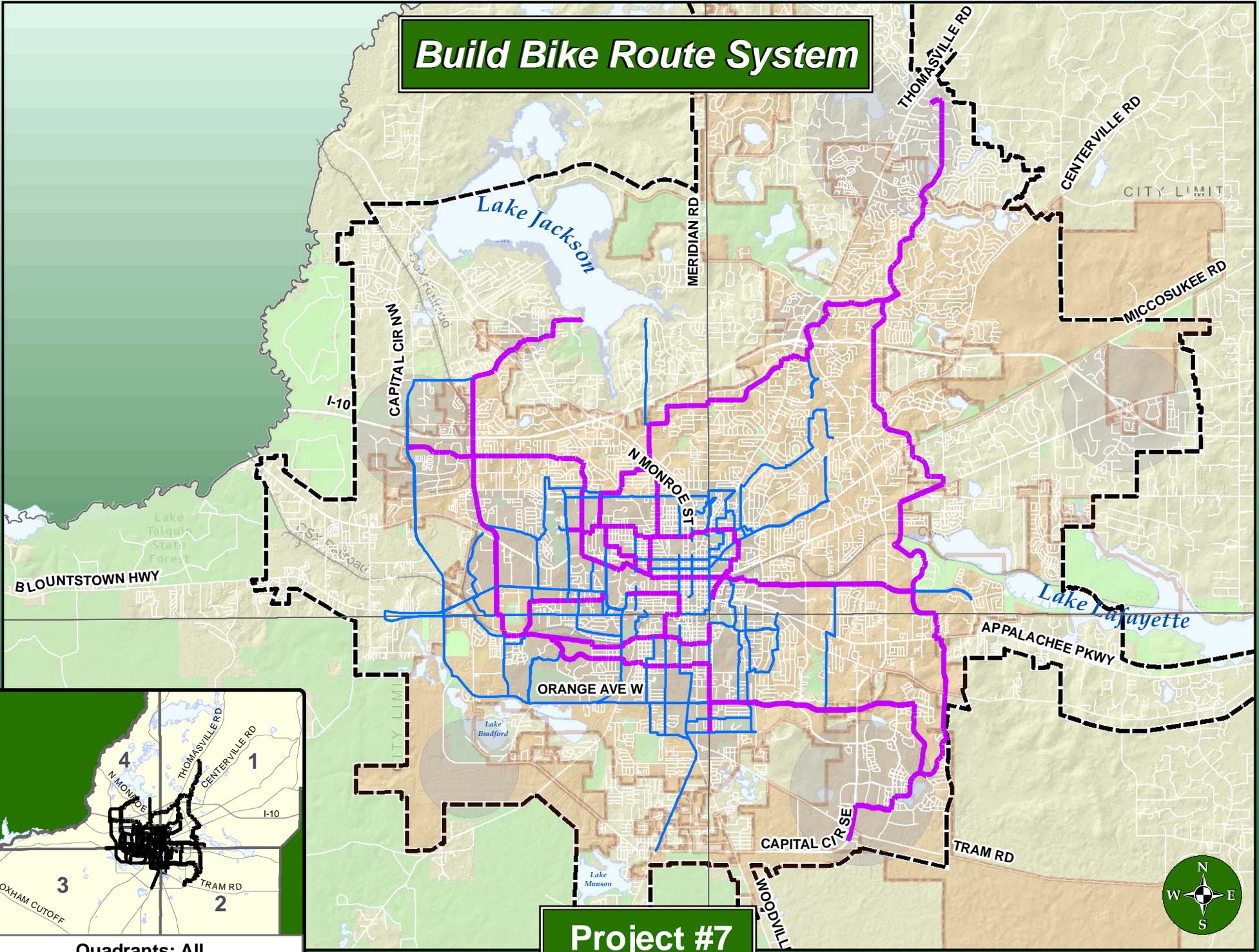
Detail Project Description

Routes have been planned throughout Leon County to promote both short trips within an area and longer trips traversing the County. The bicycle route network will connect various areas of the community via signed bicycle routes. The network will enhance recreational opportunities for residents and visitors by connecting trails, greenways, and transit service. Signage designating roadways as bicycle routes may also include directional information to surrounding attractions. A well developed bicycle route system also provides increased access to business and services, and is an amenity which can be attractive to many people looking to locate to the region. A safe, attractive system of bicycle facilities makes cycling more desirable as an alternative to driving, leading to less fuel usage, lower emissions, and better air quality. The proposed Bicycle Route System would implement numerous projects from the 2004 Bicycle and Pedestrian Master Plan, which were then carried into the Regional Mobility Plan and Mobility District Plan.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
\$15 million	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Build Bike Route System



Quadrants: All

Project #7

**Program
3b**

Connecting the Community: Build the Bike Route System

**Estimated Cost:
\$15 million**

WHAT IS IT?

In coordination with local cyclists, a bicycle route system using existing roads has been identified. The first step was to design a system for the Mobility District, and then move into the whole City and County to provide a true commuting alternative. The first phase uses signage on existing roadways and trails and could be implemented for an estimated cost of \$5 million. A second phase includes identification of infrastructure improvements that could further enhance bicycle connectivity over the longer term - such as bridges, midblock crossings and off-road trails - with an estimated cost of \$10 million.

Evaluation Criteria:

- Provides Regional Impact / Geographic Equity ✓
- Enhances Mobility ✓
- Supports Recreation and Quality of Life ✓

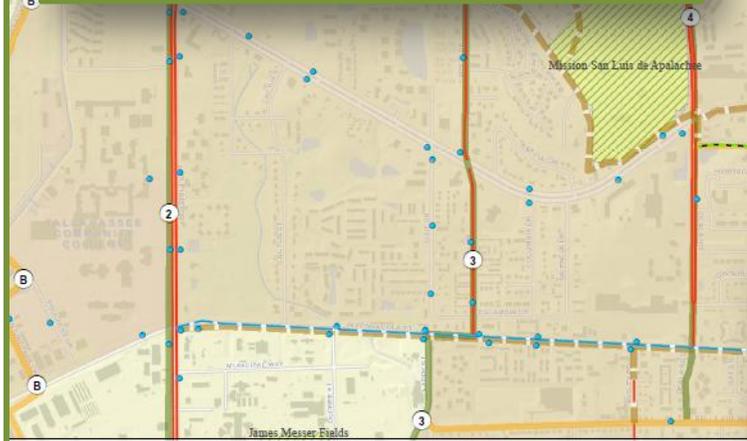
- Benefits the Environment ✓
- Supports Economic Development / Revitalization ✓
- On an Existing Master Plan or Blueprint List ✓
- Leverages Other Funds ✓



Signage, markings, and crossings are needed to designate bicycle facilities.



Each online route map can display locations of StarMetro transit stops, connecting bicycle routes, and link to StarMetro schedules for intersecting transit routes.



Mobility District: Proposed Route #4

Proposed Bicycle Route	Existing Facilities - Bicycle Lane	StarMetro Transit Parks
Proposed Route Connector	Existing Facilities - Shared ROW	Mobility District
Alternative Route	Existing Facilities - Shared Use Path (Paved)	
Funded Bicycle or Pedestrian Improvements	Existing Facilities - Shared Use Path (Natural Surface)	
Potential Future Connection: (Unfunded)		



On and off road routes would tie into the Greenways System.

WHY IS IT IMPORTANT?

Benefits the Environment – A safe, attractive system of bicycle facilities makes cycling more desirable as an alternative to driving. Reduced use of automobiles results in less fuel usage, lower emissions, and better air quality.

Provides Regional Impact / Geographic Equity – The bicycle route network will serve to connect various areas of the community via signed bicycle routes. Routes are planned throughout Leon County promoting both short trips within an area and longer trips traversing the County.

Enhances Mobility – One of the key components of a complete multimodal transportation network is a system of designated bicycle routes. Implementation of the route network will improve mobility options for people of all ages by connecting dedicated bicycle facilities and trails to local destinations.

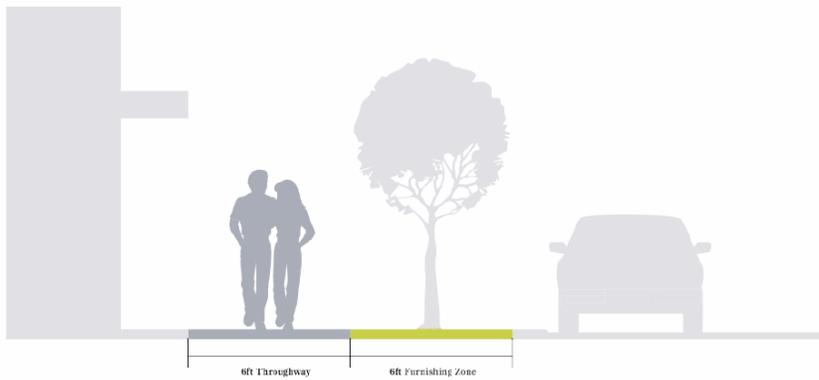
Supports Recreation and Quality of Life – The network will enhance recreational opportunities for residents and visitors by connecting trails, greenways, and transit service. Signage designating roadways as bicycle routes may also include directional information to surrounding attractions.

Supports Economic Development / Revitalization – A well developed bicycle route system provides increased access to business and services, and also is an amenity which can be attractive to many people looking to locate in Tallahassee.

On an Existing Master Plan or Blueprint List – The Bicycle Route System would implement numerous projects listed on the 2004 Bicycle and Pedestrian Master Plan, which were then carried into the Regional Mobility Plan and Mobility District Plan.

Leverages Other Funds – The City of Tallahassee is adding route signage and pavement markings whenever roads are repave for normal maintenance.

STREET/BIKE BOULEVARD



Treatments to create Bicycle Boulevards include raised intersections, bicycle boxes, signage, and bulb-outs.



Bicycle Boulevards

Roadway	Segment
Adams Street	Governor's Mansion to Pensacola Street
Georgia Street	Copeland Street to Calhoun Street
Glenview	North Monroe Street to Thomasville Road
Woodward Avenue	Alabama Street to Brevard Street

While most of the bike route system would consist of signage and pavement markings, some corridors are better suited to be Bicycle Boulevards.

Designated Bicycle Boulevards allow cars to travel on the road, but are designed to keep speeds slow and to favor movement on bicycles.



SALES TAX PROJECTS

Project Number: 8
Project Name: Complete the Sidewalk Network
Total Project Cost: \$101,437,238

Total Project Cost Notes

This project costs does not include sidewalks that are part of other proposed Sales Tax Extension projects (such as projects that include sidewalks as part of completing a road network). Therefore, the total cost for the completing the sidewalk network within the City of Tallahassee and Leon County is \$101,437,238. However, it should be noted that \$9 million of that is for a community request for sidewalks on both sides of streets in the Ox Bottom Manor neighborhood. These streets already have sidewalks on one side of the road.

Executive Project Summary

The sidewalk network is the first tier in our multimodal system. Its role is to safely and conveniently connect individual residences to schools, shopping and recreation, as well as to the collector and arterial system where transit is provided.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

This list includes 78 sidewalk projects from City and County priority lists, as well as those recommended to the Sales Tax Committee by members of the community. A list of each of these sidewalks is included in attachment #1.

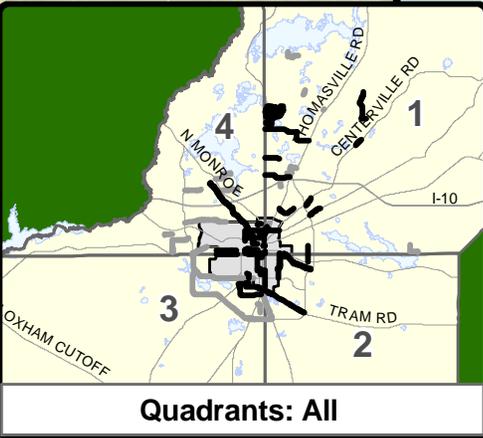
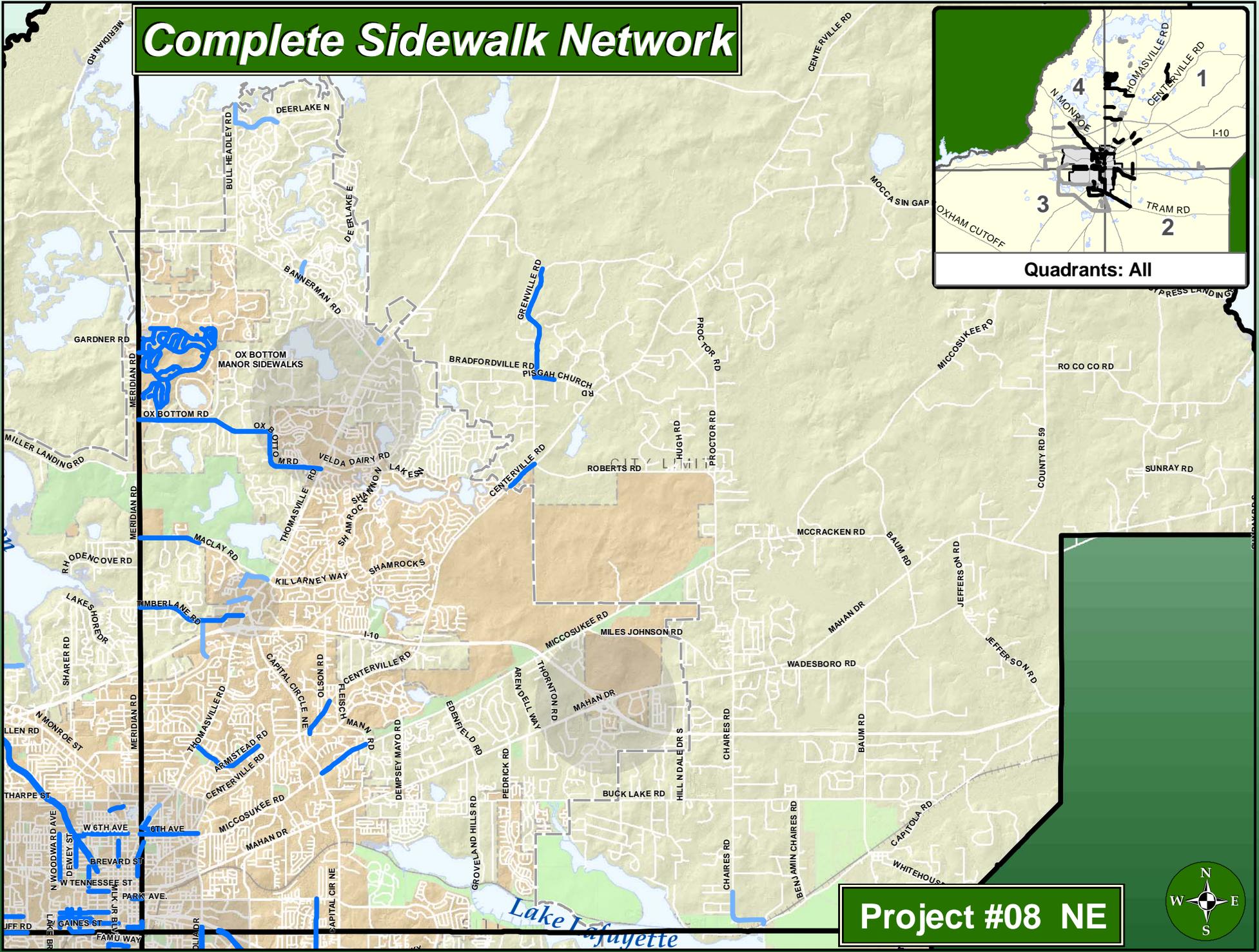
Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
\$101,437,238	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Stand Alone Sidewalk Projects			
Roadway	Segment	Improvement Type	Estimated Cost
Armistead Road	Thomasville Road to Woodgate Way	Bicycle Lanes/Sidewalk	\$7,417,800
Richview Road	Apalachee Parkkway to Park Avenue	Bicycle Lanes/Sidewalk	\$6,698,250
Jim Lee Road	Magnolia Drive to Orange Avenue	Sidewalk	\$538,000
Springsax Road	Springhill Road to Pasco Street	Bicycle Lanes/Sidewalk	\$4,625,400
Bragg Drive	Parkridge Drive to South Adams Street	Sidewalk	\$587,800
Springsax Road	Ridge Road to Galimore Drive	Sidewalk	\$390,000
Tanner Drive	Galimore Drive to Parkridge Drive	Sidewalk	\$318,000
Ridge Road	Springsax Road to Crawfordville Highway	Sidewalk	\$2,006,600
Woodward Avenue	St. Augustine Street to West Gaines Street	Sidewalk	\$94,600
Sixth Ave East	Thomasville Road to North Magnolia Drive	Sidewalk	\$551,200
Sixth Ave West	North Monroe Street to Old Bainbridge Road	Sidewalk	\$193,600
Jackson Bluff Road	Mabry Street to Hendry Street	Sidewalk	\$1,660,000
Central 10th Avenue	At Duval Street - North Monroe Street at Legion Street	Shared-use Path	\$125,000
Florida A&M University area	FAMU Way/Oakland Avenue from Martin Luther King JR Boulevard to South Monroe	Sidewalk and Intersection Improvements	\$734,000
Seventh Avenue East	Thomasville Road to Colonial Drive (North Side)	Sidewalk	\$1,200,000
Palmetto Street	Martin Luther King JR Boulevard to South Adams Street	Sidewalk	\$1,067,000
Stone Valley Way Alignment	Gaines Street to Railroad Avenue	Bicycle Boulevard	\$300,000
Meridian Street	Magnolia Street to Paul Russell Road	Bicycle Lanes/Sidewalk	\$2,294,788
Pasco Street	Wies Street to Orange Avenue	Sidewalk	\$294,800
Putnam Drive	South Adams Street to South Meridian Street	Sidewalk	\$502,400
East Park Avenue	South Calhoun Street to South Meridian Street	Sidewalk	\$141,200
Martin Luther King JR Boulevard	FAMU Way to Palmetto Street	Bicycle Boulevard/Sidewalk	
North Duval Street	Fifth Avenue West to West Brevard Street	Sidewalk	\$383,200
Dewey Street	Rollins Street to Brevard Street	Sidewalk	\$331,600
Wies Street	Holton Street to Pasco Street	Sidewalk	
Osceola Street	Martin Luther King JR Boulevard to South Adams Street	Sidewalk	\$203,000
South Belle Vue Way	Mabry Street to Hayden Road	Bicycle Lanes/Sidewalk	\$8,424,000
Centerville Road	Pimlico to Roberts Road		\$950,000
Centerville Road	Buford Boulevard to Fleischmann (East Side)	Sidewalk	\$800,000
Maclay	Meridian Road to City Limits	Sidewalk	\$1,980,000
Miccosukee Road	Capital Circle to Fleischman	Sidewalk	\$3,620,000
Ox Bottom Road	Meridian Road to Thomasville Road	Sidewalk	\$3,100,000
Timberlane Road	Meridian Road to Thomasville Road	Sidewalk	\$850,000
Tram Road	South Monroe Street to Capital Circle	Sidewalk	\$2,550,000
Magnolia Drive	Lafayette to Toochin Nene (East Side)	Sidewalk	\$850,000
Magnolia Drive	Lafayette to Toochin Nene (West Side)	Sidewalk	\$850,000
Magnolia Drive	Toochin Nene to Hokolin Nene (East Side)	Sidewalk	\$500,000
Magnolia Drive	Toochin Nene to Hokolin Nene (West Side)	Sidewalk	\$500,000
Magnolia Drive	Hokolin Nene to Jim Lee Road (East Side)	Sidewalk	\$620,000
Magnolia Drive	Hokolin Nene to Jim Lee Road (West Side)	Sidewalk	\$620,000

Magnolia Drive	Monroe Street to Meridian Street (North Side)	Sidewalk	\$400,000
Magnolia Drive	Monroe Street to Meridian Street (South Side)	Sidewalk	\$400,000
Old St. Augustine	Indianhead to Blairstone (North Side)	Sidewalk	\$1,200,000
Old St. Augustine	Indianhead to Blairstone (South Side)	Sidewalk	\$1,200,000
Old St. Augustine	Blairstone to Paul Russell Road (North Side)	Sidewalk	\$800,000
Old St. Augustine	Blairstone to Paul Russell Road (South Side)	Sidewalk	\$800,000
Old St. Augustine	Paul Russell Road to Midyette (North Side)	Sidewalk	\$1,100,000
Old St. Augustine	Paul Russell Road to Midyette (South Side)	Sidewalk	\$1,100,000
Old St. Augustine	Midyette to Capital Circle (North Side)	Sidewalk	\$650,000
Old St. Augustine	Midyette to Capital Circle (South Side)	Sidewalk	\$650,000
Gadsden Street	Carolina Street to McDaniel Street (East Side)	Sidewalk	\$470,000
Gadsden Street	McDaniel Street to Ingleside (East Side)	Sidewalk	\$530,000
Gadsden Street	Ingleside to East Seventh Avenue (East Side)	Sidewalk	\$390,000
Gadsden Street	East Seventh Avenue to East Ninth Avenue (East Side)	Sidewalk	\$330,000
Gaines Street	Monroe Street to Gadsden Street (North Side)	Sidewalk	\$200,000
Gaines Street	Monroe Street to Gadsden Street (South Side)	Sidewalk	\$100,000
Gaines Street	Gadsden Street to Lafayette Street (North Side)	Sidewalk	\$100,000
Gaines Street	Gadsden Street to Lafayette Street (South Side)	Sidewalk	\$100,000
Greenville Road	Pisgah Church Road to Proctor Road (West Side)	Sidewalk	\$3,300,000
Old Bainbridge Road	Georgia Street to Brevard Street (East Side)	Sidewalk	\$225,000
Old Bainbridge Road	Georgia Street to Brevard Street (West Side)	Sidewalk	\$225,000
Old Bainbridge Road	Brevard Street to Interstate 10 (East Side)	Sidewalk	\$4,470,000
Old Bainbridge Road	Brevard Street to Interstate 10 (West Side)	Sidewalk	\$4,100,000
Old Bainbridge Road	Interstate 10 to Fred George Road (East Side)	Sidewalk	\$3,000,000
Old Bainbridge Road	Interstate 10 to Fred George Road (West Side)	Sidewalk	\$3,000,000
Pisgah Church Road	West end of existing trail to Greenville Road	Sidewalk	\$230,000
Ox Bottom Manor Sidewalks	Roadways within and surrounding the Ox Bottom Manor Neighborhood	Sidewalk on other sides of roads (sidewalks exist on one side)	\$9,000,000
Call Street	Copeland Street to Martin Luther King JR Boulevard (South Side)	Sidewalk	\$400,000
St. Augustine Street	Woodward Avenue to South Maccomb Street (North Side)	Sidewalk	\$775,000
St. Augustine Street/Madison Street	Railroad Avenue to Duval Street (South Side)	Sidewalk	\$525,000
East Pensacola Street	Woodward Avenue to FSU College of Law (North Side)	Sidewalk	\$850,000
Gay Street	Gaines Street to Jefferson Street	Sidewalk	\$240,000
Collier Street	St. Augustine Street to Madison Street	Sidewalk	\$75,000
Lorene Street	Jefferson Street to St. Augustine Street	Sidewalk	\$200,000
North Woodward Avenue	Alabama Street to Brevard Street	Sidewalk	\$675,000
North Woodward Avenue	West Tennessee Street to Brevard Street (East Side)	Sidewalk	\$275,000
North Copeland Street	Brevard Street to West Tennessee Street (East Side)	Sidewalk	\$400,000
South Copeland Street	St. Augustine Street to Madison Street	Sidewalk	\$80,000
TOTAL			\$101,437,238

Complete Sidewalk Network

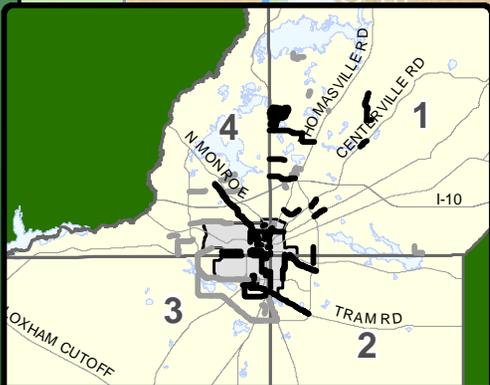
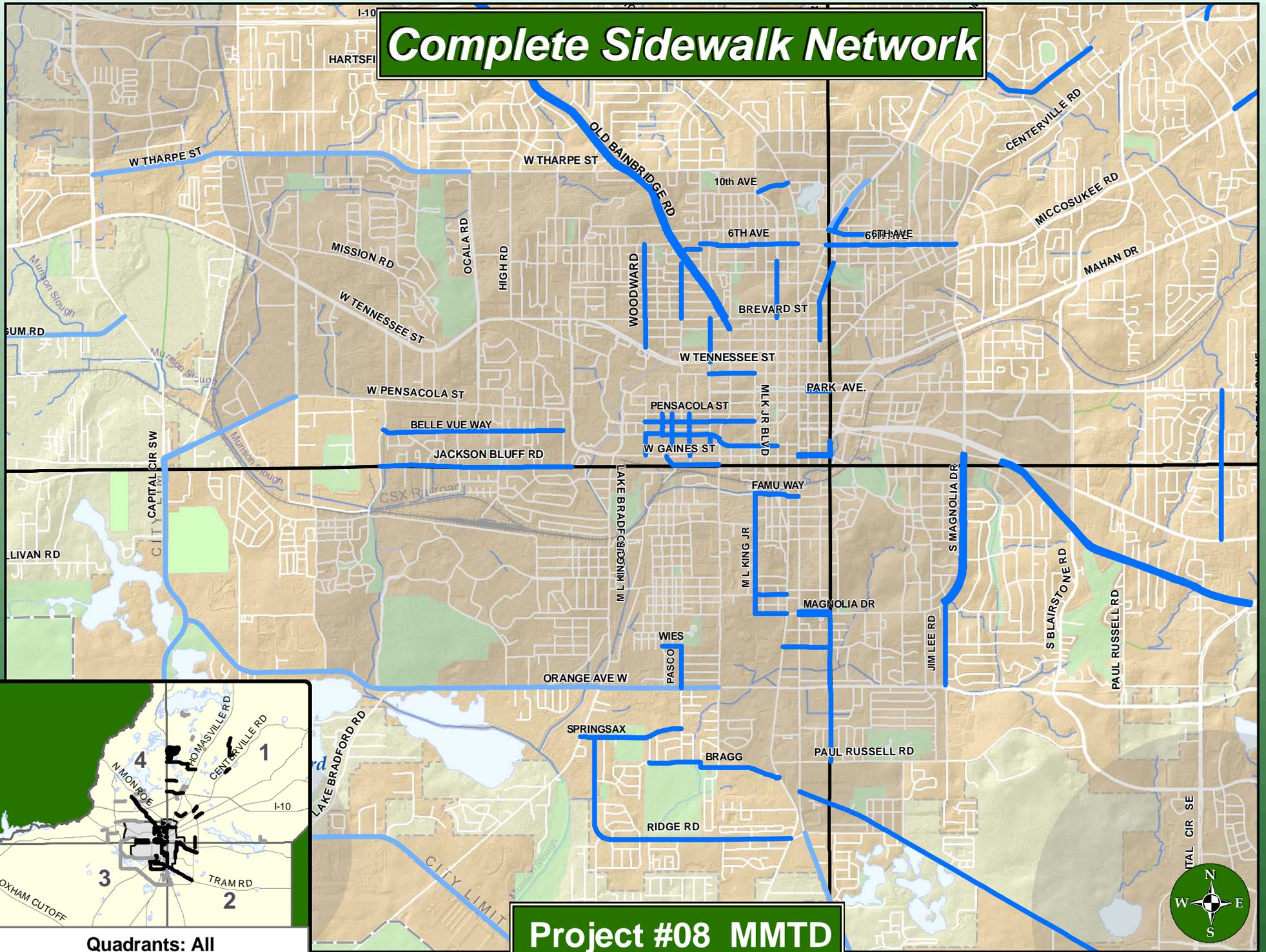


Quadrants: All

Project #08 NE



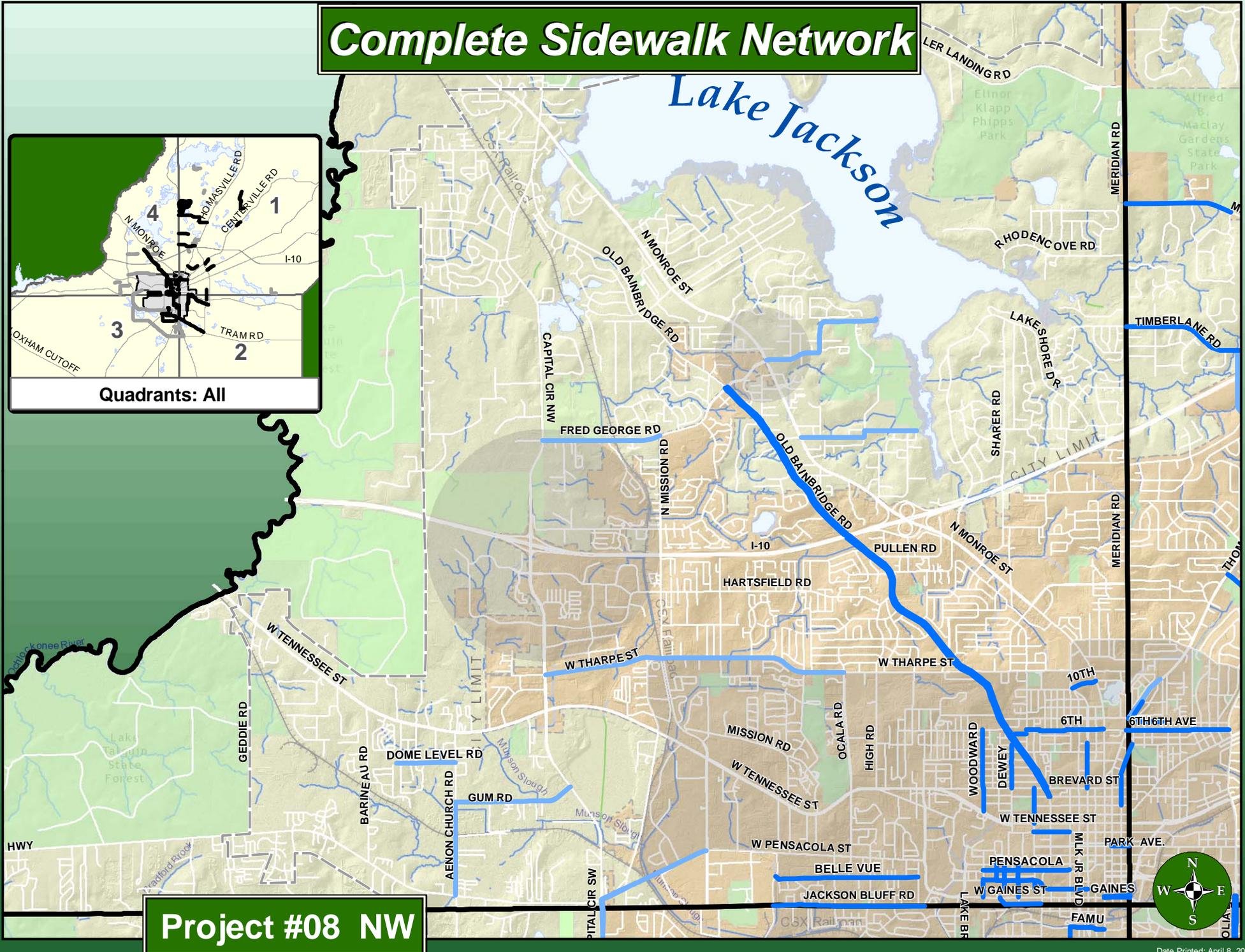
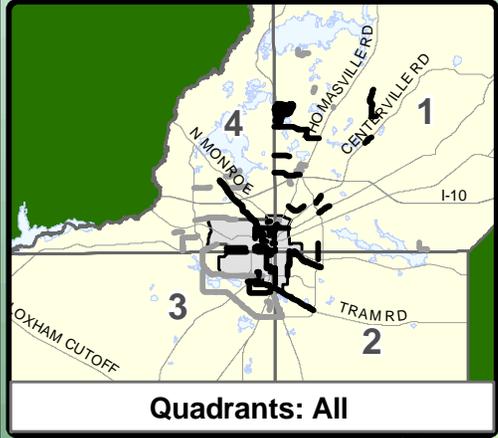
Complete Sidewalk Network



Quadrants: All

Project #08 MMTD

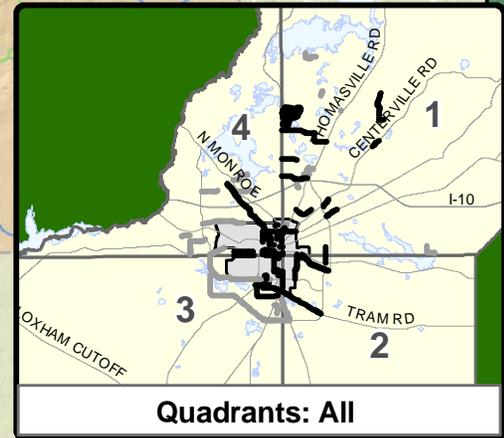
Complete Sidewalk Network



Complete Sidewalk Network



Project #08 SE



Previous Project Information for:

Project #8

Complete Sidewalk Network

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Additional Sidewalks and Mobility Improvements (*Previously Project #42*)

Connecting the Community: Complete the Sidewalk Network
(*Previously Project #16*)

Sidewalks: Primary vs. Secondary (*Previously Project #41*)

Ox Bottom Manor Sidewalks (*Previously Project #58*)

Florida State University – Pedestrian Mobility Enhancements
(*Previously Project #69*)

Proposed Projects for the Sales Tax Extension

Project #12: Additional Sidewalks and Mobility Improvements

Estimated Project Cost: \$46.9 million

Project Criteria:

- | | |
|--|--|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input type="checkbox"/> Water Quality Enhancements and Green Infrastructure |
| <input checked="" type="checkbox"/> Regional Mobility | <input type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input checked="" type="checkbox"/> Greenway Master Plan | <input type="checkbox"/> Transportation Capacity Improvements |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input checked="" type="checkbox"/> Complements BP2000 Project | |

Project Description:

This project is for the construction of sidewalks to enhance pedestrian mobility by providing pedestrian linkages along county roadways. The sidewalks in this project are in addition to any sidewalk identified in the other projects.

Project Criteria Detail:

Comprehensive Plan: This project supports the Mobility elements of the Comprehensive Plan. The areas that the project complies with are listed below.

- Policy 1.2.8: Provide a safe, accessible environment and support active living for students by: developing and maintaining programs to increase biking and walking to schools; prioritizing sidewalk and bicycle infrastructure within a two mile radius of primary schools; and continuing to identify, fund and build Safe Routes to Schools projects.
- Objective 1.3 Connectivity and Access Management: Reduce vehicle trip demand, increase access and safety for cyclists and pedestrians, and preserve the integrity of the transportation system with effective connectivity and access management programs.

Regional Mobility Plan: Sidewalks are a major component of the regional mobility plan which promotes multiple modes of transportation. Pedestrian links provides interconnectivity between residential and commercial areas thereby providing non-vehicle focused safe transportation alternatives.

Greenway Master Plan: Some sidewalk segments can provide safe connectors to the existing trail and greenway networks.

Connectivity: Constructing sidewalks may help provide safe routes to schools as well as pedestrian access to parks and other greenspace area. The proposed sidewalks also may connect neighborhoods to other local amenities such as libraries, parks, commercial areas, and other neighborhoods.

Complements Blueprint 2000 Project(s): The proposed sidewalks may complement several Blueprint 2000 Projects.

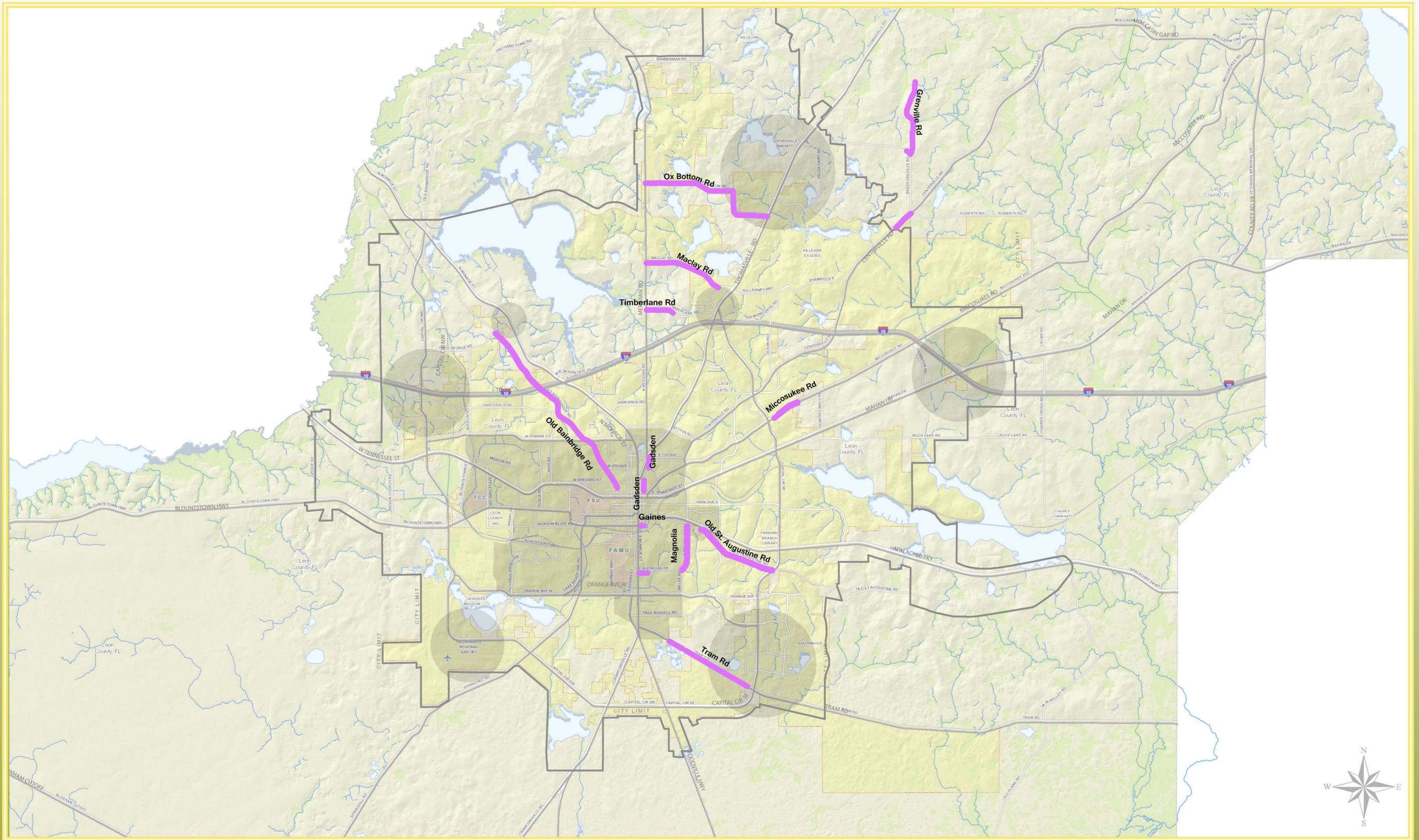
Water Quality Enhancements and Green Infrastructure: N/A

Stormwater/Sewer Capacity Improvements: N/A

Transportation Capacity Improvements: N/A

Core Infrastructure: As a major component of the regional mobility plan, sidewalks are considered a core infrastructure project which promotes multiple modes of transportation. Pedestrian links provide interconnectivity between residential and commercial areas thereby providing non-vehicle focused safe transportation alternatives.

Proposed Sidewalk and Mobility Improvements		
Location	Length	Cost
Centerville - Pimlico to Roberts Rd	2652 Ft. of sidewalk	950,000
Maclay - Meridian Rd to City Limits	5400 Ft. of sidewalk	1,980,000
Miccosukee - Capital Circle to Fleischman	5150 Ft. of sidewalk	1,810,000
Miccosukee - Capital Circle to Fleischman	5150 Ft. of sidewalk	1,810,000
Ox Bottom RD - Meridian Rd to Thomasville Rd	17607 Ft. of sidewalk	3,100,000
Timberlane RD - Meridian Rd to Thomasville Rd	3360 Ft. of sidewalk	850,000
Tram RD - S Monroe to Capital Circle	10003 Ft. of sidewalk	2,550,000
Magnolia - Hokolin Nene to Toochin Nene	1361 Ft. of sidewalk on east side	500,000
Magnolia -Hokolin Nene to Toochni Nene	1361 Ft. of sidewalk on west side	500,000
Magnolia - Jim Lee to Hokolin Nene	1750 Ft. of sidewalk on east side	620,000
Magnolia - Jim Lee to Hokolin Nene	1750 Ft. of sidewalk on west side	620,000
Magnolia - Meridian to Monroe	1094 Ft. of sidewalk on north side	400,000
Magnolia - Meridian to Monroe	1094 Ft. of sidewalk on south side	400,000
Magnolia - Toochin Nene to Lafayette	2413 Ft. of sidewalk on east side	850,000
Magnolia - Toochin Nene to Lafayette	2413 Ft. of sidewalk on west side	850,000
Old St. Augustine - Blair Stone to Indian Head	3476 Ft. of sidewalk on north side	1,220,000
Old St. Augustine - Blair Stone to Indian Head	3476 Ft. of sidewalk on south side	1,220,000
Old St. Augustine - Midyette to Paul Russell	2934 Ft. of sidewalk on north side	1,100,000
Old St. Augustine - Midyette to Paul Russell	2934 Ft. of sidewalk on south side	1,100,000
Old St. Augustine - Midyette to Capital Circle	1850 Ft. of sidewalk on the north side	650,000
Old St. Augustine - Midyette to Capital Circle	1850 Ft. of sidewalk on the south side	650,000
Old St. Augustine - Paul Russell to Blair Stone	2231 Ft. of sidewalk on north side	800,000
Old St. Augustine - Paul Russell to Blair Stone	2231 Ft. of sidewalk on south side	800,000
Centerville - Buford to Fleischmann	3101 Ft. of sidewalk on east side	800,000
Gadsden - Carolina St. to McDaniel	1340 Ft. of sidewalk on east side	470,000
Gadsden - Ingleside to Seventh Ave.	1107 Ft. of sidewalk on east side	390,000
Gadsden - McDaniel to Ingleside	1515 Ft. of sidewalk on east side	530,000
Gadsden - Seventh to Ninth	943 Ft. of sidewalk on east side	330,000
Gaines - Gadsden to Monroe	743 Ft. of sidewalk on north side	200,000
Gaines - Gadsden to Monroe	743 Ft. of sidewalk on south side	100,000
Gaines - Lafayette to Gadsden	385 Ft. of sidewalk on north side	100,000
Gaines - Lafayette to Gadsden	385 Ft. of sidewalk on south side	100,000
Grenville Road - Pisgah Church Rd to Proctor Rd	9150 Ft. of sidewalk on west side	3,300,000
Old Bainbridge - Brevard St. to Georgia	433 Ft. of sidewalk on east side	225,000
Old Bainbridge - Brevard St. to Georgia	433 Ft. of sidewalk on west side	225,000
Old Bainbridge - Brevard to I-10	12765 Ft. of sidewalk on east side	4,470,000
Old Bainbridge - Brevard to I-10	11575 Ft. of sidewalk on west side	4,100,000
Old Bainbridge - I-10 to Fred George	8465 Ft. of sidewalk on east side	3,000,000
Old Bainbridge - I-10 to Fred George	8465 Ft. of sidewalk on west side	3,000,000
Pisgah Church Road - west end of existing trail to Grenville Rd	560 Ft. of sidewalk on north side	230,000
Sidewalks Total		\$ 46,900,000



Additional Sidewalks and Mobility Improvements

Legend

- Sidewalk & Mobility Improvements
- Mobility District/Activity Nodes
- Urban Service Area
- Major Lakes
- Parks
- City Limits

Sidewalks and Mobility Improvements Project Criteria:

- | | |
|--|---|
| Comprehensive Plan | Water Quality Enhancements and Green Infrastructure |
| Regional Mobility | Stormwater/Sewer Capacity Improvements |
| Greenway Master Plan | Transportation Capacity Improvements |
| Connectivity | Core Infrastructure |
| Complements BP2000 Project | |



This product has been compiled from the most accurate source data from Leon County and the City of Tallahassee. However, this product is for reference purposes only and is not to be construed as a legal document or survey instrument. Any reliance on the information contained herein is at the user's own risk. Leon County and the City of Tallahassee assume no responsibility for any use of the information contained herein or any loss resulting therefrom.

Program
3c

Connecting the Community: Complete the Sidewalk Network

Estimated Cost:
\$36.5 million

WHAT IS IT?

The sidewalk network is the first tier in our multimodal system. Its role is to safely and conveniently connect individual residences to schools, shopping and recreation, as well as to the collector and arterial system where transit is provided. The estimated cost to complete the most critical pieces of the system is \$36.5 million.

Evaluation Criteria:

Provides Regional Impact / Geographic Equity ✓
Enhances Mobility ✓
Supports Recreation and Quality of Life ✓

Benefits the Environment ✓
Supports Economic Development / Revitalization ✓
On an Existing Master Plan or Blueprint List ✓
Leverages Other Funds ✓

A recently installed sidewalk on Maclay Boulevard connects commercial areas and neighborhoods as well as provides safe access to a major StarMetro stop in front of Premier Fitness.



A new sidewalk being built in Midtown on 5th Avenue to connect the commercial areas to Lafayette Park and neighborhoods.

WHY IS IT IMPORTANT?

Benefits the Environment – A safe, attractive sidewalk system makes walking more desirable as an alternative to driving. Reduced use of automobiles results in less fuel usage, lower emissions, and better air quality.

Provides Regional Impact / Geographic Equity – Sidewalk projects are planned throughout the community, connecting neighborhoods to schools, local attractions, employment centers and transit service.

Enhances Mobility – A complete and interconnected sidewalk network is an integral component of the local transportation network. Sidewalks allow residents and visitors to safely access businesses, neighborhoods, schools and transit service. Sidewalks are especially important for the large number of residents who do not or cannot drive, and visitors rely on sidewalks to assist in navigating our local attractions.

Supports Recreation and Quality of Life – A complete sidewalk system improves quality of life by creating a safer means of transportation, access to schools, shopping, recreation and transit stops.

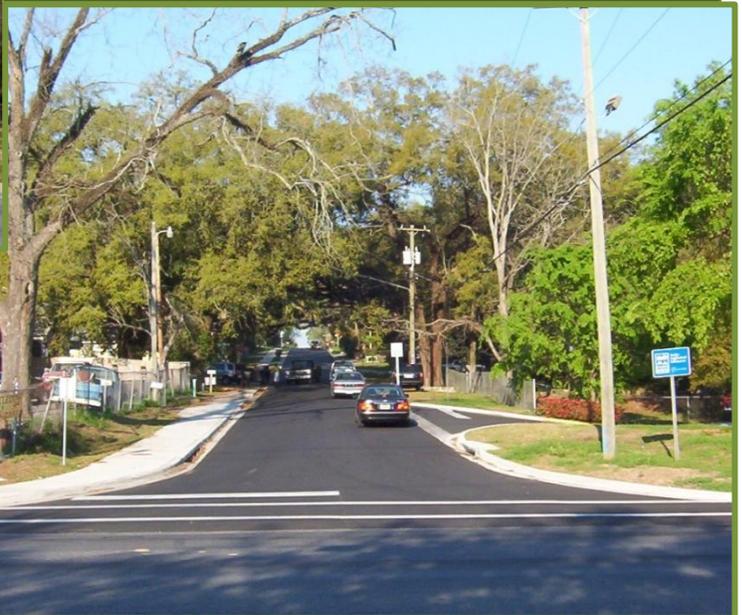
Supports Economic Development / Revitalization – Certain segments will provide safe pedestrian access to businesses in commercial activity centers, such as the Market District or South Monroe Street.

On an Existing Master Plan or Blueprint List – The proposed sidewalk projects are included in Prioritized List of Multimodal Projects, which was adopted by the City Commission in 2010.

Leverages Other Funds – Currently, the City of Tallahassee Commission Policy 600 requires developers to construct sidewalks as part of site development. This investment in our community infrastructure will be enhanced by our local contribution to connect these facilities.



Here you can see how the added sidewalk provides a safer walk from the Providence Neighborhood to the bus stop at McCaskill and Lake Bradford.



Numerous sections of sidewalk are still needed to safely connect potential StarMetro passengers to stops, and to connect neighborhoods to schools, shopping, and parks.

Complete the Sidewalk Network

The following projects (next page) are based on the City's Multimodal Project Prioritization List that was adopted in early 2010. The list was developed to combine various sidewalk lists into one with a unified system for ranking projects against one another.

Those projects that would resolve documented safety issues, provide access to schools, or provide access to StarMetro stops were identified as Top Tier projects and are the basis for the following proposed list. Projects shown in gray will be part of the overall network but alternative funding has been identified for them and they are therefore not included in the total cost.

Several large projects on the list are identified as part of the Pedestrian Access and Safety (PASS) Program. In some cases, adding a sidewalk is more complicated than simply building them at the edge of an existing road. Ditches, utilities, or other impediments may require addition measures.

PASS projects typically involve the reconstruction of the road to add curb and gutter, sidewalks, and bike lanes. They may include piping ditches and relocating utility poles so that the public right-of-way can then be used for sidewalks and bicycle facilities. PASS projects may also include intersection improvements.



Pedestrian Access & Safety (PASS) Program projects often include rebuilding the road to enclose ditches, relocate utilities, add curb, gutter, sidewalks and bicycle facilities.



Proposed Pedestrian Enhancements

Project Location	Project Limits	Project Description	On Safe Routes to Schools List											Estimated Cost	Other Funding	Funding Source			
			Supports StarMetro Decentralization	Documented History of Accidents	1. Creates new access	2a. Access to StarMetro	2b. Access to StarMetro transfer stations	3. Direct access to StarMetro transit stop(s) mile radius	4a. Direct connection to schools within quarter-mile radius	4b. Expansion/direct connection to existing Greenways or trail system	5. Within 1/4 mile of housing	6. Within 1/4 mile of transp. disadvantaged	7. Access to University and College Campuses				8. Within 1/4 mile of shopping centers	9. Access to Identified Placemaking Area	
Armistead Rd	Thomasville Rd to Woodgate Way	Sidewalks/ Bicycle Lanes*		X	x	x		x	x								\$7,417,800		
Richview Rd	Apalachee Pkwy to Park Ave	Sidewalks/ Bicycle Lanes*		X	x	x	x	x						x			\$6,698,250		
Jim Lee Road	Magnolia Dr to Orange Ave	Sidewalk		X		x	x		x					x			\$538,000		
Springsax Rd	Springhill Rd to Pasco St	Sidewalks/ Bicycle Lanes*		X	x	x		x	x	x							\$4,625,400		
Bragg Dr	Parkridge Dr to South Adams St	Sidewalk		X		x	x	x		x	x						\$587,800		
Tanner Dr	Galimore Dr to Parkridge Dr	Sidewalk		X		x	x		x	x							\$318,000		
Ridge Rd	Springsax Rd to Crawfordville Hwy	Sidewalk		X		x	x			x								\$2,006,600	City of Tallahassee
Woodward Ave	St. Augustine St to West Gaines St	Sidewalk		X	X	x	x	x						x	x		\$125,800		
Sixth Ave East	Thomasville Rd to North Magnolia Dr	Sidewalk		X	X	x	x	x	x					x			\$850,600		
Sixth Ave West	North Monroe St to Old Bainbridge Rd	Sidewalk	X		X	x	x	x						x	x		\$193,600		
Jackson Bluff Road	Mabry to Lake Bradford	Sidewalk			X	x	x	x	x	x				x	x	x	\$1,660,000		
Central 10 th Avenue	@ Duval Street to North Monroe Street at Legion Street	Shared-Use Path			X	x	x		x	x				x	x	x	\$125,000		Regional Mobility Plan Tier 1 (2016 - 2020) Project #10
FAMU area	FAMU Way/Oakland Avenue to S. Adams Pedestrian Supp. Int.: Meridian @ Van Buren Oakland @ Monroe Jennings @ Bronough Palmer @ Meridian	Sidewalks; Ped Supportive Intersections			X	x	x	x	x					x	x		\$734,000		
Seventh Ave East	Thomasville Rd to Magnolia Drive	Sidewalk		X		x	x	x	x					x	x		\$1,200,000		Regional Mobility Plan Tier 1 (2016 - 2020) Project #11
Palmetto Street	MLK Jr. Boulevard to South Adams Street	Sidewalks			X	x	x							x	x	x	\$1,067,000		Regional Mobility Plan Tier 1 (2016 - 2020) Project #24
Stone Valley Way	Gaines St to Railroad Ave	Sidewalk/ Bicycle Blvd			X	x	x	x	x	x				x	x	x	\$300,000		
Meridian St	Magnolia St to Paul Russell Rd	Sidewalks/ Bicycle Lanes*			X	x	x	x						x	x		\$2,294,788		
Pasco St	Wies St to Orange Ave	Sidewalk			X		x	x	x	x				x			\$294,800		Regional Mobility Plan Tier 1 (2016 - 2020) Project #25
Putnam Dr	South Adams St to South Meridian St	Sidewalk			X	x	x							x	x	x	\$607,600		
East Park Ave	South Calhoun St to South Meridian St	Sidewalk			X		x	x	x	x				x			\$141,200		
MLK Jr. Blvd	FAMU Way to Palmetto St	Sidewalks			X	x	x		x					x	x		\$5,336,500		Regional Mobility Plan Tier 3 (2026 - 2030) Project #106
North Duval St	Fifth Ave West to West Brevard St	Sidewalk			X		x	x	x	x				x			\$383,200		
Dewey St	Rollins St to Brevard St	Sidewalk			X	x	x		x					x	x	x	\$331,600		
Wies St	Holton St to Pasco St	Sidewalk			X		x	x		x				x			\$119,400		Regional Mobility Plan Tier 1 (2016 - 2020) Project #30
Osceola St	MLK JR Blvd to South Adams St	Sidewalk			X	x	x							x	x		\$203,000		
South Belle Vue Way	Mabry Street to Hayden Road	Sidewalks/ Bicycle Lanes*			X	x	x		x					x	x	x	\$8,424,000		
TOTALS																	\$36,434,638	\$10,149,300	

* Pedestrian Access and Safety Program (PASS)

Proposed Sales Tax Projects by the Community

Project Name: Ox Bottom Manor Sidewalks (Project #58)

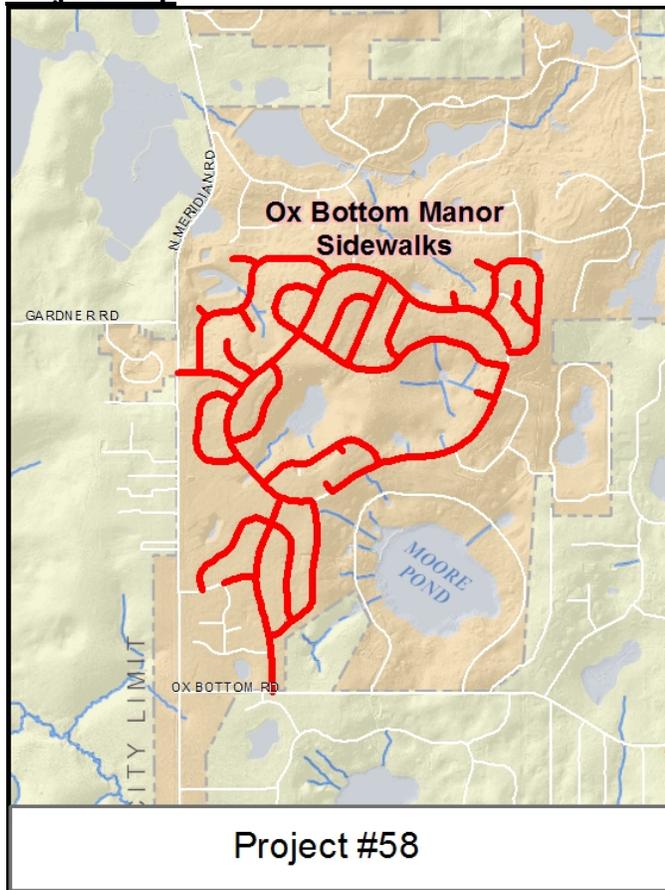
Project Themes:

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

This project involves the construction of sidewalks on Ox Bottom Road, previously identified in the Leon County Proposed Project #12 “Additional Sidewalks and Mobility Improvements.”

Project Map:



Proposed Sales Tax Projects by the Community

Project Name: Florida State University – Pedestrian Mobility Enhancements (Project #69)

Project Themes:

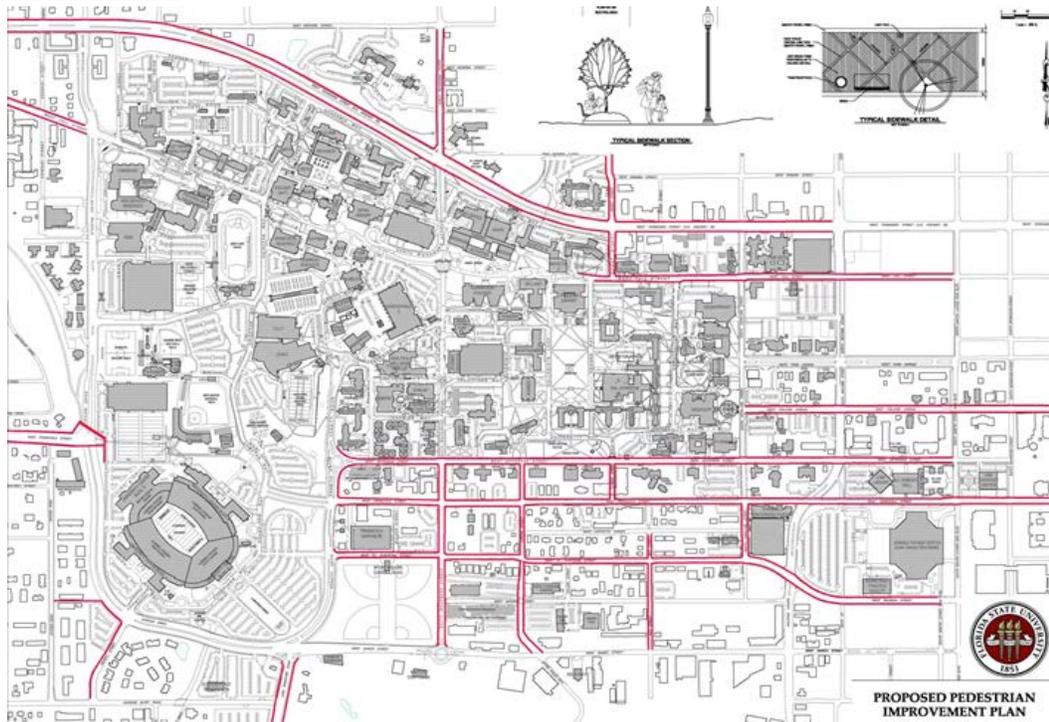
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|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

On March 14, 2013, the Florida State University presented several projects for the consideration of the Sales Tax Committee. This project is for the construction of 16 sidewalk segments and would include landscaping, park benches, and lighting. The project description for this project was developed using the materials by FSU during their presentation to the Committee. The University is requesting funding in the amount of \$4.5 million to provide for a safer, more effective way of encouraging pedestrian movements to and around the Main Campus. The University is prepared to partner with the City, where it is able to do so, by providing easements to facilitate the development of these proposed sidewalk networks. The 16 proposed sidewalk segments are as follows:

1. West Tennessee Street (Stadium Drive to Macomb Street)
2. West Call Street (Tennessee Street to Stadium Drive)
3. West Call Street (Dewey Street to Duval Street)
4. West Pensacola Street (Ocala Road to Stadium Drive)
5. Pensacola Street (Varsity Drive to Duval Street)
6. Hayden Road to Stadium Drive
7. Lake Bradford Road (Levy Street to Gaines Street)
8. St. Augustine Street (Varsity Drive to Martin Luther King Boulevard)
9. Jefferson Street (Varsity Drive to Duval Street)
10. West College Avenue (Copeland Street to Duval Street)
11. South Woodward (Gaines Street to Jefferson Street)
12. Lorene Street (Gaines Street to Jefferson Street)
13. Gay Street (Gaines Street to West Lafayette/Jefferson Street)
14. South Copeland Street (St. Augustine Street to Jefferson Street)
15. Dewey Street (Call Street to Virginia Street)
16. North Woodward Avenue (Tennessee Street to Brevard Street)

Project Map:



SALES TAX PROJECTS

Project Number: 9
Project Name: Greenways Master Plan
Total Project Cost: \$18,497,000

Total Project Cost Notes

Executive Project Summary

The Greenways Master Plan goal is two-fold; first, to protect sensitive lands within Leon County, and second, to provide recreational access to these lands. The Master Plan already provides some of the most beautiful and treasured recreational opportunities in the area, and is already a tourism destination for some. In fact, the City is starting to have the nickname "Trailhassee." This project completes the remaining Greenways projects and interconnects them via the Bike Route system for a complete Countywide system.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

The greenways included in this project are outlined in attachment #1.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
\$18,497,000	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

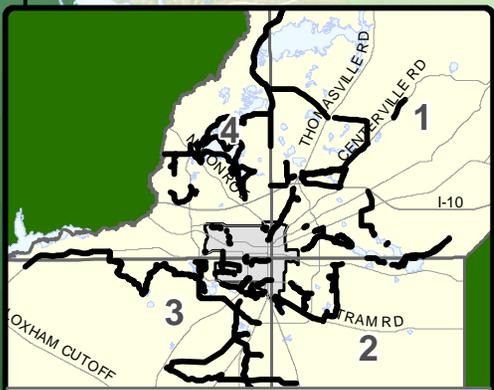
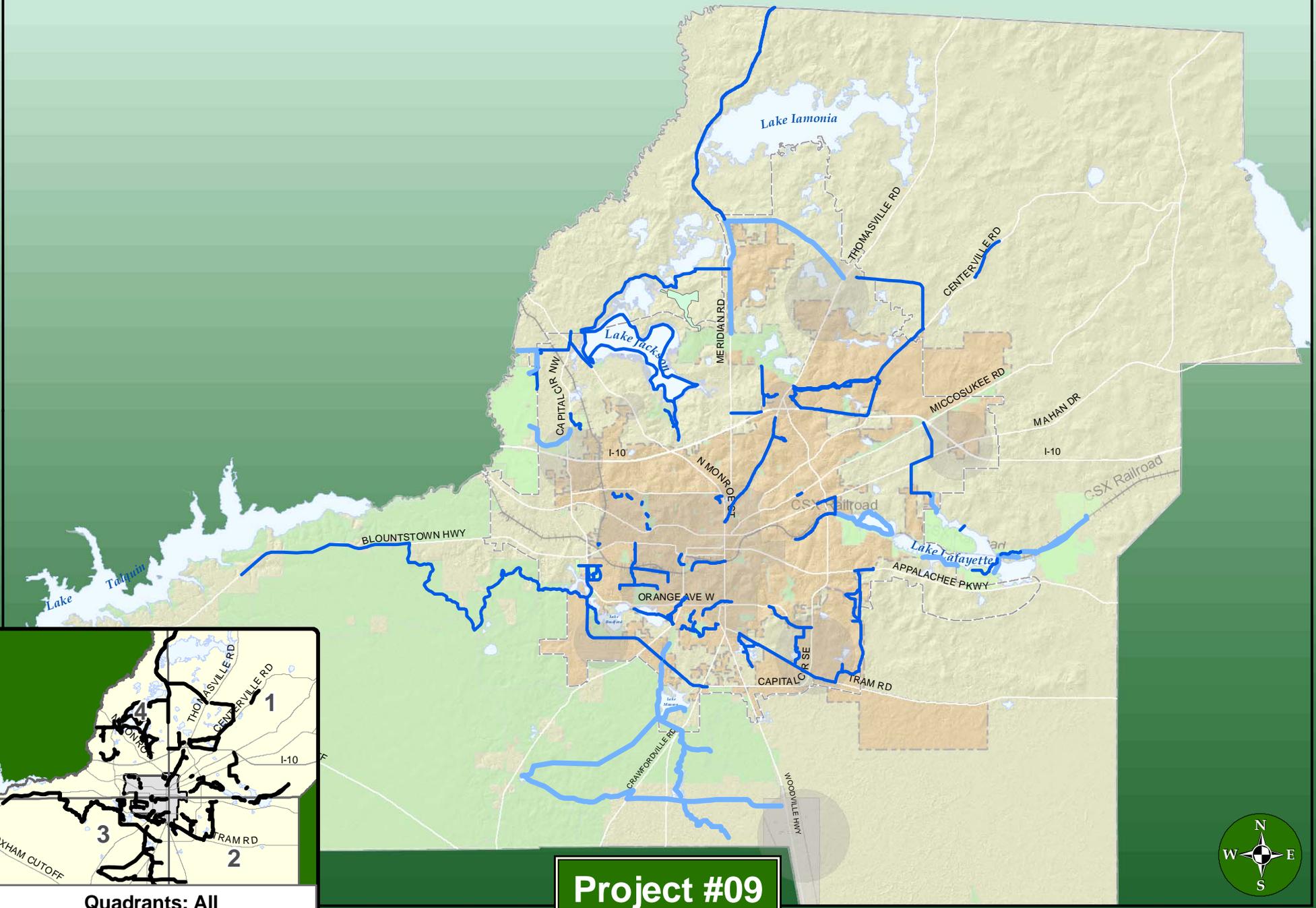
Proposed Greenways

GREENWAY PROJECT	MILES	IMPROVEMENTS	EST. COST*
Dr. Charles Billings Greenway	2	Paved trail, designation of bike routes, signage, and three trail crossings	\$730,800
Southwood Greenway	6	Paved trail, signage, and three trail crossings (Alignment "B")	\$1,080,200
Buck Lake Greenway	4.8	Paved trail, signage, and three trail crossings	\$1,735,200
Centerville Greenway	4.9	Paved and crushed rock trail and signage	\$1,484,400
I-10 Greenway	3.9	Paved trail, signage, and three trail crossings	\$1,528,800
Killlearn Greenway	1.3	Paved trail, signage	\$417,000
St. Marks Connector Trail	0.3	Paved trail, trailhead, and signage	\$322,800
San Luis Greenway	1.9	Six trail segments, designation of several existing streets as bike routes, signage, and pedestrian bridge	\$553,800
Lake Ella Connector Trail	0.2	Paved trail, trail crossing, and signage	\$169,200
Tram Road Greenway	3.3	Paved trail, trail crossing, and signage	\$997,200
Bradford Brook Chain of Lakes Greenway	10.7	Stabilized surface trail, seven pedestrian bridges, two trailheads, signage	\$1,201,200
Fern Trail	1.6	Stabilized natural surface trail, signage, boardwalk under CapCirNE adjacent to CSX RR line	\$126,720
Goose Pond Trail	1.9	Paved trail, three bridges, signage, and 11 trail crossings	\$568,080
Campbell Pond Greenway	1.7	Crushed rock trail, pedestrian bridge, and signage	\$1,774,800
Black Swamp Greenway	1.7	Stabilized surface trail, one bridge, signage	\$223,200
Southwest Sector Greenway	3.3	Paved trail, trail crossing, signage	\$994,800
Meridian Greenway	11.8	Crushed rock trail, two boardwalks, six trail crossings, signage	\$1,588,800
Phipps Greenway	6.3	Crushed rock trail, signage	\$756,000
Ft. Braden Greenway	5.5	Crushed rock trail, signage	\$660,000
Thomasville Road Greenway	3.7	Paved trail, signage, and 38 trail crossings	\$979,200
FAMU Greenway	1.2	Paved trail, signage, and two trail crossings	\$604,800
	78	TOTAL	\$18,497,000

Greenways Included in Other Sales Tax Projects

Bannerman Trail	4.8	Paved trail, 14 trail crossings, and signage
Lake Jackson Greenway (Okeeheepkee Trail)	0.7	Crushed rock trail, trailhead, and signage
Northwest Greenway	6.4	Crushed rock trail, six trail crossings, and signage
University Greenway	5.9	Four paved trail segments, designation of several existing streets as bike routes, two pedestrian bridges, 16 crossings, signage
Pine Flats Greenway	24.2	Paved, crushed rock, and stabilized soil trails, two pedestrian signals, five boardwalks, six trail crossings, exotic invasive plant control, signage
Lafayette Greenway	9.8	Crushed rock and stabilized natural surface trails, three engineered boardwalks, two bike/ped bridges, three trail crossings, signage
Timberlane Greenway	2.8	Crushed rock and stabilized natural surface trails, two bridges, two trail crossings, signage, and trail easements
Capital Circle Southwest Greenway	6.6	Paved trail, signage, and four trail crossings

Implement Greenways Master Plan



Quadrants: All

Project #09



Previous Project Information for:

Project #9

Implement Greenways Master Plan

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Connecting the Community: Implement the Greenways Master Plan (*Previously Project #17*)

Greenway Connections Timberlane Ravine to Klapp-Phipps Overstreet Park (*Previously Project #4*)

Meridian Road Greenway (*Previously Project #53*)

Program
3a

Connecting the Community: Implement the Greenways Master Plan

Estimated Cost:
\$16.5 million

WHAT IS IT?

The Greenways Master Plan goal is two-fold; first, to protect sensitive lands within Leon County, and second, to provide recreational access to these lands. The Master Plan already provides some of the most beautiful and treasured recreational opportunities in the area, and is already a tourism destination for some. In fact, the City is starting to have the nickname “Trailhassee.” This project completes the remaining Greenways projects and interconnects them via the Bike Route system for a complete Countywide system.

Evaluation Criteria:

Provides Regional Impact / Geographic Equity ✓
Enhances Mobility ✓
Supports Recreation and Quality of Life ✓

Benefits the Environment ✓
Supports Economic Development / Revitalization ✓
On an Existing Master Plan or Blueprint List ✓
Leverages Other Funds ✓



Our Greenways Master Plan seeks to protect sensitive lands within Leon County & to provide recreational access to these lands.

WHY IS IT IMPORTANT?

Benefits the Environment – Implementation of the Greenways Master Plan would benefit the environment by enhancing and protecting our community’s natural resources.

Provides Regional Impact / Geographic Equity – The proposed greenways system extends throughout all areas of our community and all Leon County citizens benefit from having locally accessible recreational opportunities.

Enhances Mobility – A well-connected system of greenways and trails provides mobility options for meeting personal transportation needs and recreational interests. As part of the multimodal transportation network, greenways and trails serve an important function beyond recreation as the trails surrounding Florida State University and the Downtown area demonstrate. Off-road facilities provide choices for residents, visitors and their families as they travel throughout Leon County.

Supports Recreation and Quality of Life – Planned greenways projects enhance quality of life through conservation and enhancement of the local environment and provision of outdoor recreational opportunities.

Supports Economic Development / Revitalization – Expansion of the greenways network promotes economic development opportunities for the local economy as residents from the Big Bend region visit Leon County to experience “Trailhassee” and the natural attractions created by the Greenways system.

On an Existing Master Plan or Blueprint List – The first Tallahassee-Leon County Greenways Master Plan was adopted by the Commissions in 2004. Several projects are included in the Regional Mobility Plan as well as the Mobility District Plan.

Leverages Other Funds – The Greenways system provides access to properties purchased under the Florida Forever Program, which is a state funded program to buy environmentally sensitive lands. The Florida Forever Program has not been funded by the State in recent years due to budget constraints. However, numerous parcels have been purchased in the past and would benefit from access provided by new Greenways.



The Bicycle Route System Plan is overlaid onto the Greenways System Plan to provide countywide parks & greenways connectivity.

Proposed Dr. Charles Billings Greenway Trail - Improvements

Implement the Greenways Master Plan

The following projects are included in the 2012 Greenways Master Plan update. The Master Plan is a Countywide document, therefore both City and County Greenways are shown.

Greenway Project	Miles	Improvements	Estimate
Projects			
Dr. Charles Billings Greenway	2	Designation of bike routes, paved trail, signage, and three trail crossings	\$525,000
Southwood Greenway	6	Paved trail, signage, and three trail crossings	\$1,506,000
Buck Lake Greenway	4	Paved trail, signage, and three trail crossings	\$1,006,000
Welaunee Arch Greenway	10	Crushed rock trail and signage	\$1,000,000
Centerville Greenway	3.5	Crushed rock trail and signage	\$350,000
I-10 Greenway	2	Paved trail, signage, and three trail crossings	\$756,000
Killearn – Maclay Greenway (Killearney Way)	3	Paved trail and signage	\$750,000
St. Marks Connector Greenway (Gamble to Lake Bradford/Gaines)	0.3	Paved trail, trailhead, and signage	\$100,000
San Luis Greenway	0.3	Designation of several existing streets as bike routes, crushed rock trail, signage, and a pedestrian bridge	\$75,000
Midtown – Lake Ella Connector Trail	0.2	Paved trail, trail crossing, and signage	\$75,000
Tram Road Greenway	2.5	Paved trail, trail crossing, and signage	\$630,000
Bradford Brook Chain of Lakes Greenway	13.5	Crushed rock trail, two trailheads, signage	\$1,400,000
Lake Jackson North Greenway	6.3	Paved trail, signage	\$1,575,000
Fern Trail	1.6	Existing trail (non-paved), signage, boardwalk under CapCirNE adjacent to CSX RR line, crossing at Weems Rd.	\$100,000
Goose Pond Trail (Thomasville Road to Apalachee Parkway)	1.7	Paved trail, signage, and two trail crossings	\$430,000
Karst Ponds Greenway	3	Crushed rock trail, signage, land acquisition	\$1,500,000
Red Hills – Cody Scarp Scenic Byway	100	Designation of bike routes, signage	\$100,000
Colin English Greenway	5	Paved trail, signage	\$1,250,000
Meridian Greenway	13.6	Paved trail, signage	\$3,400,000
	122		\$16,528,000
Greenway included in Capital Circle SW Proposed Sales Tax Extension Project			
Capital Circle SW Greenway	8	Paved trail, signage, and two trail crossings	
Greenways included in Leon County Proposed Sales Tax Extension Projects			
NE Connector Corridor Greenway (Deer Lake to Bull Headley Park)	1	Paved trail and signage	
NE Connector Corridor Greenway (Summerbrooke to Bannerman)	1	Paved trail and signage. May also require some land acquisition.	
NE Connector Corridor Greenway (Bannerman)	4.8	Paved trail, trail crossing, and signage	
Lake Jackson Enhancement Greenway	0.3	Crushed rock trail, trailhead, and signage	
Northwest Connector Corridor Greenway (Ochlockonee Valley Greenway)	5.5	Paved trail, trail crossing, and signage	
Westside Student Corridor Greenway (Education Quadrant Greenways)	1	Designation of several existing streets as bike routes, paved trail, signage	
Pine Flats Trail/Lake Munson Greenway (tie-in to Capital Cascade Greenway)	11.6	Crushed rock trail, six trail crossings, signage	
Apalachee - Lafayette Greenway/Lake Lafayette - St. Marks Linear Regional Park	3.5	3.5 mi. crushed rock trail, 1.4 mi. boardwalk, signage	
Market District Activity Center Connector Greenway (Timberlane Greenway)	2	Paved trail and signage – also requires trail easements	

Proposed Dr. Charles Billings Greenway Land Acquisition for Flood Control, Greenways and Parks



Significant public investment has already been made in the Greenways System through land acquisitions from both local funds, and the Florida Forever Program.

Blueprint 2000

Proposed Sales Tax Projects

Project Name: Greenway Connections Timberlane Ravine to Klapp-Phipps Overstreet Park
(Blueprint 2000 Map 5A)

Project Themes:

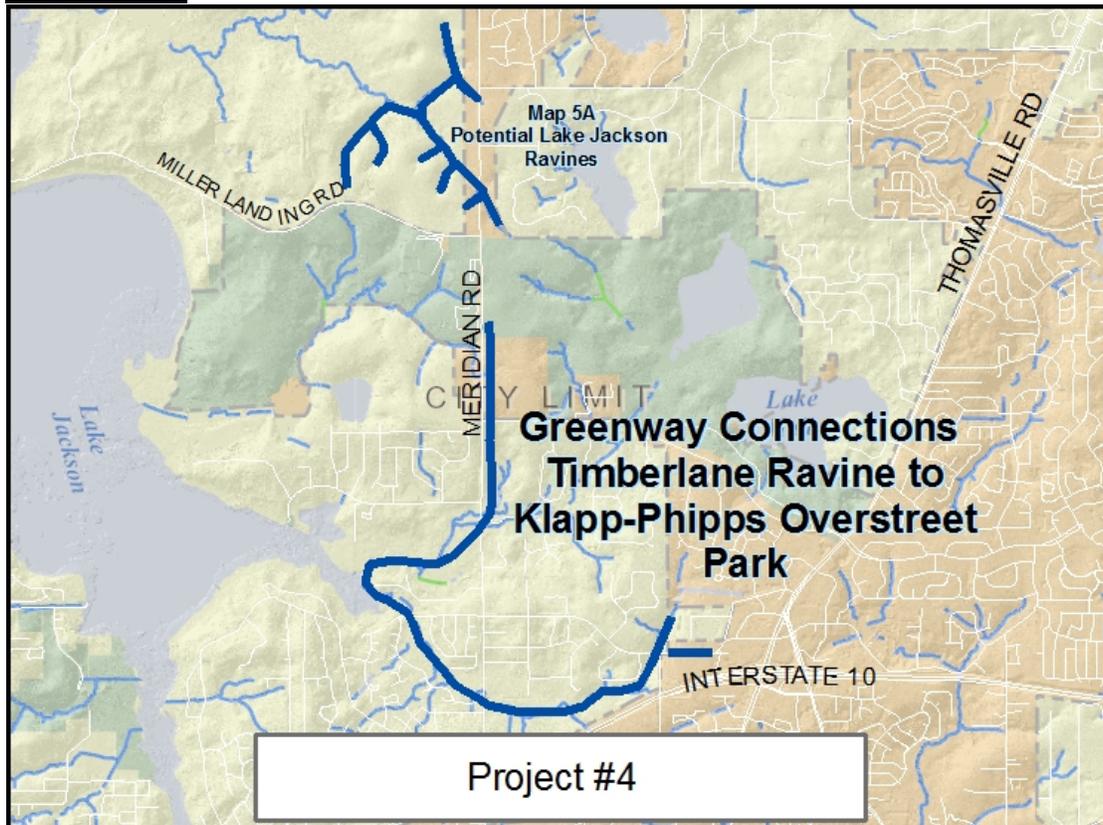
- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

The primary feature of this project is the extension of the regional greenway system which will provide functional bicycle and pedestrian linkages between Bannerman Road and the Timberlane Ravine. Acquisitions of additional ravine systems, greenways and open space associated with Lake Jackson are also proposed, which would enhance the lake's economic and recreational value. Trail crossings will be necessary and should maintain the character of the canopy road. Greenway connections in the area of Meridian Road shall use existing easements, dedications, and public-owned rights-of-way to the greatest extent feasible. However, new public access easements for critical greenway connections may be acquired.

This project also includes bicycle and pedestrian access to the open space portion of the Okeeheepkee Prairie site from North Monroe and the proposed Lake Jackson Greenway which will connect the Prairie to the Lake Jackson Mounds State Park.

Project Map:



Proposed Sales Tax Projects by the Community

Project Name: Meridian Road Greenway (Project #53)

Project Themes:

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

The proposed Meridian Greenway would be a shared use path for bicyclists, hikers, and other non-motorized users. The Greenway is intended to connect the Timberlane Greenway on its southern terminus north to Elinor Klapp-Phipps Park, Alfred B. Maclay Gardens State Park, the planned Orchard Pond Greenway, and continue to the Georgia state line.

This greenway trail will connect many residential areas to these greenways which are currently not accessible except by automobiles and other vehicles. It will run parallel to Meridian Road, which is a protected canopy road in Leon County and cannot be widened. The Northeast Connector Corridor project, proposed by the County, contains a segment of this larger proposed greenway trail.

Project Map:



SALES TAX PROJECTS

Project Number: 10
Project Name: Northwest Connector Corridor
Total Project Cost: \$53.6 million

Total Project Cost Notes

The estimated \$50 million for Tharpe Street construction includes floodplain management and right of way costs.
Trail costs associated with this project are identified in Project #9: Implementing Greenways Master Plan.

Executive Project Summary

Tharpe Street, a significant benefits project, is an existing two lane non-descript roadway and is the key east/west corridor between Tennessee Street and Interstate 10 and has been identified as a significant benefits project. The existing roadway is frequently congested and does not safely support any other modes of transportation except the car. Creating a means of multimodal transportation is crucial for this corridor as it is not only used by commercial and industrial businesses but it also serves a high student population. To date the County has spent approximately \$2.77 million and 5 ½ years to complete the PD&E Study and developed 60% Design Plans for Tharpe Street improvements from Capital Circle to Ocala Road. The remaining design work, permitting, and right of way acquisition will need to be completed before construction can begin on this transportation corridor. The enhancements to the road network are vital to support the full potential for economic activity in this commercial corridor.

In addition, the two streams crossed by Tharpe Street overwhelm the drainage culverts during heavy storms, flooding the adjacent private property. The roadway project also addresses the flooding issues along Tharpe Street and improves stormwater treatment. The pedestrian and bike connections will then extend north along Capital Circle NW to the Talquin State Forest and the new Fred George Greenway, a Blueprint 2000 project, greatly improving access to the County parks on Tower Road and Stoneler Road as well.

Finally, the project develops the Park Place Recreational Area by constructing two ball fields with restroom, concession stand, and playground.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

The Northwest Connector Corridor project proposed improvements include the following:

- Widen Tharpe Street to four lanes from Ocala Road to Capital Circle Northwest with landscaped medians, sidewalks and bike lane. This project supports the Capital Regional Transportation Planning Agency's long range plan of facilitating east west modal movements, enhancing the vibrancy of the western portion of Tallahassee, and promoting multimodal transportation and regional connectivity.
- Construct landscape medians east from Ocala Road to Monroe Street.
- Provide enhancements for public transportation.

- Stormwater upgrades to address flooding issues and provide water quality treatment. This project will provide stormwater upgrades to address flooding experienced by the commercial property at the two major stream crossings.
- Interconnect existing amenities such as Mission San Luis and address connectivity gaps to proposed Park Place Recreational Area, Fred George Park, Stoneler Park and Tower Road Landing. This project provides connectivity between the amenities located on Capital Circle and several within the central core of the city therefore enabling connections to Lake Ella, Martha Wellman Park and the proposed regional activity center of Hopkins Crossing and Park Place. The addition of the multi-use trail south of San Luis Mission Park to Mission San Luis will provide connectivity for high density residential neighborhood to the existing and proposed pedestrian/bike network.

This project adds almost eight miles of additional trail and sidewalk connectivity between Ocala Road and Mission San Luis northwest through the Park Place PUD to the Talquin State Forest and the Fred George Park, and east again toward the Lake Jackson Library. This connectivity will allow regional travel for bicycles and pedestrians and provide an efficient connection between the NE to the NW with interconnection to existing pedestrian and bike networks.

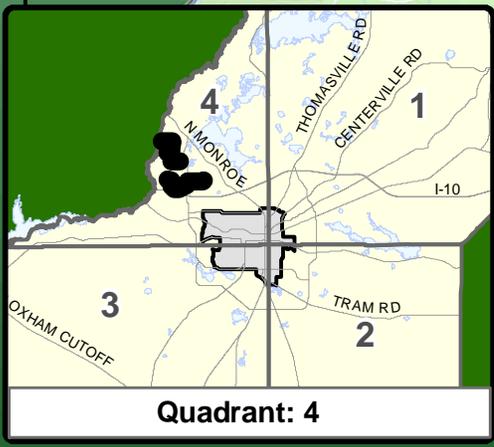
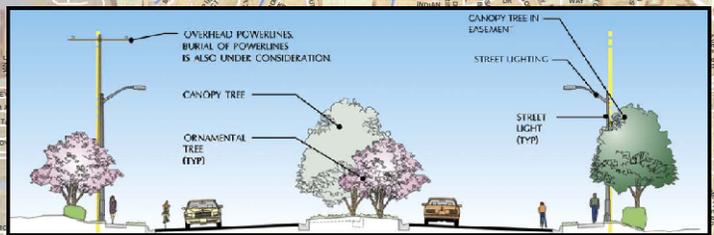
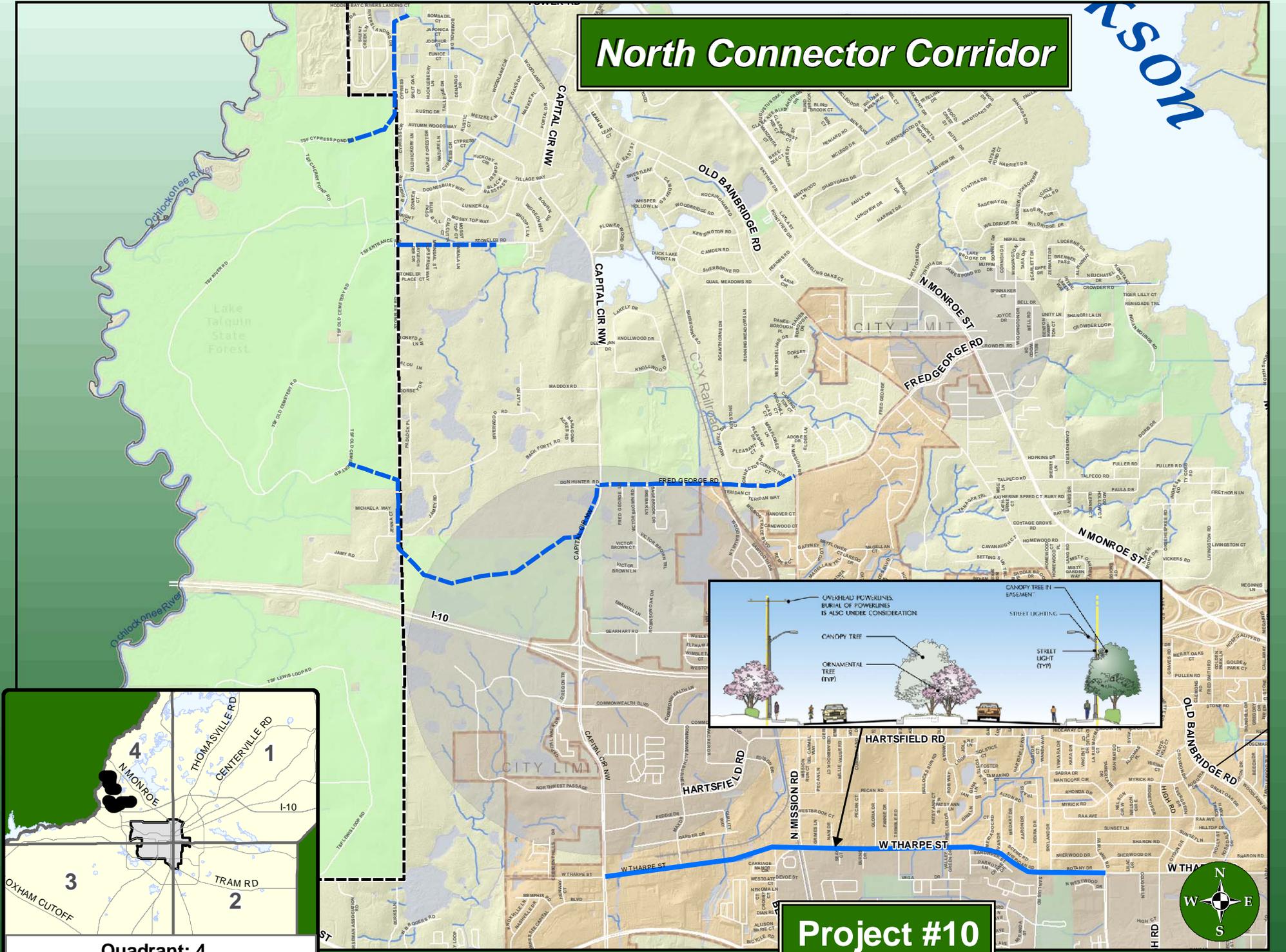
- Develop the Park Place Recreational Area by constructing two ball fields with restroom, concession stand, and playground.
- Construct sidewalks along Fred George Road to connect the Fred George Greenway with the renovated Huntington Oaks Plaza and Lake Jackson Branch Library.

Cost By Themes

\$50 million	Regional Mobility/Transportation
\$0.5 million	Sense of Community
	Environmental/Water Quality
\$1.6 million	Connectivity
	Economic Vitality
\$1.5 million	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

North Connector Corridor

Nelson



Quadrant: 4

Project #10



Previous Project Information for:

Project #10

Northwest Connector Corridor

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Northwest Connector Corridor (*Previously Project #56*)

Proposed Projects for the Sales Tax Extension

Project #3: Northwest Connector Corridor

Estimated Project Cost: \$56.9 million

Project Criteria:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input checked="" type="checkbox"/> Water Quality Enhancements & Green Infrastructure |
| <input checked="" type="checkbox"/> Regional Mobility | <input checked="" type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input checked="" type="checkbox"/> Greenway Master Plan | <input checked="" type="checkbox"/> Transportation Capacity Improvements |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input checked="" type="checkbox"/> Complements BP2000 Project | |

Project Description:

The Northwest Connector Corridor project encompasses a holistic approach to transportation capacity improvements, regional mobility, connectivity to existing/proposed amenities (including Blueprint 2000 projects), and increases stormwater capacity to address flooding concerns. Tharpe Street is an existing two lane non-descript roadway and is the key east/west corridor between Tennessee Street and Interstate 10 and has been identified as a significant benefits project. The existing roadway is frequently congested and does not safely support any other modes of transportation except the car. Creating a means of multimodal transportation is crucial for this corridor as it is not only used by commercial and industrial businesses but it also serves a high student population. To date the County has spent approximately \$2.77 million and 5 ½ years to complete the PD&E Study and developed 60% Design Plans for Tharpe Street improvements from Capital Circle to Ocala Road. The remaining design work, permitting, and right of way acquisition will need to be completed before construction can begin on this transportation corridor. The enhancements to the road network are vital to support the full potential for economic activity in this commercial corridor. In addition, the two streams crossed by Tharpe Street overwhelm the drainage culverts during heavy storms, flooding the adjacent private property. The roadway project also addresses the flooding issues along Tharpe Street and improves stormwater treatment. The pedestrian and bike connections will then extend north along Capital Circle NW to the Talquin State Forest and the new Fred George Greenway, a Blueprint 2000 project, greatly improving access to the County parks on Tower Road and Stoneler Road as well. Finally, the project develops the Park Place Recreational Area by constructing two ball fields with restroom, concession stand, and playground.

The Northwest Connector Corridor project proposed improvements include the following:

- Widen Tharpe Street to four lanes from Ocala Road to Capital Circle Northwest with landscaped medians;
- Construct sidewalks and bike lanes;
- Enhancements for public transportation;
- Stormwater upgrades to address flooding issues and provide water quality treatment;
- Interconnect existing amenities such as Mission San Luis and address connectivity gaps to proposed Park Place Recreational Area, Fred George Park, Stoneler Park and Tower Road Landing;
- Develop the Park Place Recreational Area by constructing two ball fields with restroom, concession stand, and playground.
- Construct sidewalks along Fred George Road to connect the Fred George Greenway with the renovated Huntington Oaks Plaza and Lake Jackson Branch Library.

Project #3: Northwest Connector Corridor

Page 2

Project Criteria Detail:

Comprehensive Plan: This project supports the Stormwater Management, Mobility, and Parks and Recreation elements of the Comprehensive Plan. The areas that the project complies with are listed below.

- Stormwater Management Goal 1: Provides a stormwater management system which protects the health, welfare and safety of the general public by reducing damage and inconvenience from flooding and protects surface water and groundwater quality.
- Mobility Element
 - Policy 1.1.6: A functional transportation network shall be coordinated and maintained with the Florida State University, Florida A&M University, and Tallahassee Community College master plans to link those educational institutions and provide access to transit and surrounding supporting land uses.
 - Policy 1.2.2: Safe and convenient facilities for pedestrians, cyclists and transit users shall be evaluated for all new road and road widening projects. Specifically, all road projects, including resurfacing projects, shall be evaluated for the addition of bicycle lanes or paved shoulders, and transit shelters where they did not previously exist.
 - Policy 1.2.3: Establish and maintain a safe and effective system of bicycle lanes, sidewalks, and shared-use paths in conjunction with existing and planned roadways and the Greenways Master Plan. Where design criteria allow and safe operation will occur, separate bicycle and pedestrian traffic from vehicular traffic. Provide adequate and secure bicycle parking facilities at major destinations.
 - Policy 1.2.7: Require that all new or rebuilt multi-lane (four or six-lane) arterial and major collector streets be constructed with grassed and/or landscaped medians where sufficient right-of-way can be obtained, unless limited by environmental constraints.
 - Policy 1.2.8: Provides a safe, accessible environment and support active living for students by: developing and maintaining programs to increase biking and walking to schools; prioritizing sidewalk and bicycle infrastructure within a two mile radius of primary schools; and continuing to identify, fund and build safe routes to schools projects.
 - Policy 1.2.9: Special consideration shall be given to areas with concentrations of students, seniors, low-income families or others that are more dependent on modes other than the automobile to provide a safe, accessible environment.
- Parks and Recreation
 - Goal 1: Provide, manage and fund adequate recreational and cultural facilities to encourage a physically and mentally healthy lifestyle for all citizens of Tallahassee-Leon County
 - Policy 1.5.3: The County shall help fund and develop a Greenways Trail System that provides the public opportunities to access a safe and convenient trail system in the unincorporated County. This system should incorporate public lands, right of way, easements on private lands, and open space designated lands to achieve this goal. The trail system will connect to the maximum extent possible existing and future residential areas to employment, education, and activity centers.

Regional Mobility Plan: This project supports the Capital Regional Transportation Planning Agency's long range plan of facilitating east west modal movements, enhancing the vibrancy of the western portion of Tallahassee, and promoting multimodal transportation and regional connectivity.

Greenway Master Plan: This project connects multiuse paths to the proposed San Luis Greenway and the Ochlockonee River Valley Greenway as identified in the Greenway Master Plan. The development of the Park Place PUD recreation facilities will expand upon the original scope of the Ochlockonee River Valley Greenway and improve multi-use connections to the Talquin State Forest.

Connectivity: This project provides connectivity between the amenities located on Capital Circle and several within the central core of the city therefore enabling connections to Lake Ella, Martha Wellman Park and the proposed regional activity center of Hopkins Crossing and Park Place. The addition of

Project #3: Northwest Connector Corridor

Page 3

the multi-use trail south of San Luis Mission Park to Mission San Luis will provide connectivity for high density residential neighborhood to the existing and proposed pedestrian/bike network.

This project adds almost eight miles of additional trail and sidewalk connectivity between Ocala Road and Mission San Luis northwest through the Park Place PUD to the Talquin State Forest and the Fred George Park, and east again toward the Lake Jackson Library. This connectivity will allow regional travel for bicycles and pedestrians and provide an efficient connection between the NE to the NW with interconnection to existing pedestrian and bike networks.

Complements Blueprint 2000 Project(s): This project complements the following Blueprint 2000 projects: Martha Wellman Park, Fred George Greenway, and Capital Circle trail system.

Water Quality Enhancements and Green Infrastructure: This project includes proposed trail connectors to fill in connectivity gaps and link existing and proposed amenities of the Park Place PUD with Fred George Greenway, and Talquin State Forest, Stoneler Park to trails in Talquin State Forest, Tower Road Park and Tower Road Landing, which total almost eight miles of additional trails.

Stormwater/Sewer Capacity Improvements: The This project will provide stormwater upgrades to address flooding experienced by the commercial property at the two major stream crossings.

Transportation Capacity Improvements: This project will widen Tharpe Street from Ocala Road to Capital Circle NW to four lanes. The project will include landscaped medians, sidewalks, and bike lanes. Tharpe Street currently functions at Level of Service D. The proposed roadway improvements are necessary to maintain function as growth continues in this quadrant of our community. The enhanced multi-modal opportunities provided by the transit and bicycle/pedestrian facilities along this constrained corridor were not reflected in the traffic analysis completed in 2003.

Core Infrastructure: The Northwest Connector Corridor project is considered a core infrastructure project as it widens an arterial road and provides capacity improvements to a key commercial and industrial corridor in our community.

SALES TAX PROJECTS

Project Number: 11
Project Name: Lake Lafayette & St. Marks Regional Linear Park
Total Project Cost: \$20.8 million

Total Project Cost Notes

This project cost does include right of way costs.
Sidewalk for Chaires Crossroad is only excluded cost from Ecotourism/Parks theme (\$400,000).

Executive Project Summary

This project provides connectivity by linking the public recreation lands that encompass more than 7,200 acres east of Capital Circle SE with trails and board walks, many of which are identified in the Greenway Master Plan. The project also constructs a sidewalk along Chaires Cross Road providing safe pedestrian access from adjacent residential areas to Chaires elementary school, community center, recreation center and the myriad of interlinks to other regional amenities.

Additional recreational facilities will be constructed at Tom Brown Park and the St. Marks Headwaters. Ecosystem restoration will be achieved through stream restoration, stormwater retrofit, and exotic/invasive plant management on the public lands. Critical floodwater analysis of the St. Marks River and Lafayette Basin system will assist residents as well as protect the proposed facilities. Stormwater treatment will be pursued for the tributary to Upper Lake Lafayette flowing through Tom Brown Park as well as incorporate ecosystem restoration through the removal of invasive and exotic vegetation.

The project also funds the construction of the additional ball fields at Tom Brown Park, provided that the land is conveyed to the County from the federal government. Congressman Southerland recently filed a bill requesting that 9 acres of the Federal Correctional Institute property be conveyed from the Bureau of Prisons to Leon County for use for additional recreational space.

Finally, this project conducts a critical analysis of the floodwaters generated in the St. Marks and Lafayette basins which as the potential to provide flood insurance relief to property owners east of Chaires Crossroad.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

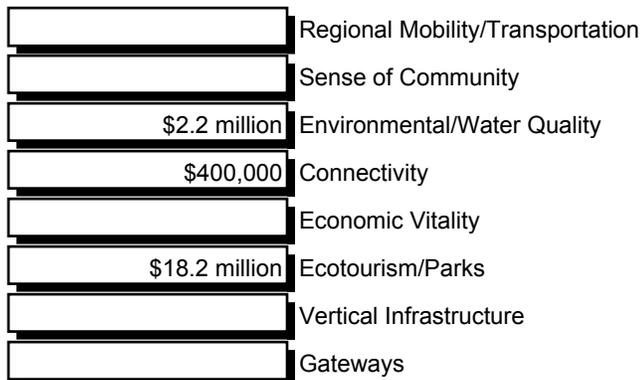
Detail Project Description

The proposed improvements for this project include the following:

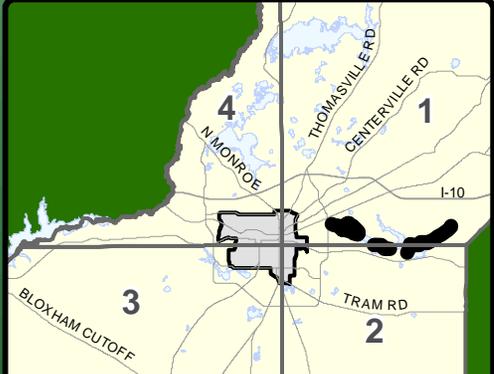
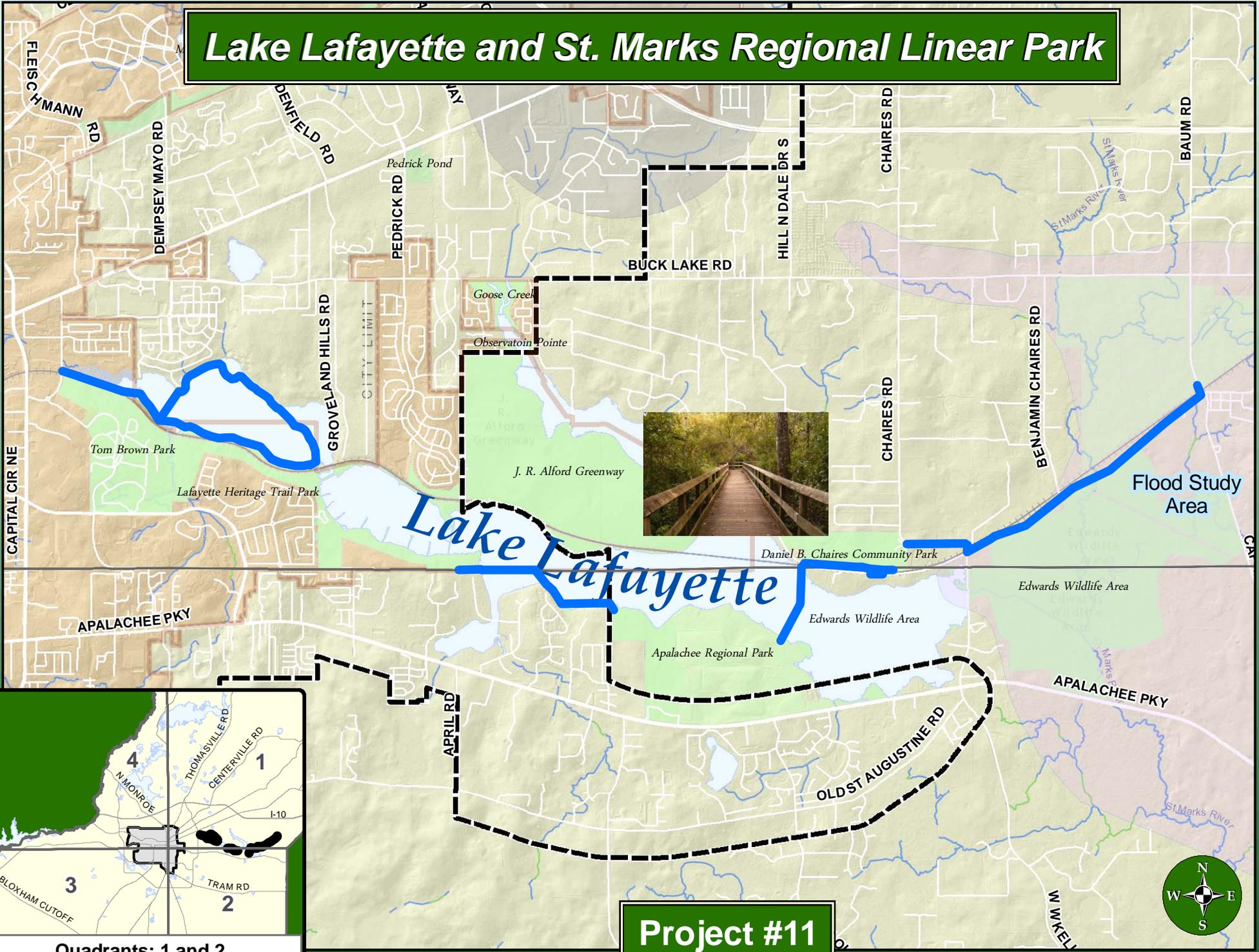
- Providing off-road trails from Upper Lake Lafayette to St. Marks Headwaters.
- Adding stormwater treatment facilities in Tom Brown Park.
- Construction of boardwalk across the lake from the Apalachee Regional Park will provide access to Lower Lake Lafayette wetlands. This project provides critical connectivity by linking Upper Lake Lafayette east to the St. Marks Headwater with trails and board walks. The construction of a boardwalk spanning the lake from the Apalachee Regional Park north to the Road to the Lake and ends at the Lake landing provides unprecedented access to the unique Lower Lake Lafayette wetlands.

- Construction of Goose Creek Trailhead and link to Nusbickel site;
- Trailhead enhancements in the St. Marks Headwaters include parking, canoe launching and educational features. This project significantly advances the implementation of the Lake Lafayette Greenway and the St. Marks Greenways identified in the Master Plan.
- Construction of additional ball field at Tom Brown Park.
- Construction of a sidewalk on Chaires Cross Road from Green Oak to Capitola Road. This sidewalk will provide safe pedestrian access from adjacent residential areas to Chaires elementary school, community center, recreation center and the myriad of interlinks to other regional amenities.
- Analysis of floodwaters generated in the St. Marks and Lafayette basins to protect proposed improvements. This project conducts critical analysis of the floodwaters generated in the St. Marks and Lafayette basins to protect the residents in this area, and properly place the proposed amenities. The study also has the potential to provide flood insurance relief to property owners east of Chaires Crossroad.
- Ecosystem restoration by linking City, County, State, and Federal property and removing exotic and invasive vegetation during the construction of the trail network.

Cost By Themes



Lake Lafayette and St. Marks Regional Linear Park



Quadrants: 1 and 2

Project #11



Previous Project Information for:

Project #11

Lake Lafayette and St. Marks Regional Linear Park

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Lake Lafayette and St. Marks Linear Regional Park (*Previously Project #52*)

Phase II – Eastern Leon County Groundwater and Floodplain Protection (*Previously Project #8*)

Proposed Project for the Sales Tax Extension

**Project #7: Lake Lafayette and St. Marks
Linear Regional Park**

Estimated Project Cost: \$18.3 million

Project Criteria:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input checked="" type="checkbox"/> Water Quality Enhancements & Green Infrastructure |
| <input checked="" type="checkbox"/> Regional Mobility | <input checked="" type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input checked="" type="checkbox"/> Greenway Master Plan | <input type="checkbox"/> Transportation Capacity Improvements |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input checked="" type="checkbox"/> Complements BP2000 Project | |

Project Description:

The Lafayette and St. Marks Linear Regional Park incorporates a holistic approach to regional mobility and connectivity to existing/proposed recreational amenities (including Blueprint 2000 projects) as well as conducts a critical analysis of the St. Marks flood waters. The project provides connectivity by linking the public recreation lands that encompass more than 7,200 acres east of Capital Circle SE with trails and board walks, many of which are identified in the Greenway Master Plan. The project also constructs a sidewalk along Chaires Cross Road providing safe pedestrian access from adjacent residential areas to Chaires elementary school, community center, recreation center and the myriad of interlinks to other regional amenities. Stormwater treatment will be pursued for the tributary to Upper Lake Lafayette flowing through Tom Brown Park as well as incorporate ecosystem restoration through the removal of invasive and exotic vegetation. The project also funds the construction of the additional ball fields at Tom Brown Park, provided that the land is conveyed to the County from the federal government. Congressman Southerland recently filed a bill requesting that 9 acres of the Federal Correctional Institute property be conveyed from the Bureau of Prisons to Leon County for use for additional recreational space. Finally, this project conducts a critical analysis of the floodwaters generated in the St. Marks and Lafayette basins which as the potential to provide flood insurance relief to property owners east of Chaires Crossroad.

The proposed improvements for this project include the following:

- Providing off-road trails from Upper Lake Lafayette to St. Marks Headwaters;
- Adding stormwater treatment facilities in Tom Brown Park;
- Construction of boardwalk across the lake from the Apalachee Regional Park will provide access to Lower Lake Lafayette wetlands;
- Construction of Goose Creek Trailhead;
- Trailhead enhancements in the St. Marks Headwaters include parking, canoe launching and educational features;
- Construction of additional ball fields at Tom Brown Park;
- Construction of a sidewalk on Chaires Cross Road from Green Oak to Capitola Road;
- Analysis of floodwaters generated in the St. Marks and Lafayette basins to protect proposed improvements;
- Ecosystem restoration by linking City, County, State, and Federal property and removing exotic and invasive vegetation.

Project #7: Lake Lafayette and St. Marks Linear Regional Park

Page 2

Project Detail Criteria:

Comprehensive Plan: This project supports the Parks and Recreation, Conservation, Mobility, and Stormwater elements of the Comprehensive Plan. The areas that the project complies with are listed below.

- Parks and Recreation:
 - Goal 1: Provide, manage and fund adequate recreational and cultural facilities to encourage a physically and mentally healthy lifestyle for all citizens of Tallahassee-Leon County
 - Policy 1.5.3: The County shall help fund and develop a Greenways Trail System that provides the public opportunities to access a safe and convenient trail system in the unincorporated County. This system should incorporate public lands, right of way, easements on private lands, and open space designated lands to achieve this goal. The trail system will connect to the maximum extent possible existing and future residential areas to employment, education, and activity centers.
- Conservation Element:
 - Goal 1: Preserve, protect and conserve the ecological value and diversity of natural resources in Tallahassee and Leon County.
 - Goal 3: Protect, enhance, and restore natural resources, wildlife habitat, and natural vegetative communities to maintain a diversity of native flora and fauna to assure the maintenance of a viable population of native species.
- Mobility Element:
 - Policy 1.1.4: Promote neighborhood parks to reduce the need for long distance trips for recreation.
 - Policy 1.1.5: Maintain a Greenways Master Plan that integrates pedestrian and bicycle mobility into a linear park and open space system that connects local, regional, and state facilities, with specific emphasis on connections within Downtown and energy efficiency districts.
 - Policy 1.2.8: Provide a safe, accessible environment and support active living for students by: developing and maintaining programs to increase biking and walking to schools; prioritizing sidewalk and bicycle infrastructure within a two mile radius of primary schools; and continuing to identify, fund and build Safe Routes to Schools projects.
- Stormwater Management Goal 1: Provide a stormwater management system which protects the health, welfare, and safety of the general public by reducing damage and inconvenience from flooding and protects surface water and groundwater quality.

Regional Mobility Plan: This project provides regional connectivity through multi use trails is one of the cornerstones of the mobility plan. The project provides interconnectivity to a multitude of recreational activities and park destinations reduces the reliance on the car for these trips.

Greenway Master Plan: This project significantly advances the implementation of the Lake Lafayette Greenway and the St. Marks Greenways identified in the Master Plan.

Connectivity: This project provides critical connectivity by linking Upper Lake Lafayette east to the St. Marks Headwater with trails and board walks. The construction of a boardwalk spanning the lake from the Apalachee Regional Park north to the Road to the Lake and ends at the Lake landing provides unprecedented access to the unique Lower Lake Lafayette wetlands. The project also constructs a sidewalk along Chaires Cross Road providing safe pedestrian access from adjacent residential areas to Chaires elementary school, community center, recreation center and the myriad of interlinks to other regional amenities.

Complements Blueprint 2000 Project(s): This project complements the following Blueprint 2000 Projects: St. Marks Headwaters Greenway, Copeland Sink, and the Sensitive Lands project.

Project #7: Lake Lafayette and St. Marks Linear Regional Park

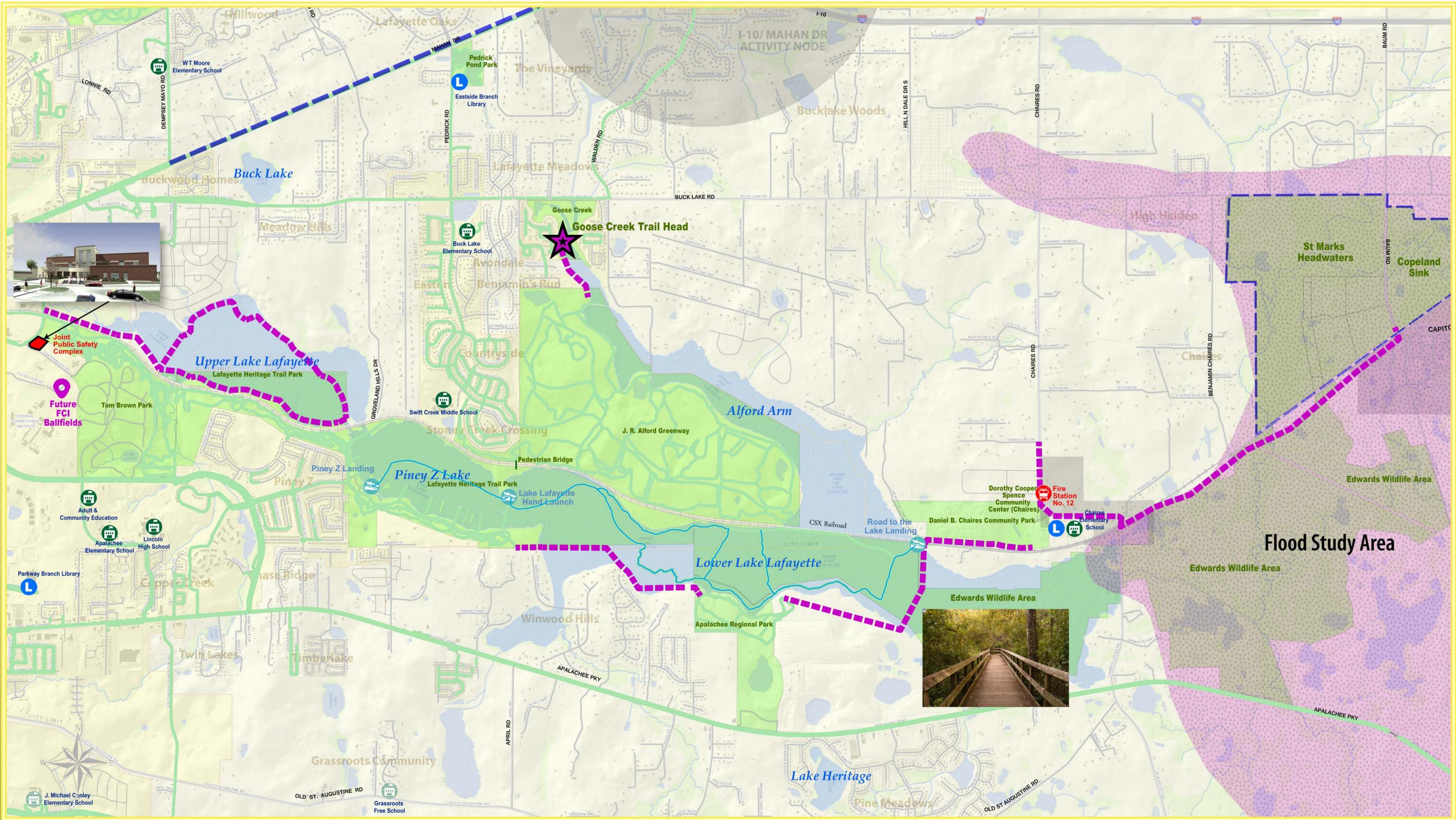
Page 3

Water Quality Enhancements and Green Infrastructure: This project by linking City, County, State, and Federal property. Invasive and exotic vegetation will be removed during the construction of the trail network. Stormwater treatment will be pursued for the tributary to Upper Lake Lafayette flowing through Tom Brown Park. In addition, this project will fund the construction of the additional ball fields at Tom Brown Park, provided that the land is conveyed to the County. On March 7, 2012, Congressman Southerland recently filed a bill requesting that 9 acres of the Federal Correctional Institute property be conveyed from the Bureau of Prisons to Leon County for use for additional recreational space at Tom Brown Park.

Stormwater/Sewer Capacity Improvements: This project conducts critical analysis of the floodwaters generated in the St. Marks and Lafayette basins to protect the residents in this area, and properly place the proposed amenities. The study also has the potential to provide flood insurance relief to property owners east of Chaires Crossroad.

Transportation Capacity Improvements: N/A

Core Infrastructure This project is considered a core infrastructure project due to the stormwater/sewer capacity improvements, green infrastructure, and water quality enhancements as detailed above.



Lake Lafayette and St. Marks Regional Linear Park

Legend

- Existing Sidewalks, Bike Routes
- Park Trails
- Proposed Trails
- Flood Study Area
- Blueprint 2000 Projects
- Parks
- Canoe Trails

Lake Lafayette and St. Marks Linear Regional Park Project Criteria:

- Comprehensive Plan
- Regional Mobility
- Greenway Master Plan
- Connectivity
- Complements BP2000 Project
- Water Quality Enhancements & Green Infrastructure
- Stormwater/Sewer Capacity Improvements
- Transportation Capacity Improvements
- Core Infrastructure



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Blueprint 2000

Proposed Sales Tax Projects

Project Name: Phase II-Eastern Leon County Groundwater and Floodplain Protection (Blueprint 2000 Map 7)

Project Themes:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

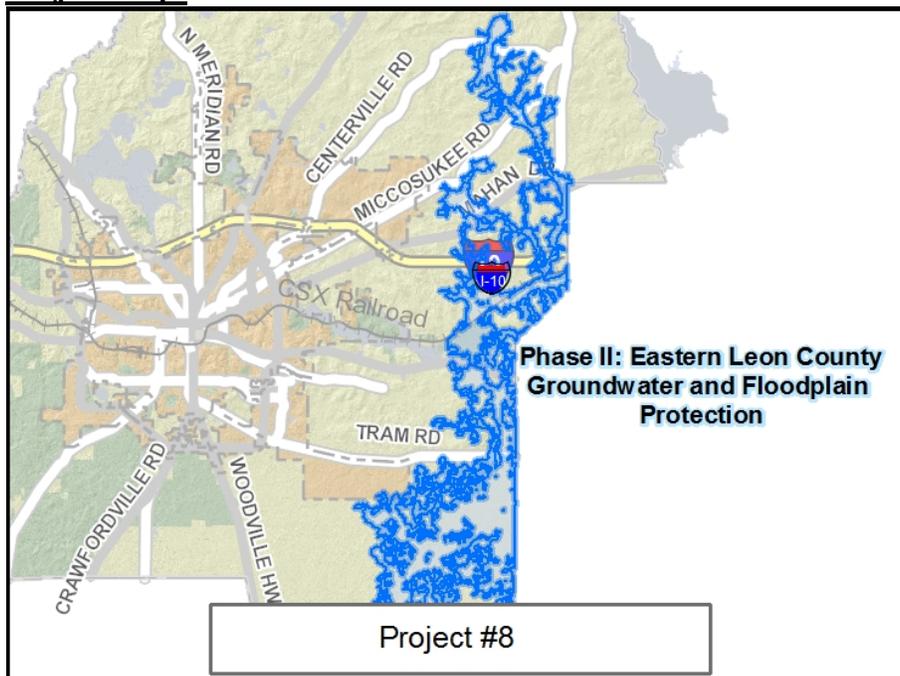
Project Description:

The primary benefit of this project is the continuation of efforts for groundwater protection and the preservation of extensive floodplains and wetlands east of the Interstate 10/Mahan Drive interchange. Addressing the need to protect groundwater now will avoid future flooding problems associated with new development in the area, and will preserve the quality of the headwaters of the St. Marks Rivers as well as the community’s drinking water. Only passive storage and biological treatment of stormwater are proposed in this project.

This project conducts a critical analysis of the floodwaters generated in the St. Marks and Lafayette basins. Using the results of this analysis, targeted acquisitions of floodplain may be necessary to achieve appropriate levels of flood attenuation and water quality treatment to ensure that stormwater entering the sink complexes has been treated to eliminate and potential impacts to groundwater.

Greenways should contain and connect the greatest number of environmentally sensitive areas east of the Interstate 10/Mahan Drive interchange including those owned by the Florida Department of Environmental Protection. The development of trail heads and trail crossings for greenways is a project component. Spatial extent of greenways should be adequate to ensure buffers for habitat value and sufficient biological treatments of stormwater runoff from adjoining lands prior to discharge to water courses, ponds or sinkholes.

Project Map:



SALES TAX PROJECTS

Project Number: 12
Project Name: Pine Flats Trail
Total Project Cost: \$21.2 million

Total Project Cost Notes

This project costs includes \$9.4 million for flood attenuation through purchase and excavation of compensating volume

Executive Project Summary

This project provides 24 miles of trail connectivity for pedestrians and bicyclists by creating a greenway corridor from the existing Cascades Trail system to the St. Marks Trail and the Apalachicola National Forest trails. This trail system greatly enhances the interconnectivity of the county and implements a portion of the Lower Capital Cascades Greenway Project from the adopted Greenways Master Plan. The southern trail section reestablishes the former river bed, containing the floodwaters which will prevent flooding of almost 400 home sites and their associated septic tanks. The restoration of the 8-Mile Pond improves not only the stormwater flow-way but also supports the ecosystem restoration of this area by further reducing pollutant loads to the aquifer at Ames Sink.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

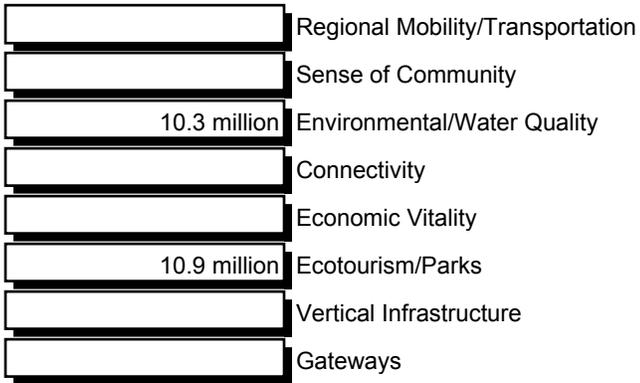
Detail Project Description

The Pine Flats Trail project proposed improvements include the following:

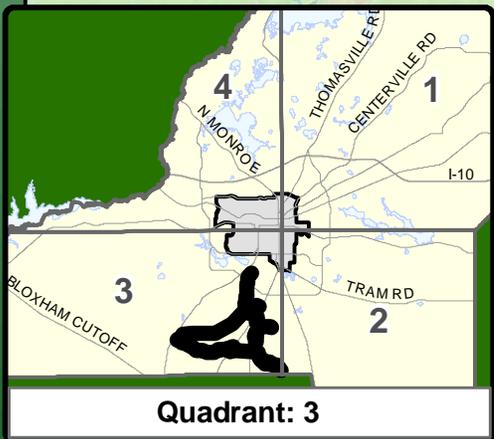
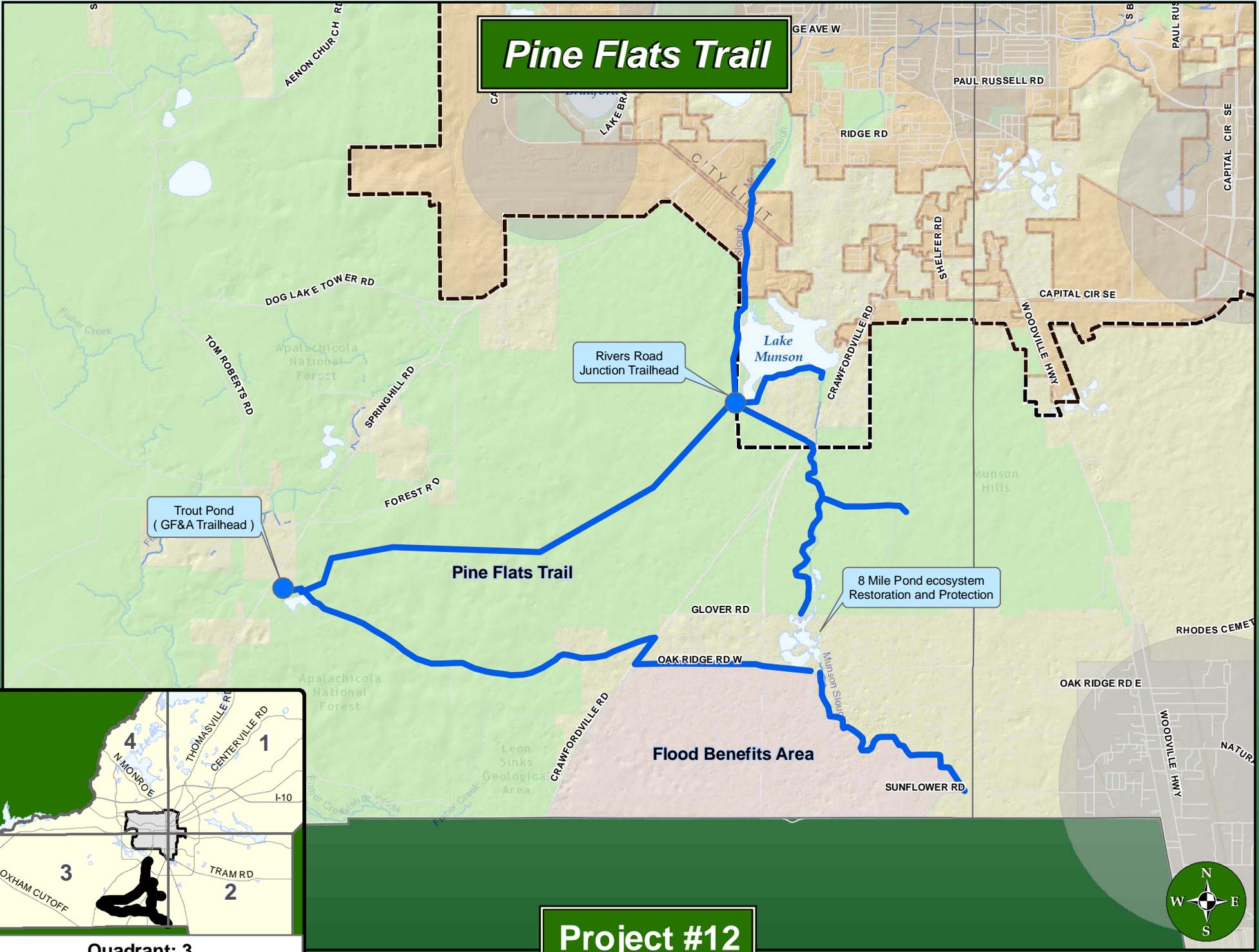
- Restoration and protection of the 8-Mile Pond ecosystem. The ecosystem restoration at 8-Mile Pond will improve water quality prior to discharge to the aquifer at Ames Sink, as well as replace exotic and invasive plants with native species. In addition, the project will also provide water quality enhancement in the Wakulla Karst Plain. The planned purchase of additional property will also serve to improve the stormwater quality and support the ecosystem restoration of this area by further reducing pollutant loads to the aquifer.
- Improve water quality prior to discharge to the aquifer at Ames Sink and in the Wakulla Karst Plain Pond and flood mitigation for 400 properties. The neighborhoods south of the Apalachicola National Forest have historically borne the brunt of high stormwater volume from the 42,500-acre Lake Munson Basin which overwhelms Ames Sink. The rising water levels surround homes, flooding the wells and septic tanks, for long periods of time. Leon County purchased a number of affected properties using the 1996 federal assistance and the local Flooded Property Acquisition Program. This area is also the focus of the current Disaster Recovery Enhancement Fund (DREF) acquisition grant. The southern Trail section can be used to reestablish the former river bed, containing the floodwaters to property owned by Leon County. This will prevent flooding of almost 400 home sites and their associated septic tanks. Purchase of additional property in the flow-way will also serve to improve the stormwater quality and support the ecosystem restoration of this area by further reducing pollutant loads to the aquifer.
- Connectivity to the St. Marks Trail southeast of Ames Sink using Sunflower and Elgin Roads as well as to Munson Slough, Lake Henrietta Park, Capital Cascades Park, the 8-Mile Pond property, with spurs to Gilbert Waters Park on Crawfordville Road and the GFA trailhead at LL Wallace Road. The Pine Flats

Trail provides tremendous opportunities for unpaved trail connectivity to Munson Slough, Lake Henrietta Park, Capital Cascades Park, the 8-Mile Pond property, with spurs to Gilbert Waters Park on Crawfordville Road and the GFA trailhead at LL Wallace Road. The trailhead development includes the restoration and protection of the 8-Mile Pond ecosystem. Pine Flats Trail will continue southeast of Ames Sink, connecting to the St. Marks Trail using Sunflower and Elgin Roads.

Cost By Themes



Pine Flats Trail



Quadrant: 3

Project #12



Previous Project Information for:

Project #12

Pine Flats Trail

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Pine Flats Trail (Expansion of Lower Cascades) (*Previously Project #59*)

Proposed Project for the Sales Tax Extension

Project #5: Pine Flats Trail (Expansion of Lower Cascades)

Estimated Project Cost: \$17 million

Project Criteria:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input checked="" type="checkbox"/> Water Quality Enhancements and Green Infrastructure |
| <input checked="" type="checkbox"/> Regional Mobility | <input checked="" type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input checked="" type="checkbox"/> Greenway Master Plan | <input type="checkbox"/> Transportation Capacity Improvements |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input checked="" type="checkbox"/> Complements BP2000 Project | |

Project Description:

The Pine Flats Trail incorporates a holistic approach to regional mobility and connectivity to existing/proposed amenities (including Blueprint 2000 projects) as well as increases stormwater capacity thereby addressing flooding concerns to 400 properties in the area. The project provides unpaved trail connectivity for pedestrians and bicyclists by creating a greenway corridor from the existing Cascades Trail system to the St. Marks Trail and National Forest trails. This trail system greatly enhances the interconnectivity of the county and implements a portion of the Lower Capital Cascades Greenway Project from the adopted Greenways Master Plan. The southern trail section reestablishes the former river bed, containing the floodwaters which will prevent flooding of almost 400 home sites and their associated septic tanks. The restoration of the 8-Mile Pond improves not only the stormwater flow-way but also supports the ecosystem restoration of this area by further reducing pollutant loads to the aquifer at Ames Sink.

The Pine Flats Trail project proposed improvements include the following:

- Restoration and protection of the 8-Mile Pond ecosystem;
- Improve water quality prior to discharge to the aquifer at Ames Sink and in the Wakulla Karst Plain Pond;
- Flood mitigation for 400 properties;
- Connectivity to the St. Marks Trail southeast of Ames Sink using Sunflower and Elgin Roads as well as to Munson Slough, Lake Henrietta Park, Capital Cascades Park, the 8-Mile Pond property, with spurs to Gilbert Waters Park on Crawfordville Road and the GFA trailhead at LL Wallace Road.

Project Criteria Detail:

Comprehensive Plan: This project supports the both the Parks and Recreation and Stormwater Management elements of the Comprehensive Plan. The areas that the project complies with are listed below.

- Parks and Recreation:
 - Goal 1: Provide, manage and fund adequate recreational and cultural facilities to encourage a physically and mentally healthy lifestyle for all citizens of Tallahassee-Leon County
 - Policy 1.1.5: Maintain a Greenways Master Plan that integrates pedestrian and bicycle mobility into a linear park and open space system that connects local, regional, and state facilities, with specific emphasis on connections within Downtown and energy efficiency districts.
 - Policy 1.5.3: The County shall help fund and develop a Greenways Trail System that provides the public opportunities to access a safe and convenient trail system in the unincorporated County. This system should incorporate public lands, right of way, easements on private lands, and open space designated lands to achieve this goal. The trail system will connect to the maximum extent possible existing and future residential areas to employment, education, and activity centers.

Project #5: Pine Flats Trail

Page 2

- Stormwater Management Goal 1: Provide a stormwater management system which protects the health, welfare and safety of the general public by reducing damage and inconvenience from flooding and protects surface water and groundwater quality.
- Mobility Element Policy 1.2.3: Establish and maintain a safe and effective system of bicycle lanes, sidewalks, and shared-use paths in conjunction with existing and planned roadways and the Greenways Master Plan. Where design criteria allow and safe operation will occur, separate bicycle and pedestrian traffic from vehicular traffic. Provide adequate and secure bicycle parking facilities at major destinations.

Regional Mobility Plan: This project supports the mobility plan by interconnecting regional trail networks promoting pedestrian and bicycle modes.

Greenway Master Plan: This project provides a greenway corridor from the existing Blueprint Trail system to St. Marks Trail and State Forest trails greatly enhancing the interconnectivity of the county. This project implements a portion of the Lower Capital Cascades Greenway Project from the adopted Greenways Master Plan.

Connectivity: The Pine Flats Trail provides tremendous opportunities for unpaved trail connectivity to Munson Slough, Lake Henrietta Park, Capital Cascades Park, the 8-Mile Pond property, with spurs to Gilbert Waters Park on Crawfordville Road and the GFA trailhead at LL Wallace Road. The trailhead development includes the restoration and protection of the 8-Mile Pond ecosystem. Pine Flats Trail will continue southeast of Ames Sink, connecting to the St. Marks Trail using Sunflower and Elgin Roads.

Complements Blueprint 2000 Project(s): The Pine Flats Trail complements the following Blueprint 2000 Projects: Capital Cascades Park and trail system, Martha Wellman Park, Debbie Lightsey Park, and Capital Circle trail system.

Water Quality Enhancements and Green Infrastructure: The Pine Flats Trail will provide recreational use, flood mitigation for almost 400 properties west of the proposed Trail, and ecosystem restoration at the 8-Mile Pond. The trail will act as greenway corridor from the existing Blueprint trail system along Capital Circle, Capital Cascades Trails, to St. Marks Trail and State Forest trails and provide additional recreational opportunities for pedestrians and bicyclists. The ecosystem restoration at 8-Mile Pond will improve water quality prior to discharge to the aquifer at Ames Sink, as well as replace exotic and invasive plants with native species. In addition, the project will also provide water quality enhancement in the Wakulla Karst Plain. The planned purchase of additional property will also serve to improve the stormwater quality and support the ecosystem restoration of this area by further reducing pollutant loads to the aquifer.

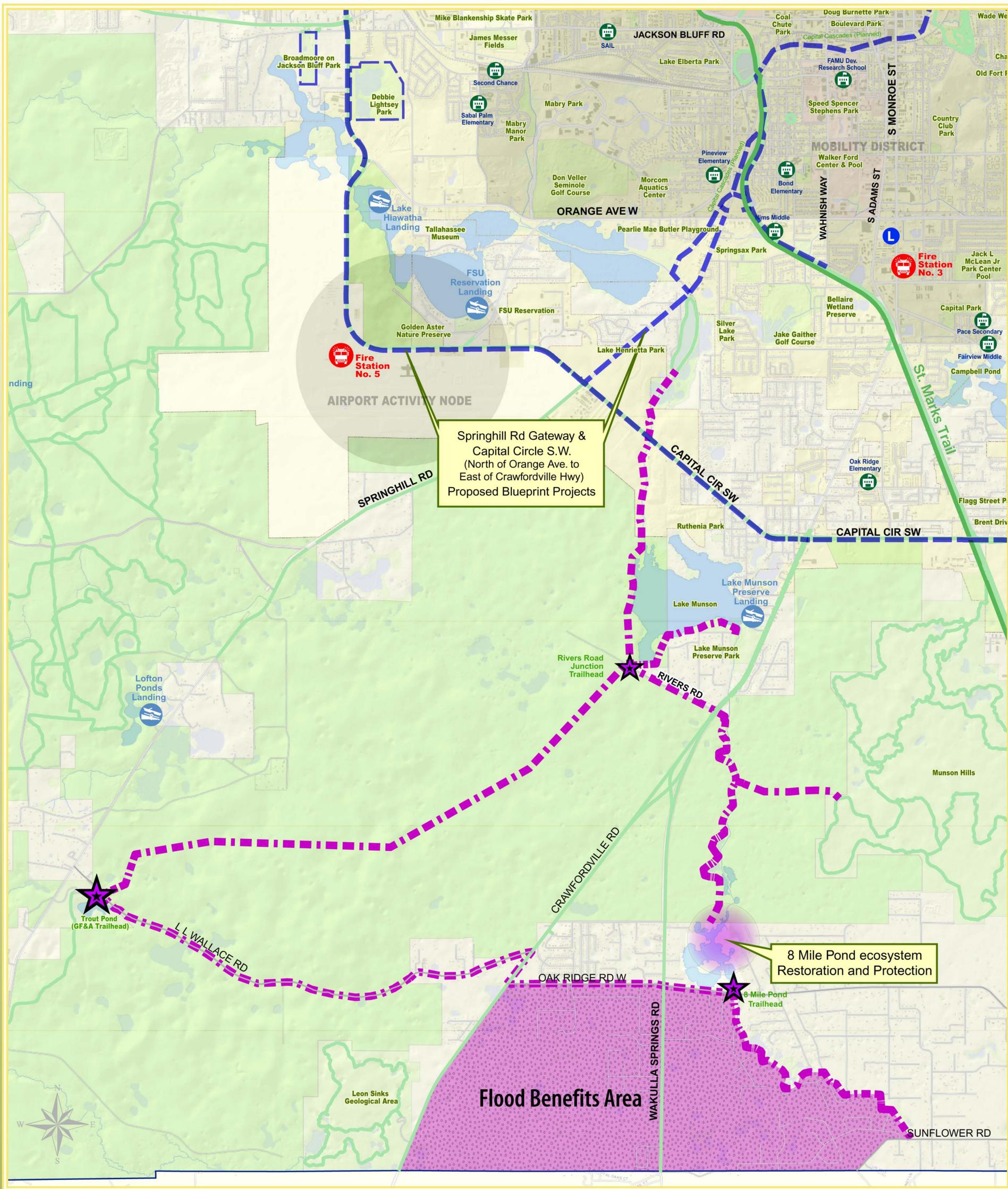
Stormwater/Sewer Capacity Improvements: The neighborhoods south of the Apalachicola National Forest have historically borne the brunt of high stormwater volume from the 42,500-acre Lake Munson Basin which overwhelms Ames Sink. The rising water levels surround homes, flooding the wells and septic tanks, for long periods of time. Leon County purchased a number of affected properties using the 1996 federal assistance and the local Flooded Property Acquisition Program. This area is also the focus of the current Disaster Recovery Enhancement Fund (DREF) acquisition grant. The southern Trail section can be used to reestablish the former river bed, containing the floodwaters to property owned by Leon County. This will prevent flooding of almost 400 home sites and their associated septic tanks. Purchase of additional property in the flow-way will also serve to improve the stormwater quality and support the ecosystem restoration of this area by further reducing pollutant loads to the aquifer.

Project #5: *Pine Flats Trail*

Page 3

Transportation Capacity Improvements: N/A

Core Infrastructure: The Pine Flats Trail project is considered a core infrastructure project due to the stormwater/sewer capacity improvements, green infrastructure, and water quality enhancements as detailed above.



Pine Flats Trail (Expansion of Lower Cascades)

Legend

- Existing Sidewalks, Bike Routes
- Park Trails
- - - Pine Flats Trail
- Flood Benefits Area
- - - Blueprint 2000 Projects
- St. Marks Trail (Existing)
- Parks

Pine Flats Trail (Expansion of Lower Cascades) Project Criteria

- Comprehensive Plan
- Water Quality Enhancements and Green Infrastructure
- Regional Mobility
- Stormwater/Sewer Capacity Improvements
- Greenway Master Plan
- Transportation Capacity Improvements
- Connectivity
- Core Infrastructure
- Complements BP2000 Project



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SALES TAX PROJECTS

Project Number: 13
Project Name: Black Swamp Restoration, Regional Pond, and Cascades to Munson Slough Greenway Trail Developments
Total Project Cost: \$33,600,000
Total Project Cost Notes
NA

Executive Project Summary

The project proposes to restore floodplains associated with the Cascade Chain-of-Lakes including Black Swamp, Grassy Lake, and Silver Lake. Floodplain protection and water quality enhancement are to be accomplished via acquisition, management (restoration of hydrocycle), construction of a regional stormwater pond, and improved management of the Munson Slough.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

The project proposes to preserve floodplains associated with the Cascade Chain-of-Lakes and Black Swamp for water quality and passive flood storage. Floodplain protection and water quality enhancement are to be accomplished via acquisition and management of Black Swamp (restore hydrocycle), construction of a pond near the West Drainage Ditch, and acquisition and restoration of active, developed properties in the floodplain.

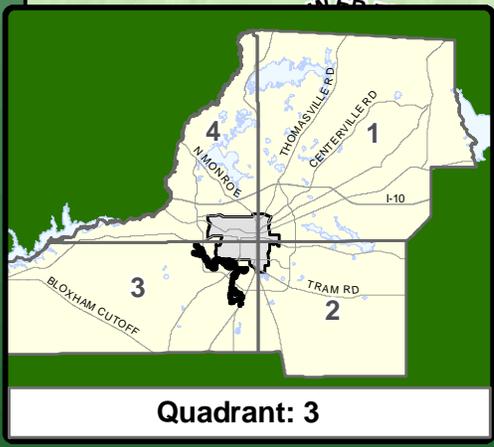
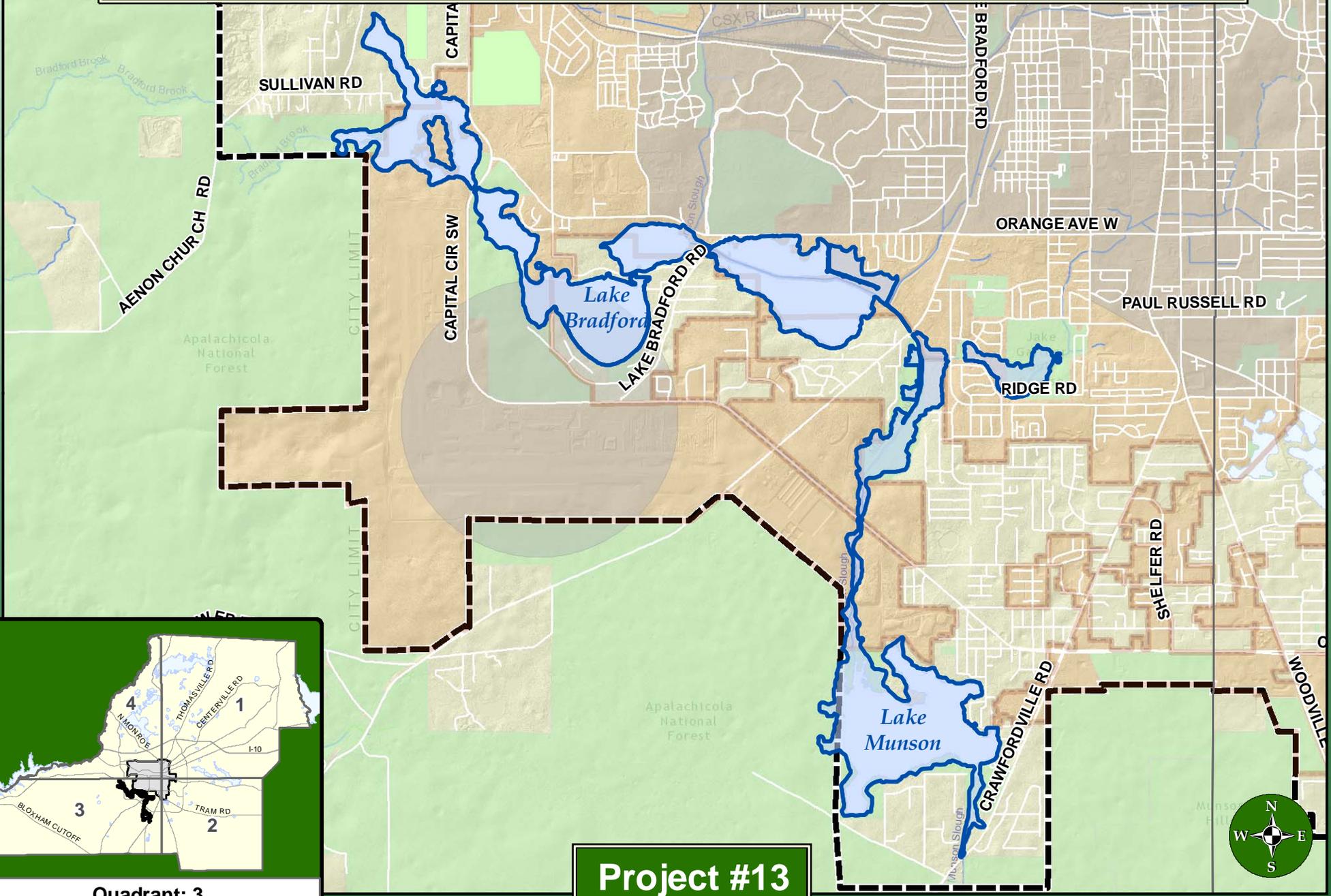
Water quality enhancement is to be accomplished via improved management of Munson Slough from the junction of the Central and West Drainage Ditches to its terminus below Lake Munson. In addition, this project will restore Silver Lake and Grassy Lake in the context of the Munson Watershed enhancements.

This project also proposes a greenway to link the Cascade Chain-of-Lakes to Munson Slough via the Black Swamp.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
\$23,200,000	Environmental/Water Quality
	Connectivity
	Economic Vitality
\$10,400,000	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Black Swamp Restoration, Regional Pond and Cascades to Munson Slough Greenway Trail Developments



Quadrant: 3

Project #13

Previous Project Information for:

Project #13

Black Swamp Restoration, Regional Pond and Cascades to Munson Slough Greenway Trail Developments

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Black Swamp Restoration, Regional Pond and Cascades to
Munson Slough Greenway Trail Developments (*Previously
Project #2*)

Blueprint 2000

Proposed Sales Tax Projects

Project Name: Black Swamp Restoration, Regional Pond and Cascades to Munson Slough Greenway Trail Developments (Blueprint 2000 Map 2B)

Project Themes:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

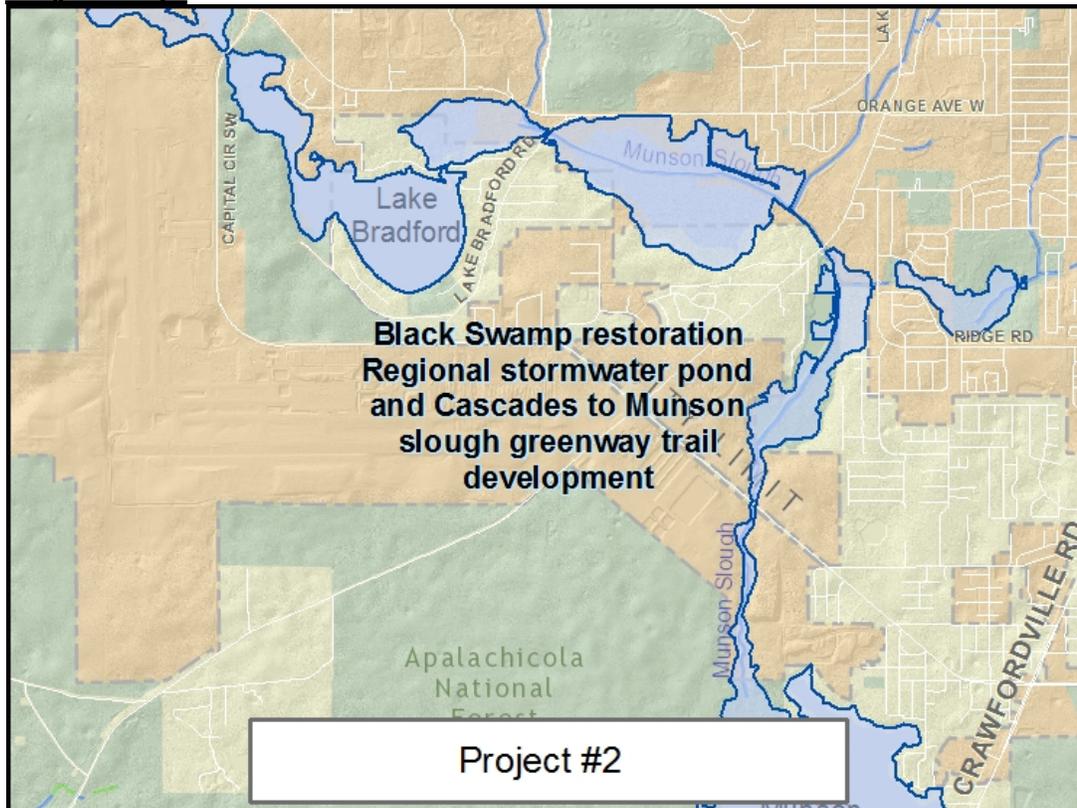
Project Description:

The project proposes to preserve floodplains associated with Black Swamp for water quality and passive flood storage. Floodplain protection and water quality enhancement are to be accomplished via acquisition and management of Black Swamp (restore hydrocycle), construction of a pond near the West Drainage Ditch, and acquisition and restoration of active, developed properties in the floodplain.

Water quality enhancement is to be accomplished via improved management of Munson Slough from the junction of the Central and West Drainage Ditches to its terminus below Lake Munson. In addition, this project will restore Silver Lake and Grassy Lake in the context of the Munson Watershed enhancements.

This project also proposes a greenway to link the Cascade Chain of Lakes to Munson Slough via the Black Swamp.

Project Map:



SALES TAX PROJECTS

Project Number: 14

Project Name: Market District Activity Center

Total Project Cost: \$9.4 million

Total Project Cost Notes

This project includes elements seen both in the City and County proposed projects, as well as improvements suggested by the citizens who work and live in the District. All of these suggestions are consistent with the adopted Action Plan.

Executive Project Summary

This project implements major elements of the Market District Corridor Placemaking Action Plan and includes the following objectives:

Activities, Events, & Community

- Create a visual edge for the district through gateways and branding
- Make the District a Regional Destination
- Create an aesthetically attractive district
- Support and Strengthen local business

Infrastructure & Public Amenities

- Create an interconnected district that links businesses and neighborhoods
- Reclaim or create new public spaces
- Improve traffic safety for all users

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

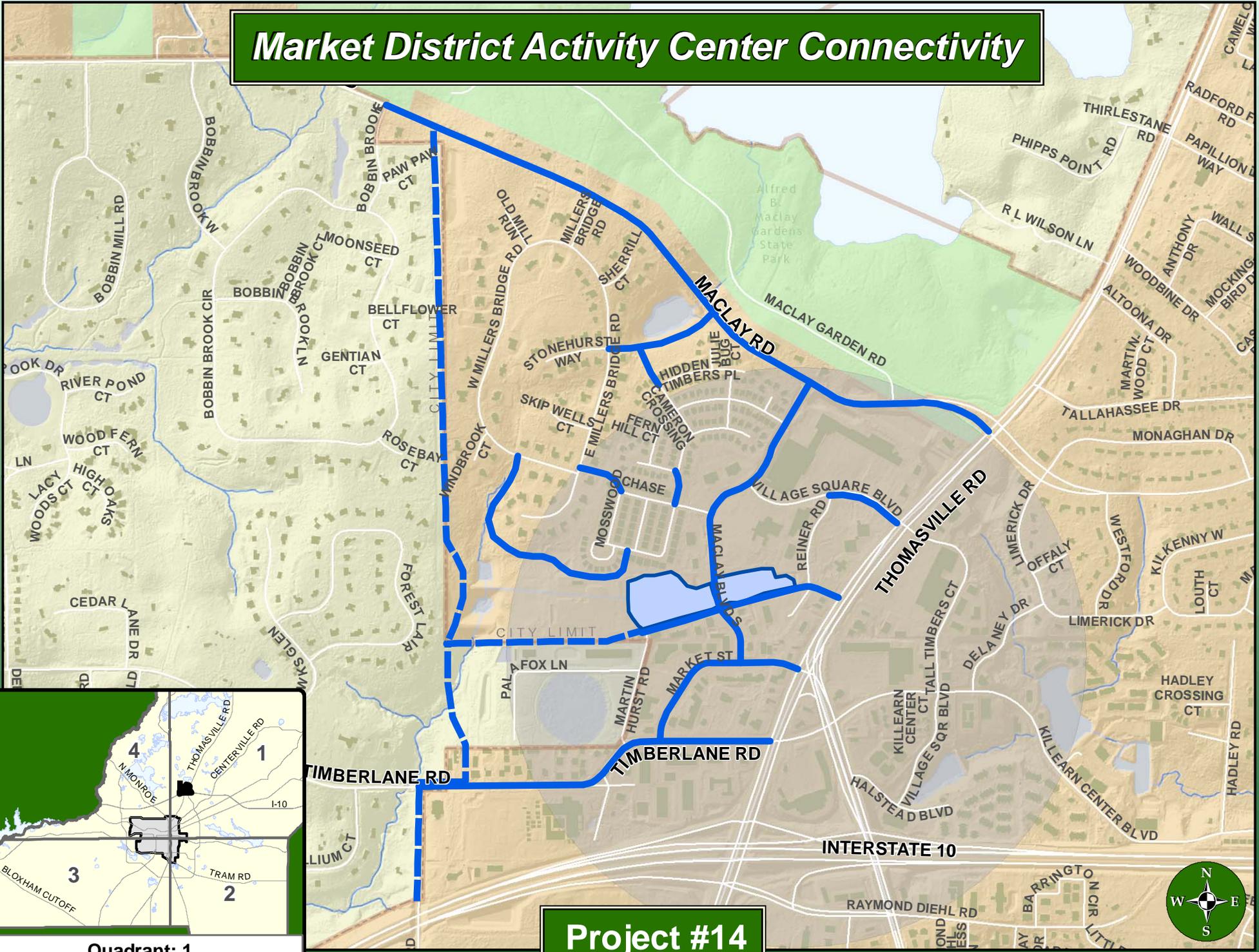
Detail Project Description

- Stormwater ponds would be retrofit as amenities with park facilities & parking (\$2.1 million)
- Pedestrian safety and streetscaping would be implemented along Timberlane Road, Maclay Boulevard, and Market Street (\$4.4 million)
- Missing links of sidewalks & trails would be added to connect residential areas to parks, schools, and commercial areas. Sidewalks include along Maclay Road, on the south side of Village Square Boulevard, and on Timberlane School Road connecting to Live Oak Plantation. Trails would include a north-south multiuse connection from Timberlane Road to Maclay Gardens State Park, an eastward connection to Maclay Boulevard, and neighborhood connections. (\$2.9 million)

Cost By Themes

	Regional Mobility/Transportation
\$9.4 million	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Market District Activity Center Connectivity



Project #14



Previous Project Information for:

Project #14

Market District Activity Center Connectivity

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: The Market District Activity Center Connectivity (*Previously Project #40*)

Inspiring the Creative Class: The Market District Placemaking
(*Previously Project #40*)

Proposed Project for the Sales Tax Extension

Project #8: The Market District Activity Center Connectivity

Estimated Project Cost: \$1.6 million (Leon County's Portion)

Project Criteria:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input checked="" type="checkbox"/> Green Infrastructure & Water Quality Enhancements |
| <input checked="" type="checkbox"/> Regional Mobility | <input checked="" type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input checked="" type="checkbox"/> Greenway Master Plan | <input type="checkbox"/> Transportation Capacity Improvements |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input type="checkbox"/> Complements BP2000 Project | |

Project Description:

The Market District Activity Center Connectivity project encompasses a holistic approach to transportation capacity improvements, regional mobility, and connectivity to existing/proposed amenities in The Market District (District). The District is home to numerous local businesses and Maclay Gardens, with several neighborhoods within easy walking distance. Many aspects of this projects focus on connecting the residential and commercial components of the area. The project includes multiple infrastructure projects throughout the regional hub located at the intersections of Interstate 10, Thomasville Road, and Capital Circle NE. The overall project would be a joint effort between the County and the City.

The proposed improvements for this project include the following:

- **Leon County Improvements:**
 - North-South multi-use trail connecting Timberlane Road to Maclay Gardens State Park, with eastward connection to Maclay Boulevard.
- **City of Tallahassee Improvements:**
 - Construct sidewalks on Maclay Road, Maclay Boulevard, and Maclay Commerce Drive.
 - Improvements to pedestrian intersections and mid-block crossings;
 - Reconfiguration of stormwater management facilities as public amenities, to include improved fencing, landscaping, sidewalk connections, and exercise equipment;
 - Pedestrian signage design, sitting, and fabrication.
- **Joint County/City Improvements:**
 - Gateway signage at four entry locations;
 - Sidewalk on Timberlane School Road connecting the Market District to Live Oak Plantation;
 - Streetscaping (e.g., street trees, wide sidewalks, street lights, underground utilities) for Maclay Boulevard, Market Street, and Timberlane Road.

Project Criteria Detail:

Comprehensive Plan: This project supports the Comprehensive Plan Land Use and Mobility Elements. The Market District is located within the Urban Services Area, which is the geographic area the Plan directs capital expenditures be prioritized. Those actions listed above are infrastructure projects that will improve quality of life in an urbanized area that currently lacks urban facilities.

- **Land Use Element**
 - Policy 1.1.2: Improvement of capital infrastructure shall be provided within the designated urban service area and shall be phased over the life of the plan.

Project #8: The Market District Activity Center Connectivity

Page 2

- Policy 2.1.5: Residential developments shall be designed to include a system of internal and inter-neighborhood circulation which promotes pedestrian and bicycle mobility. Within the Urban Service Area, sidewalks shall be required to provide pedestrian mobility.
- *Mobility Element*
 - Policy 1.2.3: Establish and maintain a safe and effective system of bicycle lanes, sidewalks, and shared-use paths in conjunction with existing and planned roadways and the Greenways Master Plan. Where design criteria allow and safe operation will occur, separate bicycle and pedestrian traffic from vehicular traffic. Provide adequate and secure bicycle parking facilities at major destinations.
 - Policy 1.2.13: Wherever feasible, bury utility lines. Priority should be given to areas where underground utilities can be incorporated into roadway construction and reconstruction projects.
 - Policy 1.3.2: Reduce demand for more and wider roads by evaluating traffic operations to maximize efficiency of the existing street system capacity and support modal shifts to public transit, bicycling, and walking.
 - Policy 1.3.6: Require tree plantings, where practical, for both new and old roads to increase screening, beauty, runoff control, and reduction of summer heat. Existing trees shall be protected during transportation system development and maintenance.
 - Policy 1.3.7: Aesthetically enhance and provide added environmental protection to existing and new transportation corridors by the following methods which include but are not limited to:
 - a) Incorporating for new, or increasing for existing corridors, the number of green spaces/open spaces and pedestrian oriented areas.
 - b) Recognizing plantations as a significant part of the natural landscape when roads are being designed in areas of the County where they are present.
 - c) Encouraging the use of native vegetation and natural systems such as swales to control runoff.
 - d) Maintaining natural ground cover, canopy, and understory where new roads are built.
 - e) Design public infrastructure improvements to minimize development impacts to protect designated canopy roads consistent with the Conservation Element.
 - f) Applying access management strategies that enhance the character of transportation corridors and gateways to the community by promoting shared access and consolidated signage and preserving green space for landscaping.
 - Objective 1.1 Land Use and Transportation Coordination: Coordinate transportation and land use systems that foster vibrant communities with compact urban forms and a mixture of uses to minimize travel distances, reduce vehicle miles traveled and greenhouse gases, and to enhance pedestrian and bicycle mobility and transit accessibility.
 - Objective 1.2 Complete Streets: The transportation system shall be designed and operated to provide safe, convenient and context-sensitive access for pedestrians, bicyclists, motorists, and public transportation users of all ages and abilities.
 - Objective 1.1 Connectivity and Access Management: Reduce vehicle trip demand, increase access and safety for cyclists and pedestrians, and preserve the integrity of the transportation system with effective connectivity and access management programs.

Regional Mobility Plan: This project supports the Regional Mobility Plan by constructing sidewalks and multi-use trails. These trails are listed in the Capital Regional Transportation Authority's Trails Cost Feasible and Needs Plan.

Greenway Master Plan: This project supports the Greenways Master Plan by providing a critical greenway corridor linkage between Maclay Gardens State Park and the proposed Timberlane Ravine trails just north of Interstate 10.

Connectivity: This project includes two goals related to connectivity: (1) create an interconnected district that links businesses and neighborhoods and to (2) knit the district together across Thomasville Road. The sidewalk, multi-use trail, and intersection improvements listed here advance those goals by providing safe, connected routes for bicycle and pedestrian traffic.

Complements Blueprint 2000 Project(s): N/A

Project #8: *The Market District Activity Center Connectivity*

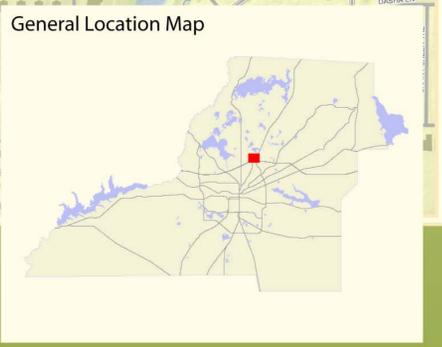
Page 3

Green Infrastructure and Water Quality Enhancements: This project will provide critical greenway corridor linkage between Maclay Gardens State Park and the proposed Timberlane trails just north of Interstate 10.

Stormwater Capacity Improvements: This project will analyze the necessary stormwater capacity expansion that would facilitate urban infill within The Market District. In addition, the project will reconfigure stormwater management facilities as public amenities, to include improved fencing, landscaping, sidewalk, and exercise equipment.

Transportation Capacity Improvements: N/A

Core Infrastructure: The Market District Action Plan is primarily a core infrastructure project due to the basic urban infrastructure needs included therein. These include sidewalks, bike lanes, multi-use trails and potential stormwater capacity improvements detailed above.



Market District Activity Center Connectivity

- Legend**
- Existing Sidewalks & Bike Routes
 - Lakes
 - County Proposed Sidewalks/Bike Trails
 - Extension of I-10 Landscaping
 - City Proposed Sidewalks/Bike Trails/Crosswalk Improvements
 - Proposed Greenspace
 - Gateways
 - Parks/Greenspace

- Market District Action Plan Project Criteria:**
- Comprehensive Plan
 - Regional Mobility
 - Greenway Master Plan
 - Connectivity
 - Complements BP2000 Project
 - Water Quality Enhancements & Green Infrastructure
 - Stormwater/Sewer Capacity Improvements
 - Transportation Capacity Improvements
 - Core Infrastructure



Program
4c

Inspiring the Creative Class: The Market District Placemaking

Estimated Cost:
\$6.7 million

WHAT IS IT?

The Market District Placemaking Plan includes the following objectives:

Activities, Events, & Community

- Create a visual edge for the district through gateways and branding
- Make the District a Regional Destination
- Create an aesthetically attractive district
- Support and Strengthen local business

Infrastructure & Public Amenities

- Create an interconnected district that links businesses and neighborhoods
- Reclaim or create new public spaces
- Improve traffic safety for all users
- Knit the district together across Thomasville Road

Evaluation Criteria:

Provides Regional Impact / Geographic Equity ✓
Enhances Mobility ✓
Supports Recreation and Quality of Life ✓

Benefits the Environment ✓
Supports Economic Development / Revitalization ✓
On an Existing Master Plan or Blueprint List ✓
Leverages Other Funds ✓



One of the major priorities identified by the Working Group of residents and business owners was to reclaim a fenced in stormwater pond and convert it to a public open space, similar to Lake Ella.



WHY IS IT IMPORTANT?

Benefits the Environment – The proposals provide infrastructure essential for creating a walkable town center in The Market District. The Timberlane Road streetscape enhancements and redesign of vacant public lands will add considerable tree canopy, which provides stormwater retention benefits. Redevelopment and urban infill often follows such public investment. This domino effect can reduce vehicle miles traveled and preserve greenfields from development on the urban outskirts.

Provides Regional Impact/Geographical Equity – The Placemaking package of projects distributes investment throughout the community. In The Market District the projects primarily provide local benefits, but the greenway components will benefit all Leon County citizens with improved recreational opportunities.

Enhances Mobility – The Timberlane Road streetscape improvements and greenway connection enhances several modes of mobility. Pedestrian connectivity and comfort is improved with wider sidewalks, street trees, and street lighting; and, the greenway offers bicycle and pedestrian routes that avoid major automotive thoroughfares. These improvements will accommodate users of all ages.

Supports Recreation and Quality of Life – The streetscape enhancements and park and plaza amenities will improve the quality of life by offering new recreational activities in close proximity to several neighborhoods as well as improving area aesthetics. The greenway proposal will utilize an existing, single use utility corridor to provide paved multi-use trails to enhance recreational and commuter connections.

Supports Economic Development/Revitalization – As with all Placemaking Action Plans, the selected projects were vetted by local business groups. The Timberlane Road streetscape enhancements and improvements to parkland are intended to spur investment and redevelopment in The Market District. The streetscape improvements are conducive to increasing pedestrian traffic and the recreational amenities can help make the District a destination for recreational enthusiasts.

On an Existing Master Plan or Blueprint List – The proposed projects are included in The Market District Placemaking Action Plan. The Greenways Master Plan includes the greenway project.

Leverages Other Funds – The City is investing \$500,000 to begin Implementation of smaller scale projects, plus there has been discussion of a public-private partnership that could implement certain items.

Potential greenway amenities



Top Priority Future Projects	Estimated Cost
Parkland Reclamation	
▪ Stormwater Ponds*	\$1 million
▪ Remnant lands at Martin Hurst Road	\$200,000
▪ Greenway system components	\$2 million
Timberlane Road urban streetscape	\$3.5 million
TOTAL Needed <small>[potential property acquisition not included]</small>	\$6.7 million

* Potential public-private partnership

SALES TAX PROJECTS

Project Number: 15

Project Name: Midtown Placemaking

Total Project Cost: \$22 million

Total Project Cost Notes

Executive Project Summary

This project implements major elements of the Midtown Placemaking Plan and includes the following objectives:

Activities, Events, Community

- Create a Midtown brand that will help form a visual district edge.
- Promote the arts through Murals, Music, Fashion, and Cuisine.
- Relieve parking complications and nuisances.
- Identify ways to support local business!

Infrastructure, Amenities, Urban Form

- Support urban infill and mixed use buildings for live, work and play.
- Create a walkable and bikable community.
- Reclaim or create new public spaces.
- Improve traffic safety for all users.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Detail Project Description

Intersection Improvements at 5 Points = \$3 million

Monroe Streetscaping = \$8 million

Thomasville Road Streetscaping (Post to Grape) = \$7 million

Thomasville Road Streetscaping (7th to Monroe) = \$8 million

Streetscaping estimates include treatments that can be seen on the reconstructed Gaines Street, such as wider sidewalks, enhanced crosswalks, benches, lighting, landscaping, and signage.

Cost By Themes

	Regional Mobility/Transportation
\$22 million	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Previous Project Information for:

Project #15

Midtown Placemaking

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Inspiring the Creative Class: Midtown Placemaking (*Previously Project #25*)

Program
4a

Inspiring the Creative Class: Midtown Placemaking

Estimated Cost:
\$22 million

WHAT IS IT?

The Midtown Placemaking Plan includes the following objectives:

Activities, Events, Community

- Create a Midtown brand that will help form a visual district edge.
- Promote the arts through Murals, Music, Fashion, and Cuisine.
- Relieve parking complications and nuisances.
- Identify ways to support local business!

Infrastructure, Amenities, Urban Form

- Support urban infill and mixed use buildings for live, work and play.
- Create a walkable and bikable community.
- Reclaim or create new public spaces.
- Improve traffic safety for all users.

Evaluation Criteria:

- Provides Regional Impact / Geographic Equity ✓
- Enhances Mobility ✓
- Supports Recreation and Quality of Life ✓

- Benefits the Environment ✓
- Supports Economic Development / Revitalization ✓
- On an Existing Master Plan or Blueprint List ✓
- Leverages Other Funds ✓



Medians, landscaping, and wider sidewalks on both Monroe and Thomasville Roads to soften the edges and make them more inviting for people walking and biking is a top priority for Midtown.



WHY IS IT IMPORTANT?

Benefits the Environment – The proposals will effectively reduce vehicle miles traveled. An improved pedestrian environment supports urban infill on underutilized properties in the area. Urban infill can preserve greenfields from development on the urban outskirts. The streetscape enhancements include extensive tree plantings, the canopies of which provide stormwater retention benefits.

Provides Regional Impact/Geographical Equity – The Placemaking package of projects distributes investment throughout the community. In Midtown, the projects primarily provide local, district-specific benefits through streetscape beautification and improved pedestrian facilities, although the intersection improvements at 5 Points will also have community-wide benefits for commuters. The aesthetic improvements to each corridor define a gateway to downtown that all residents and visitors experience.

Enhances Mobility – The three proposed Midtown projects advance the multi-modal policy direction of the local Comprehensive Plan. The streetscape enhancements will improve pedestrian mobility by limiting conflicts between pedestrians and automobiles while improving pedestrian comfort in our subtropical environment with shade trees. As with all new construction, these improvements will accommodate pedestrians of all ages.

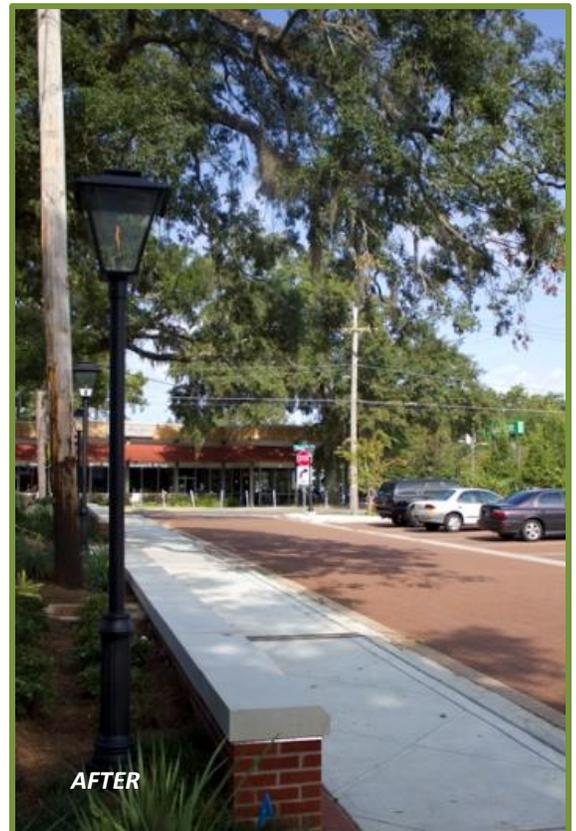
Supports Recreation and Quality of Life – The streetscape enhancements on both segments will have economic benefits for area retailers by making it safe for clients to walk by. Enhancements will expand areas for safe pedestrian activity and improving area aesthetics on two major thoroughfares.

Supports Economic Development/Revitalization – The existing vacant buildings surrounding 5 Points speak volumes, and the landscape along the Monroe Street and Thomasville Road corridor is barren. The proposed projects will correct both conditions. Reconfiguring the intersection should improve automobile and pedestrian accessibility to the surrounding properties. And, as is the case on Gaines Street, streetscape projects promote reinvestment and increased pedestrian traffic for area retailers.

On an Existing Master Plan or Blueprint List – Yes, the three proposed Midtown infrastructure projects are included in the Midtown Placemaking Action Plan. These projects have also been incorporated into the Mobility District Master Transportation Plan.

Leverages Other Funds – These projects are not budgeted at this time. CRA funds are available for use on the west side of Monroe Street. Furthermore, as State maintained roadways and part of the Master Transportation Plan, the CRTPA should be a funding partner.

The Fifth Avenue Plaza, recently completed, was the Working Group's top priority and will host street parties and vendors on a regular basis. Another top priority is to redesign the 5 Points intersection.



Top Priority Future Projects	Estimated Cost
Intersection improvements @ 5 Points	\$3 million
Monroe Street streetscape	\$8 million
Thomasville Road streetscape	
1. Post Road to Grape Street	\$7 million
2. 7 th Avenue to Monroe Street	\$4 million
TOTAL Needed [potential property acquisition not included]	\$22 million

SALES TAX PROJECTS

Project Number: 16
Project Name: Goodwood Children's Park and Community Garden

Total Project Cost: \$200,000

Total Project Cost Notes

Executive Project Summary

The Goodwood Children's Park and Community Garden Project is a potential partnership between The Goodwood Museum Foundation, Tallahassee Memorial Hospital, and the City of Tallahassee to build and maintain a children's activity park and a community garden near the Goodwood Museum. The Goodwood Museum Foundation owns land abutting Miccosukee Road which is envisioned as a park for the surrounding neighborhoods as well as people visiting patients at the hospital.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

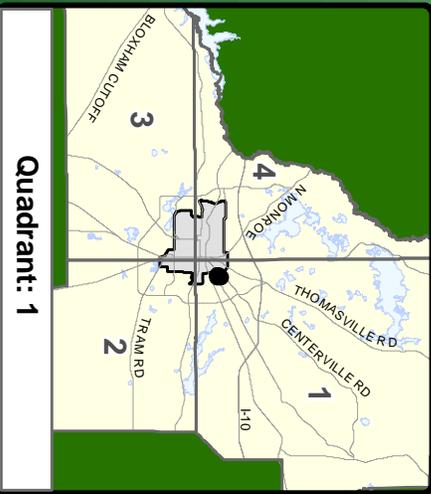
The Goodwood Children's Park and Community Garden Project would provide a park in walking distance to neighborhoods and businesses along Miccosukee Road, as well as to and people visiting patients at Tallahassee Memorial Hospital. Land fronting Miccosukee and owned by the Goodwood Museum Foundation would be converted into a park, playground, and community garden.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
\$200,000	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Goodwood Playground and Community Garden

Project #16



Previous Project Information for:

Project #16

Goodwood Playground and Community Garden

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Inspiring the Creative Class: Goodwood Playground & Community Garden (*Previously Project #24*)

Program
4f

Inspiring the Creative Class: Goodwood Playground & Community Garden

Estimated Cost:
\$200,000

WHAT IS IT?

The Goodwood Children’s Park and Community Garden Project is a potential partnership between The Goodwood Museum Foundation, Tallahassee Memorial Hospital, and the City of Tallahassee to build and maintain a children’s activity park and a community garden near the Goodwood Museum. The Goodwood Museum Foundation owns land abutting Miccosukee Road which is envisioned as a park for the surrounding neighborhoods as well as people visiting patients at the hospital.

Evaluation Criteria:

Provides Regional Impact / Geographic Equity	✓	Benefits the Environment	✓
Enhances Mobility	✓	Supports Economic Development / Revitalization	✓
Supports Recreation and Quality of Life	✓	On an Existing Master Plan or Blueprint List	✓
		Leverages Other Funds	✓



The Goodwood Children’s Park and Community Garden Project would provide a park in walking distance to neighborhoods and businesses along Miccosukee Road, as well as to and people visiting patients at Tallahassee Memorial Hospital.

WHY IS IT IMPORTANT?

Benefits the environment – The Community garden will be a dedicated, public green space which reduces greenhouse gas emissions, reduces the number of potential “heat islands” and supports beneficial native plant pollinators.

Provides Regional Impact/Geographical Equity – The subject area is the region’s largest medical campus and includes Tallahassee Memorial Hospital. Regional visitors of the surrounding medical facilities could experience the park, playground and garden amenities.

Enhances Mobility – The residents of the adjacent Brandt Hills and Betton Hills subdivisions could walk or bike to this park and playground. Today, the nearest playground facility is much further and requires driving.

Supports Recreation and Quality of Life – Playgrounds encourage an active lifestyle and reduce childhood obesity.

Supports Economic Development/Revitalization – The Goodwood Park and Community Garden users will likely support surrounding businesses such as Fanny’s Garden Cafe. The park and garden would also increase the public awareness of the Goodwood Conference Center and the related on-site meeting amenities.

Leverages Other Funds – Tallahassee Memorial Hospital, The Goodwood Museum and local government officials have reached a tentative agreement whereby the hospital would maintain the park and garden sites. The Goodwood Museum intends to dedicate the necessary property.

Land fronting Miccosukee and owned by the Goodwood Museum Foundation would be converted into a park, playground, and community garden.



SALES TAX PROJECTS

Project Number: 17
Project Name: College Avenue Placemaking
Total Project Cost: \$7 Million
Total Project Cost Notes

Executive Project Summary

Located in the heart of Downtown near Florida State University, the College Avenue Placemaking Action Plan supports investment another “destination district” in our community. The goal is to redevelop the College Park District to meet its fullest potential as a distinguishable and vibrant urban space that attracts people to live and visit, while honoring its history and prominent location between the City and the University.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

This placemaking area is bound by Tennessee Street to the north, Madison Street to the south, Duval Street to the east and Copeland Street to the west.

The Action Plan was developed by FSU Urban and Regional Planning Department in coordination with key stakeholders, including the City of Tallahassee, Leon County, Florida State University, Sigma Chi Development Corporation, the Greek Alumni Association, and local property owners.

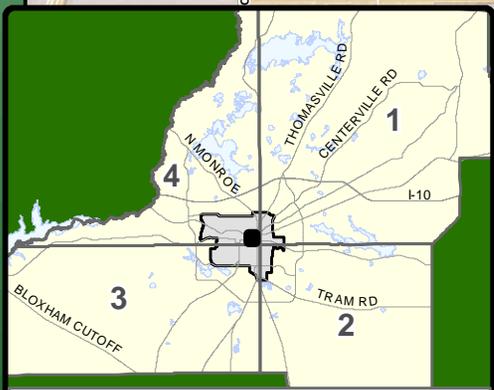
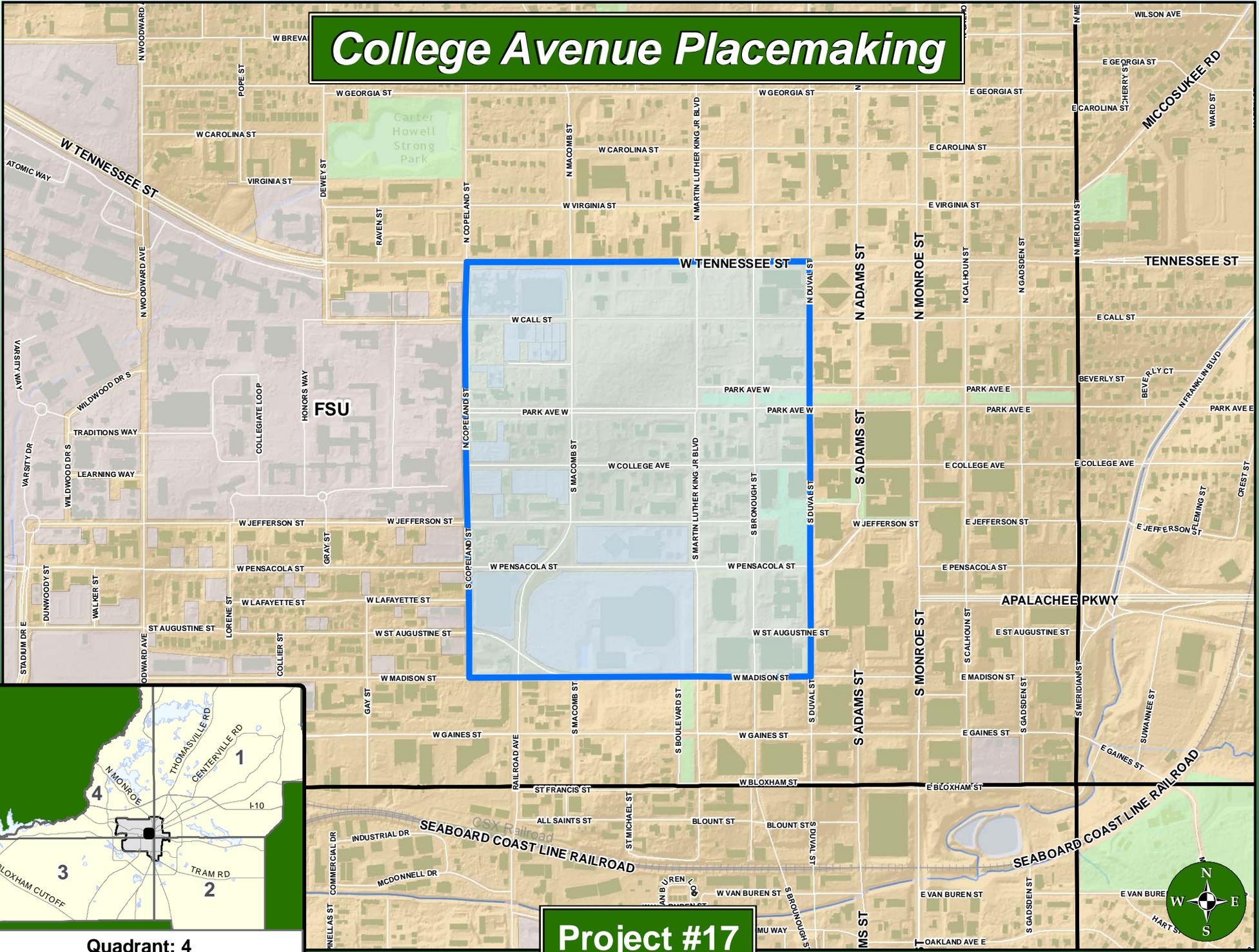
The Plan seeks to unify the interests of a broad spectrum of stakeholders ranging from private developers, the City of Tallahassee, the area’s universities, Greek organizations, its local business owners, and more. The Plan achieves this by identifying four strategies critical to the redevelopment of the College Park District: Enhance Comfort & Image, Upgrade Infrastructure, Improve Transportation Facilities, and Encourage Infill Development.

These strategies would be supported by streetscaping along College, Park, and Macomb; the creation of pedestrian walkways to connect the Civic Center, Main Campus, Law School and Downtown; as well as signage, intersection improvements and filling in missing sidewalk links.

Cost By Themes

	Regional Mobility/Transportation
\$7 million	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

College Avenue Placemaking



Quadrant: 4

Project #17

Previous Project Information for:

Project #17

College Avenue Placemaking

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Inspiring the Creative Class: College Avenue Placemaking
(*Previously Project #23*)

Florida State University and Tallahassee Downtown
Improvement Authority: College Avenue Placemaking
(*Previously Project #70*)

Program
4d

Inspiring the Creative Class: College Avenue Placemaking

Estimated Cost:
\$7 million

WHAT IS IT?

Located in the heart of Downtown near Florida State University, the College Avenue Placemaking Project will support another “destination district” in our community. Already underway, the first task of this placemaking project will be a studio project to be conducted by FSU Urban and Regional Planning Department students. The placemaking area is bound by Tennessee Street to the north, Madison Street to the south, Duval Street to the east and Copeland Street to the west. Key stakeholders include the City of Tallahassee, Leon County, Florida State University, Sigma Chi Development Corporation, the Greek Alumni Association, and local property owners.

Evaluation Criteria:

Provides Regional Impact / Geographic Equity	✓	Benefits the Environment	✓
Enhances Mobility	✓	Supports Economic Development / Revitalization	✓
Supports Recreation and Quality of Life	✓	On an Existing Master Plan or Blueprint List	✓
		Leverages Other Funds	✓



The College Avenue study area is the subject of a Capstone Project for Master's Degree students of the FSU Urban and Regional Planning Department.

WHY IS IT IMPORTANT?

Benefits the Environment – This placemaking project should result in a plan that will enhance and improve the area, make it more accessible and provide uses and activities to make it a destination for people to come and hopefully return often.

Provides Regional Impact / Geographic Equity – This placemaking project is in an area that includes FSU, the Civic Center, local businesses, and offices. Potential enhancements would impact the area and the region since area already provides uses that are utilized by the community and the region.

Enhances Mobility – This placemaking district is in the Multimodal Transportation District and the Downtown District. The districts promote safe and comfortable streets for pedestrians, cyclists and transit users. The districts also promote urban infill and mixed use buildings. As the Action Plan for this area unfolds, it will most likely include improvements to the streets, buildings and other infrastructure.

Supports Recreation and Quality of Life – Any enhancements coming out of the Action plan for the area will improve the quality of life for citizens.

Supports Economic Development / Revitalization – Any improvement projects coming out of the Action Plan should have economic benefits for area businesses, stakeholders and the community at large.

Leverages Other Funds – CRA funds could be available for potential projects in this area. This project also complements the stormwater conveyance project included under the “Complete the Downtown” Sales Tax Extension proposed program of projects.



While the Capstone project is set to be complete in December of 2012, key features are already known. Stormwater conveyance and integration of the Civic Center are central issues for this area.



Underground stormwater infrastructure to convey runoff to Lake Elberta is key to supporting any redevelopment along the corridor. Another important piece is increasing pedestrian and bicycle safety along Macomb Street, an auto oriented north/south street.



Proposed Sales Tax Projects by the Community

Project Name: Florida State University and Tallahassee Downtown Improvement Authority: College Avenue Placemaking (Project #70)

Project Themes:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Project Description:

On March 14, 2013, the Florida State University presented several projects for the consideration of the Sales Tax Committee. During the presentation, the University endorsed the City’s project 4d “College Avenue Placemaking.” The project description is derived from the handouts provided by the University to the Committee. It is the University’s opinion that the cost included in the City’s description is significantly under represented. The University recommends that this project should include funding for more than stormwater improvements on College Avenue but also include other improvements suggested in the College Park District Action Plan.

During this same meeting, the Downtown Improvement Authority (DIA) also endorsed the College Avenue placemaking project. The DIA also recommends that this project include wider sidewalks, street trees, street furniture, and other amenities. This rebuild also would include the stormwater conveyance upgrades.

Project Map:



SALES TAX PROJECTS

Project Number: 18
Project Name: Downtown Outdoor Pedestrian Mall
Total Project Cost: \$6 million
Total Project Cost Notes

Executive Project Summary

As part of the redevelopment of the Gaines Street Corridor and related to the construction of a convention center on the former O'Connell site, is the construction of an outdoor pedestrian mall on the former Chevron site (city-owned) located on Gaines Street between South Bronough Street and Martin Luther King Boulevard. The mall would include linear pedestrian plaza with water features and public gathering space that compliments Gaines Street investments. It would become a catalyst for the redevelopment of the City/County/CRA owned properties that consist of three city blocks between Macomb Street and Bronough Street.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input checked="" type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

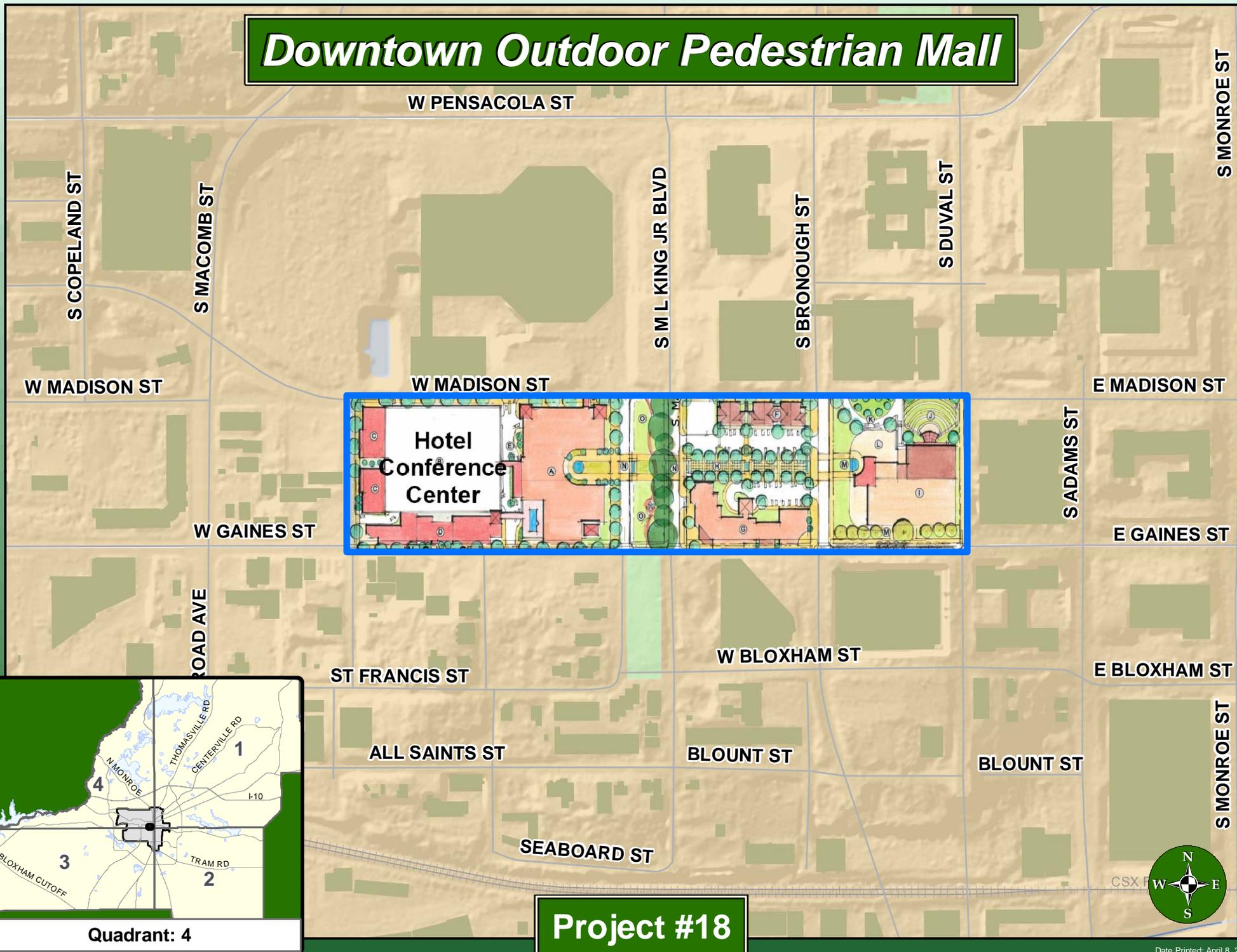
Detail Project Description

The pedestrian mall would link the former John's Building site with the O'Connell property, potentially connecting the proposed performing arts and convention centers on those sites, and at the same time, promote the redevelopment of the remaining portion of the former Chevron site.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
\$6 million	Vertical Infrastructure
	Gateways

Downtown Outdoor Pedestrian Mall



Previous Project Information for:

Project #18

Downtown Outdoor Pedestrian Mall

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Building the Core: Outdoor Pedestrian Mall (*Previously Project #19*)

Program
1a

Building the Core: Outdoor Pedestrian Mall

Estimated Cost:
\$6 million

WHAT IS IT?

As part of the redevelopment of the Gaines Street Corridor and related to the construction of a convention center on the former O'Connell site, is the construction of an outdoor pedestrian mall on the former Chevron site (city-owned) located on Gaines Street between South Bronough Street and Martin Luther King Boulevard. The pedestrian mall would link the former John's Building site with the O'Connell property, potentially connecting the proposed performing arts and convention centers on those sites, and at the same time, promote the redevelopment of the remaining portion of the former Chevron site.

Evaluation Criteria:

Provides Regional Impact / Geographic Equity

Enhances Mobility

Supports Recreation and Quality of Life



Benefits the Environment

Supports Economic Development / Revitalization

On an Existing Master Plan or Blueprint List

Leverages Other Funds



Artist's rendering looking East towards a potential Performing Arts Center (Bronough St.)



Artist's rendering looking West towards a potential Hotel Conference Center (Martin Luther King, Jr. Blvd)



Example of a pedestrian mall along Pearl Street in Boulder, CO

WHY IS IT IMPORTANT?

Enhances Mobility – The development of a pedestrian mall connecting the Johns Building site at the intersection of Bronough and Gaines Street with the former O’Connell property (site of the proposed convention center and hotel) two blocks to the west will help connect the traditional downtown with the revitalized Gaines Street District both physically and visually.

Supports Recreation and Quality of Life – It is expected the mall will also support recreation and quality of life by providing an intimate outdoor venue for public gatherings, as well as a connection to the downtown Chain of Parks.

Supports Economic Development / Revitalization – The mall will incorporate natural and man-made features designed to attract community residents and visitors to the area and provide opportunities for mixed-use developments on the mall edges, creating a regional impact that will also promote economic development and revitalization.

Leverages Other Funds – This project would leverage the investments in the surrounding Gaines Street area, as well as any potential funds spent on the Convention Center, the associated hotel, and the Performing Arts Center.



Gaines Street Pedestrian Mall

The mall would include linear pedestrian plaza with water features and public gathering space that compliments Gaines Street investments.

It would become a catalyst for the redevelopment of the City/County/CRA owned properties that consist of three city blocks between Macomb Street and Bronough Street.



16th Street Plaza in Denver CO

SALES TAX PROJECTS

Project Number: 19
Project Name: Florida A & M Entry Points
Total Project Cost: \$20 million

Total Project Cost Notes

1. Railroad Avenue Enhancements between Gaines Street and FAMU Way (\$3.5M)
2. FAMU Way Upgrade Monroe Street to M.L. King Jr. Boulevard (\$15M)
3. Traffic Signal at Palmer Avenue and Adams Street (\$500K)
4. Upgrade intersection improvements at Osceola Street and Adams Street(\$500K)
5. Upgrade intersection improvements at Perry Street and Gamble Street(\$500K)

Executive Project Summary

Improve the aesthetics of the road infrastructure and intersections accessing FAMU. Improvements should include such elements as monument signs at major entries, tree lined streets, wider sidewalks, signalized intersections with appropriate left turn signals, and pedestrian level lighting.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input checked="" type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Detail Project Description

Bring aesthetic parity to Railroad Avenue between Gaines Street and FAMU Way as this represents the northern gateway into FAMU, including installing monument gateway signs at Gaines Street. Upgrade the segment of FAMU Way between Monroe Street and M.L. King Jr. Boulevard so as to match the aesthetics of the rest of FAMU Way, including installing monument gateway signs at Monroe Street. Install a traffic signal with protected turning phases at the intersection of Palmer Avenue and Adams Street as this intersection. Aesthetically upgrade the intersections of Osceola/Adams Streets and Perry/Gamble Streets as these are high profile entries unto FAMU.

Cost By Themes

	Regional Mobility/Transportation
\$2 million	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
\$2 million	Vertical Infrastructure
\$16 million	Gateways

Florida A & M Entry points

GAINES ST. & RAILROAD AVE.
FLORIDA A&M UNIVERSITY
ENTRANCE ENHANCEMENT

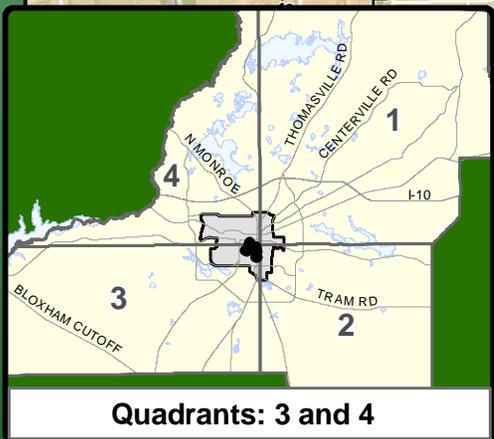
MONROE ST. & FAMU WAY
FLORIDA A&M UNIVERSITY
ENTRANCE ENHANCEMENT

GAMBLE ST. & PERRY ST.
INTERSECTION ENHANCEMENT

E. PALMER AVE. & W. PALMER AVE.
LEFT TURN SIGNAL TO ALLOW
ACCESS TO FLORIDA A&M
UNIVERSITY ENTRANCE

OSCEOLA ST. & ADAMS ST.
INTERSECTION ACCESS
IMPROVEMENTS

Project #19



Previous Project Information for:

Project #19

Florida A&M Entry Points

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Florida A&M University – Gateways (*Previously Project #67*)

Proposed Sales Tax Projects by the Community

Project Name: Florida A&M University - Gateways (Project #67)

Project Themes:

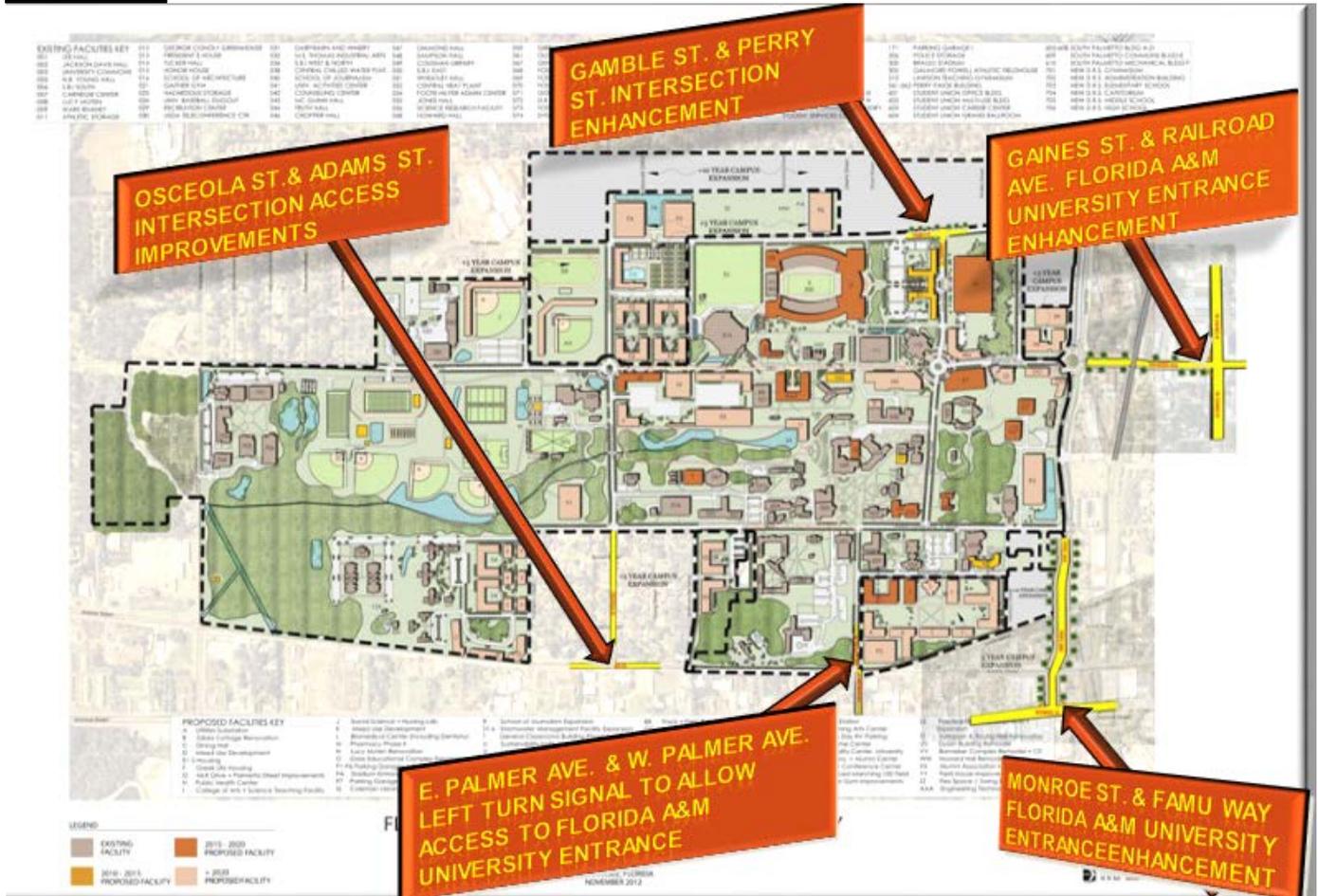
- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input checked="" type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Project Description:

On March 14, 2013, Florida A&M University presented a project for the consideration of the Sales Tax Committee. This project includes the construction of several gateways throughout Florida A&M Campus. The proposed gateways would be located at the following intersections:

- Osceola Street and Adams Street Intersection Access Improvements
- E. Palmer Ave. & W. Palmer Ave. Left Turn Signal to allow access to Florida A&M University
- Monroe St. & FAMU Way Florida A&M University Entrance Enhancement
- Gaines St. & Railroad Ave. Florida A&M University Entrance Enhancement
- Gamble St. & Perry St. Intersection Enhancement

Project Map:



SALES TAX PROJECTS

Project Number: 20
Project Name: Downtown Parking Garage
Total Project Cost: \$25 million

Total Project Cost Notes

Assumes \$20M for vertical construction and \$5M for property acquisition as a contingency against property constraints.

Executive Project Summary

The potential redevelopment of the Donald L. Tucker Civic Center and its proximity to the Gaines Street Corridor vitalization effort support the need for a Downtown Parking Garage in the vicinity of the Civic Center. This project sites and constructs a 1,200 car parking garage to support the areas parking needs.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input checked="" type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

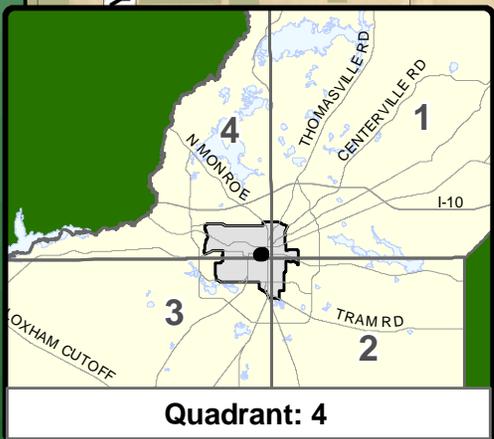
Detail Project Description

The redevelopment potential of the Donald L. Tucker Civic Center is such that if realized would result in a loss of the existing surface parking around the center. As a result, event parking would need accommodations in a structured parking garage. A 1,200 space parking garage would not just satisfy the needs of the civic center but also the surrounding commercial developments along Gaines Street and Madison Street. The estimated cost of such a parking garage is \$25M, which assumes \$5M for property acquisition.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
\$25 million	Vertical Infrastructure
	Gateways

Downtown Parking Garage



Quadrant: 4

Project #20



Previous Project Information for:

Project #20

Downtown Parking Garage

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Florida State University – Downtown Parking Garage
(*Previously Project #68*)

Proposed Sales Tax Projects by the Community

Project Name: Florida State University – Downtown Parking Garage (Project #68)

Project Themes:

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input checked="" type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

On March 14, 2013, the Florida State University presented several projects for the consideration of the Sales Tax Committee. This project is for the construction of a downtown parking garage. The project description for this project was developed using the materials by FSU during their presentation to the Committee.

Since July 1, 2012, upon the transfer of the Donald L. Tucker Civic Center to FSU, the University began conducting a comprehensive analysis of the entire facility. The preliminary results of this study suggest that the University’s long-term usage of this facility could include additional academic and mixed-use developments. If such developments become a reality, then the surface parking lots that currently serve the Civic Center could be lost. The University is requesting funding in the amount of \$20 million for the construction of a 1,200 space parking garage that would not only serve the Civic Center but also other development in the area including the commercial revitalization that is currently underway along the Gaines Street corridor.

Project Map:



SALES TAX PROJECTS

Project Number: 21
Project Name: The Florida Center of Performing Arts and Education
Total Project Cost: \$30 million

Total Project Cost Notes

The Florida Center for the Performing Arts and Education is requesting \$30 million from the proceeds of the sales tax. These funds will go toward the anticipated \$50 million construction cost currently being revised by consultants. \$10 million would come from private donations, corporate sponsorships, and grants. The balance of \$10 million would come from CRA funds and the bed tax money that has accumulated for the proposed performing arts center.

Executive Project Summary

In the past year, the Florida Center for the Performing Arts and Education has presented to the Sales Tax Committee three times, most recently on Thursday, March 14, 2013. The project description for this project was developed using the materials provided by the Florida Center for the Performing Arts and Education. For a full copy of the report, please click here: <http://leoncountyfl.gov/PerformingArts.pdf>

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input checked="" type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

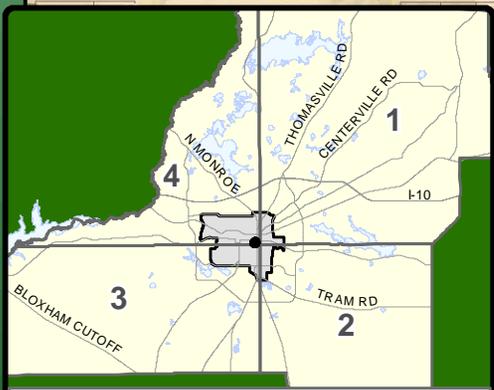
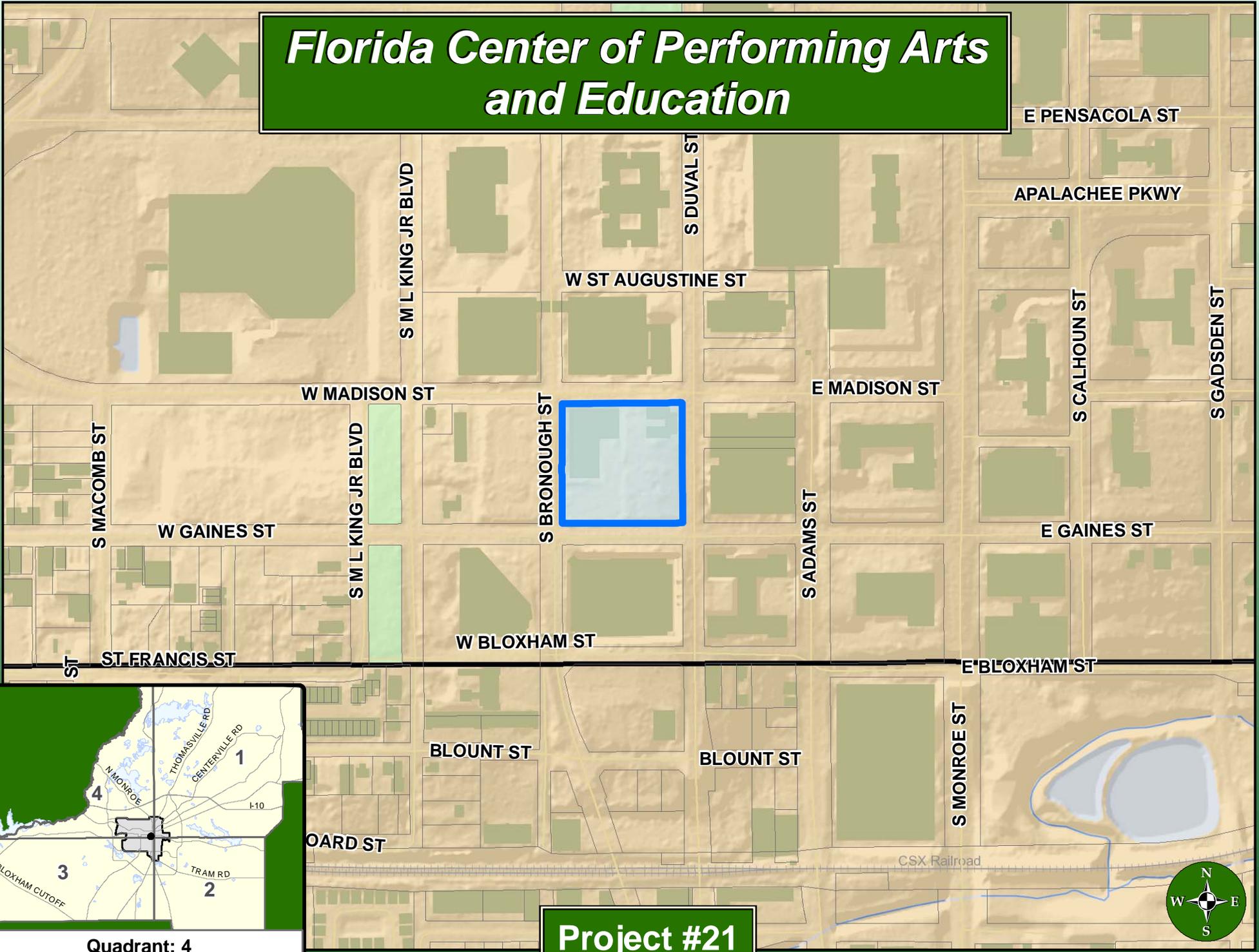
Detail Project Description

This project will provide for concert, dance, and theatre productions, including traveling entertainers, Broadway shows, and Distinguished Lecture Series. Currently, the project is being resized and re-priced with the assistance of Gilchrist Ross Crowe, the architectural firm that did the remodel of Ruby Diamond on the Florida State University Campus. It is estimated that the construction costs will be lowered and will have approximately 1,700 seats and an active lobby for the community. The Performing Arts Center would be located on Gaines Street between Bronough and Duval Streets. The Performing Arts Center is estimate to generate approximately \$85 million in new jobs, construction, and new tax revenues. The project may sustain an economic impact of \$10.6 million annually in sales and revenues once operational.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
\$30 million	Vertical Infrastructure
	Gateways

Florida Center of Performing Arts and Education



Quadrant: 4

Project #21



Previous Project Information for:

Project #21

Florida Center of Performing Arts and Education

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Performing Arts Center (*Previously Project #66*)

Proposed Sales Tax Projects by the Community

Project Name: Performing Arts Center (Project #66)

Project Themes:

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input checked="" type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

In the past year, the Florida Center for the Performing Arts and Education has presented to the Sales Tax Committee three times, most recently on Thursday, March 14, 2013. The project description for this project was developed using the materials provided by the Florida Center for the Performing Arts and Education. For a full copy of the report, please click here:

<http://leoncountyfl.gov/PerformingArts.pdf>

This project will provide for concert, dance, and theatre productions, including traveling entertainers, Broadway shows, and Distinguished Lecture Series. Currently, the project is being resized and re-priced with the assistance of Gilchrist Ross Crowe, the architectural firm that did the remodel of Ruby Diamond on the Florida State University Campus. It is estimated that the construction costs will be lowered and will have approximately 1,700 seats and an active lobby for the community. The Performing Arts Center would be located on Gaines Street between Bronough and Duval Streets.

The Florida Center for the Performing Arts and Education is requesting \$30 million from the proceeds of the sales tax. These funds will go toward the anticipated \$50 million construction cost currently being revised by consultants. \$10 million would come from private donations, corporate sponsorships, and grants. The balance of \$10 million would come from CRA funds and the bed tax money that has accumulated for the proposed performing arts center.

Project Map:

N/A

SALES TAX PROJECTS

Project Number: 22
Project Name: Monroe-Adams Corridor Placemaking

Total Project Cost: \$7 million

Total Project Cost Notes

Executive Project Summary

This project implements major elements of the Monroe-Adams Corridor Placemaking Action Plan and includes the following objectives:

Build Place

- Make safe, comfortable streets for pedestrians, transit users, and cyclists, and a suitable gateway to the Capital City
- Improve the appearance, comfort and safety of public streets and private property
- Encourage urban infill and mixed-use buildings.

Promote Place

- Inform the public of business incentives, redevelopment assistance, Codes, and Code Enforcement
- Support local business and bring in new business.

Make Places Within

- Implement an initial Placemaking Project (in progress, Palmer Avenue)
- Follow with similar improvements to all east-west streets in the Corridor

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Detail Project Description

Monroe Street Medians (Oakland to Magnolia) = \$2 million

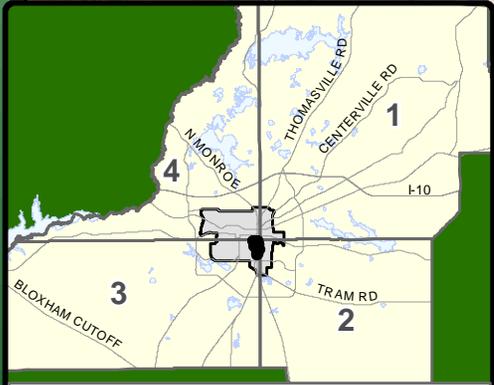
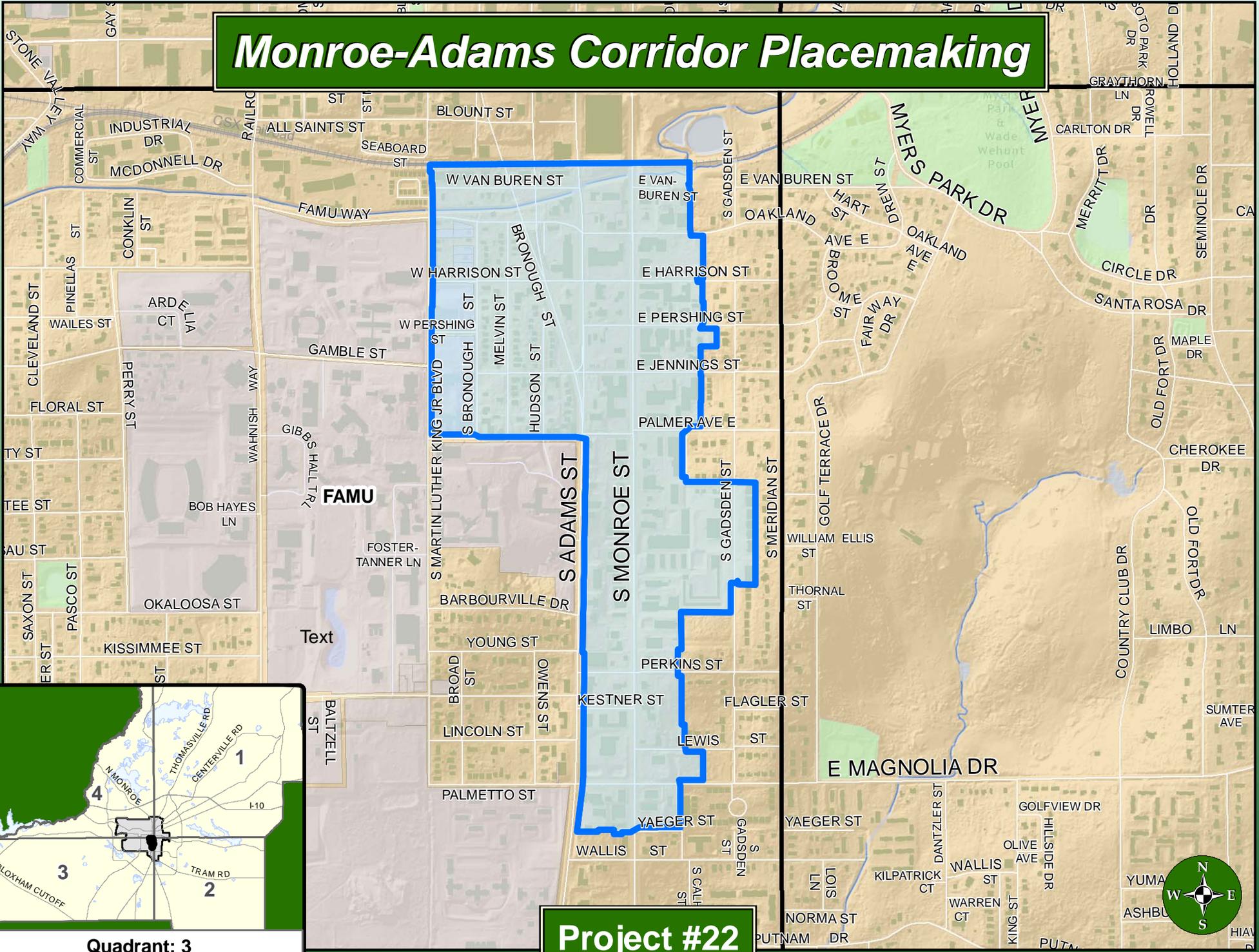
Streetscape Improvements (similar to Palmer Avenue), with sidewalks, lighting, signage, landscaping, underground utilities, and street parking (where possible)

- Harrison Street = \$1 million
- Pershing Street = \$1 million
- Jennings Street = \$1 million
- Perkins Road = \$1 million
- Kestner Street = \$1 million

Cost By Themes

	Regional Mobility/Transportation
\$7 million	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Monroe-Adams Corridor Placemaking



Quadrant: 3

Project #22

Previous Project Information for:

Project #22

Monroe-Adams Corridor Placemaking

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Inspiring the Creative Class: Monroe-Adams Corridor
Placemaking (*Previously Project #26*)

Program
4b

Inspiring the Creative Class: Monroe-Adams Corridor Placemaking

Estimated Cost:
\$7 million

WHAT IS IT?

The Monroe-Adams Corridor Placemaking Plan includes the following objectives:

Build Place

- Make safe, comfortable streets for pedestrians, transit users, and cyclists, and a suitable gateway to the Capital City
- Improve the appearance, comfort and safety of public streets and private property
- Encourage urban infill and mixed-use buildings.

Promote Place

- Inform the public of business incentives, redevelopment assistance, Codes, and Code Enforcement
- Support local business and bring in new business.

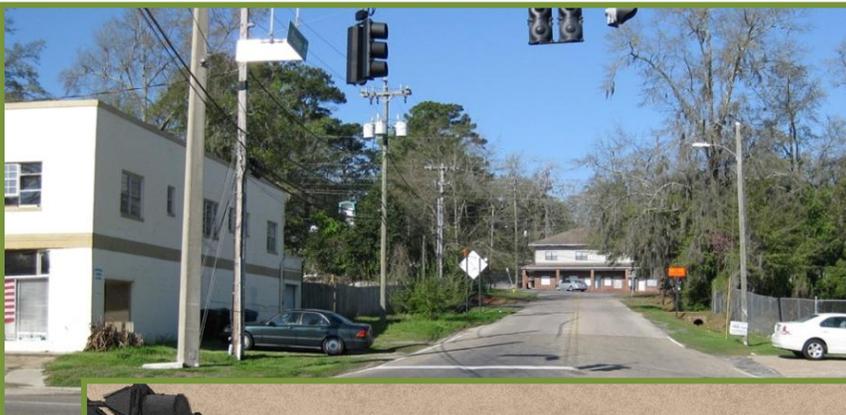
Make Places Within

- Implement an initial Placemaking Project
- Follow with similar improvements to all east-west streets in the Corridor

Evaluation Criteria:

- | | |
|--|---|
| Provides Regional Impact / Geographic Equity | ✓ |
| Enhances Mobility | ✓ |
| Supports Recreation and Quality of Life | ✓ |

- | | |
|--|---|
| Benefits the Environment | ✓ |
| Supports Economic Development / Revitalization | ✓ |
| On an Existing Master Plan or Blueprint List | ✓ |
| Leverages Other Funds | ✓ |



The initial Placemaking project selected by the Working Group and funded by the City Commission is the rebuilding of Palmer Avenue, a major connection between residents east of Monroe and the FAMU Campus. Construction begins Fall 2012.



WHY IS IT IMPORTANT?

Benefits the Environment – The proposals provide infrastructure that is essential for creating a walkable, mixed-use district. The projects add considerable tree canopy that provides stormwater retention benefits. This project also supports redevelopment and infill, which reduces vehicle miles traveled. It also can preserve greenfields from development.

Provides Regional Impact/Geographical Equity – The Placemaking package of projects distributes investment throughout the community. In the Monroe-Adams Corridor, the projects primarily provide local benefits through streetscape beautification, increased public parking, and improved pedestrian connectivity, but the South Monroe Street enhancements provide a gateway for all citizens and visitors to the capital city.

Enhances Mobility – The projects enhance mobility considerably. The cross street upgrades will increase public parking, cover open ditches and add sidewalks, and increase pedestrian comfort with shade trees and pedestrian scale lighting. The pedestrian related improvements improve access to transit stops. These improvements will comply with tenets of universal design.

Supports Recreation and Quality of Life – The streetscape enhancements and medians will improve the quality of life by expanding areas for safe pedestrian activity and improving area aesthetics.

Supports Economic Development/Revitalization – The streetscape enhancements and medians on both segments will have economic benefits for area retailers by making it safe pedestrians to venture beyond a single business establishment. The public infrastructure can spur reinvestment and development.

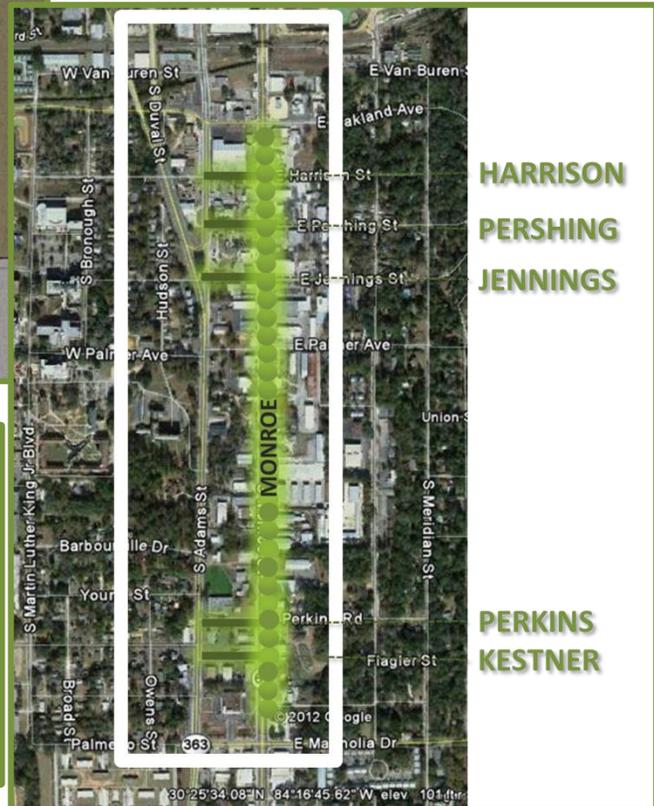
On an Existing Master Plan or Blueprint List – The proposed projects are included in the Monroe-Adams Corridor Placemaking Action Plan, and the CRTPA is conducting an access management study of the Monroe Street median concept.

Leverages Other Funds – CRA funds are available for use on the west side of Monroe Street, which includes each cross street upgrade. Furthermore, the CRTPA is currently conducting an access management (medians) study along Monroe Street and should be a funding partner on future implementation.



Proposed streetscape enhancements for S. Monroe St.

Below is a proposal to do similar streetscaping to the five east/west streets as is being done for Palmer, as well as median installations for South Monroe.



Top Priority Future Projects	Estimated Cost
Streetscape Improvements (e.g., Palmer Ave)	
▪ Harrison Street	\$1 million
▪ Pershing Street	\$1 million
▪ Jennings Street	\$1 million
▪ Perkins Road	\$1 million
▪ Kestner Street	\$1 million
Monroe Street medians (Oakland to Magnolia)	\$1.9 million
TOTAL Needed <small>[potential property acquisition not included]</small>	\$7 million

SALES TAX PROJECTS

Project Number: 23

Project Name: Orange/Meridian Placemaking

Total Project Cost: \$4.1 million

Total Project Cost Notes

This project cost includes right-of-way costs for stormwater improvements; SuperStop relies on independent site development, or will require total \$1 million as stand-alone project.

Executive Project Summary

This Orange/Meridian Placemaking project revitalizes the commercial area from Orange Avenue north to the Town South shopping center. A StarMetro SuperStop will be constructed with covered seating and bus bays at the intersection of Orange Avenue and Meridian Road to support this high ridership area. The East Drainage Ditch improvements enhance stormwater management for new and existing development. The existing stormwater facility south of Orange Avenue will be incorporated into a park-like amenity for the area.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input checked="" type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

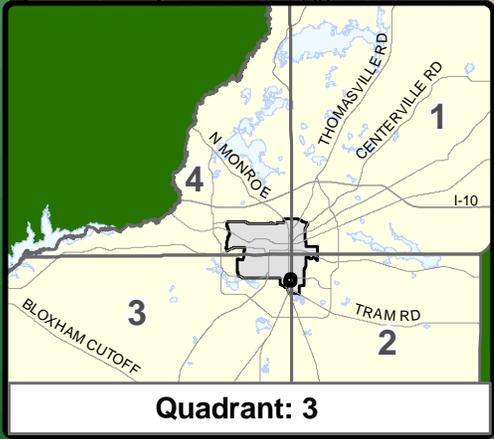
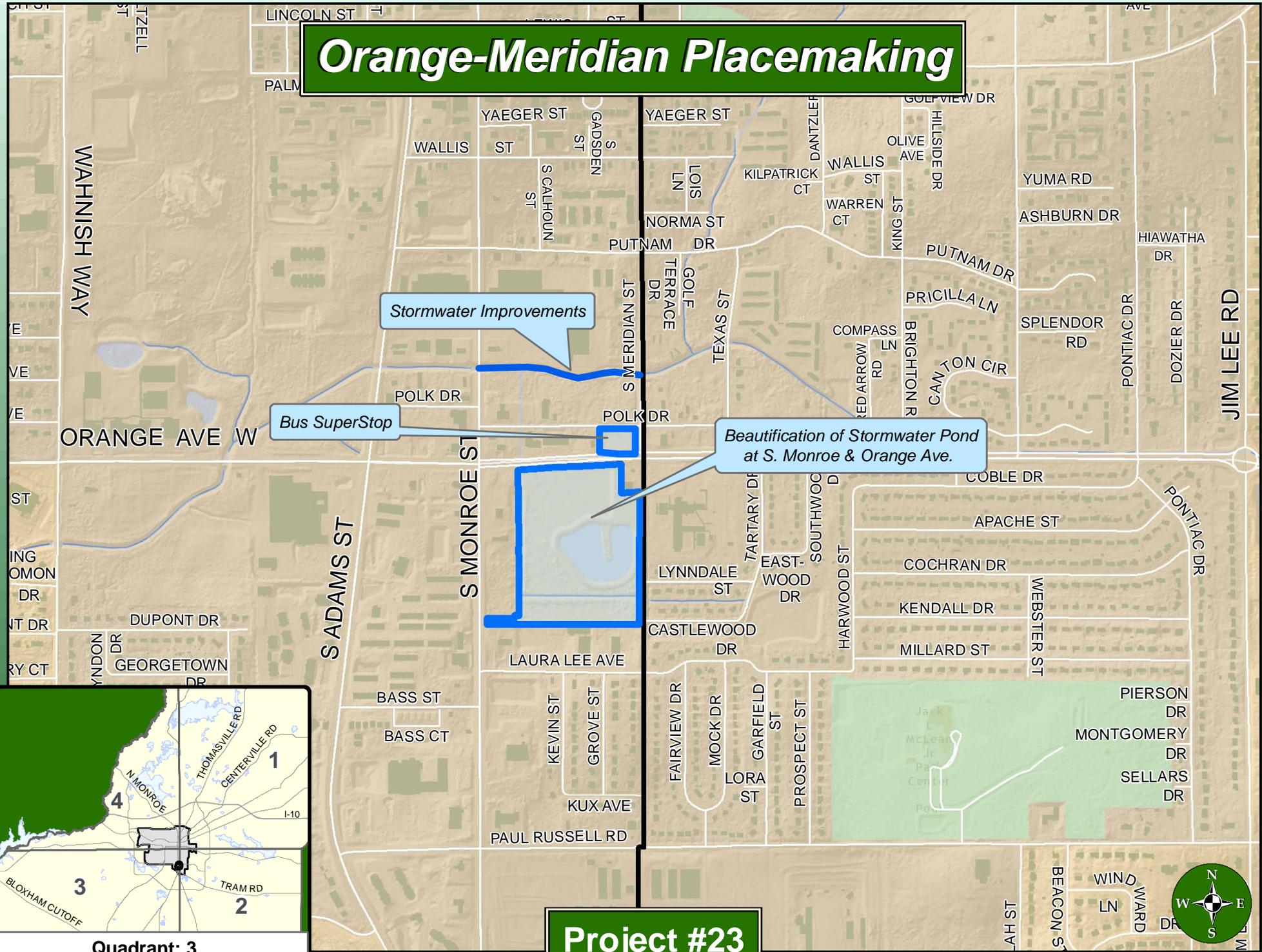
The Orange/Meridian Placemaking project proposed improvements include the following:

- SuperStop will include two bus bays each on Orange Avenue and Meridian Road. Off street parking is provided, and covered seating will extend from the roof-line of a building constructed at this location.
- Stormwater improvements along the East Drainage Ditch include treatment facilities for new development and enclosure of a portion of the ditch to improve access between the Town South shopping center and the new development. Flood level reductions will allow for new development north of Polk Drive.
- Compensating floodplain south of Orange Avenue will be incorporated into a park-like amenity with new plantings and walking trails.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
\$3.1 million	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
\$1 million	Vertical Infrastructure
	Gateways

Orange-Meridian Placemaking



Project #23

Previous Project Information for:

Project #23

Orange/Meridian Placemaking

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Beautification of the Stormwater Pond at S. Monroe and Orange Avenue (*Previously Project #46*)

Proposed Sales Tax Projects by the Community

Project Name: Beautification of the Stormwater Pond at S. Monroe and Orange Avenue (Project #46)

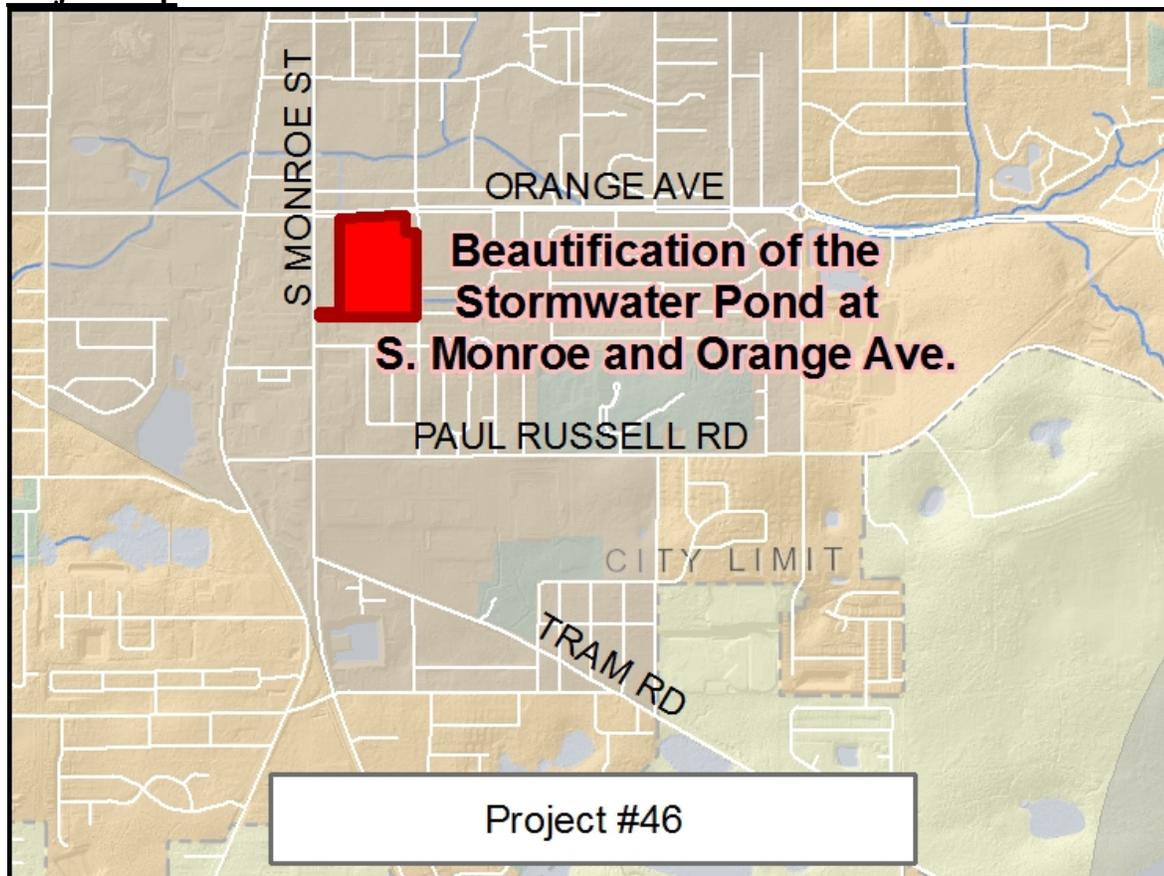
Project Themes:

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

This project would enhance the appearance of the compensating floodplain created on the south side of Orange Avenue between South Monroe Street and Meridian Road. The area would continue to receive stormwater to reduce flooding in the adjacent neighborhoods, but a variety of trees, shrubs and groundcovers would be planted to provide a park-like appearance.

Project Map:



SALES TAX PROJECTS

Project Number: 24
Project Name: Beautification and Improvements to the Fairgrounds
Total Project Cost: \$12 million
Total Project Cost Notes

Executive Project Summary

Currently, the Fairgrounds site is significantly underutilized in terms of uses and intensities. This project intends to provide the necessary infrastructure that will allow the Fairgrounds activities to be repositioned on the site while allowing for a mixture of land uses and intensities.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input checked="" type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

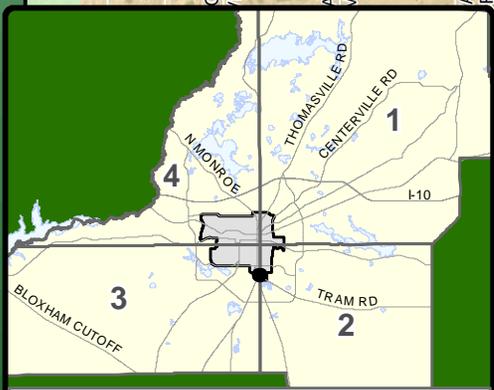
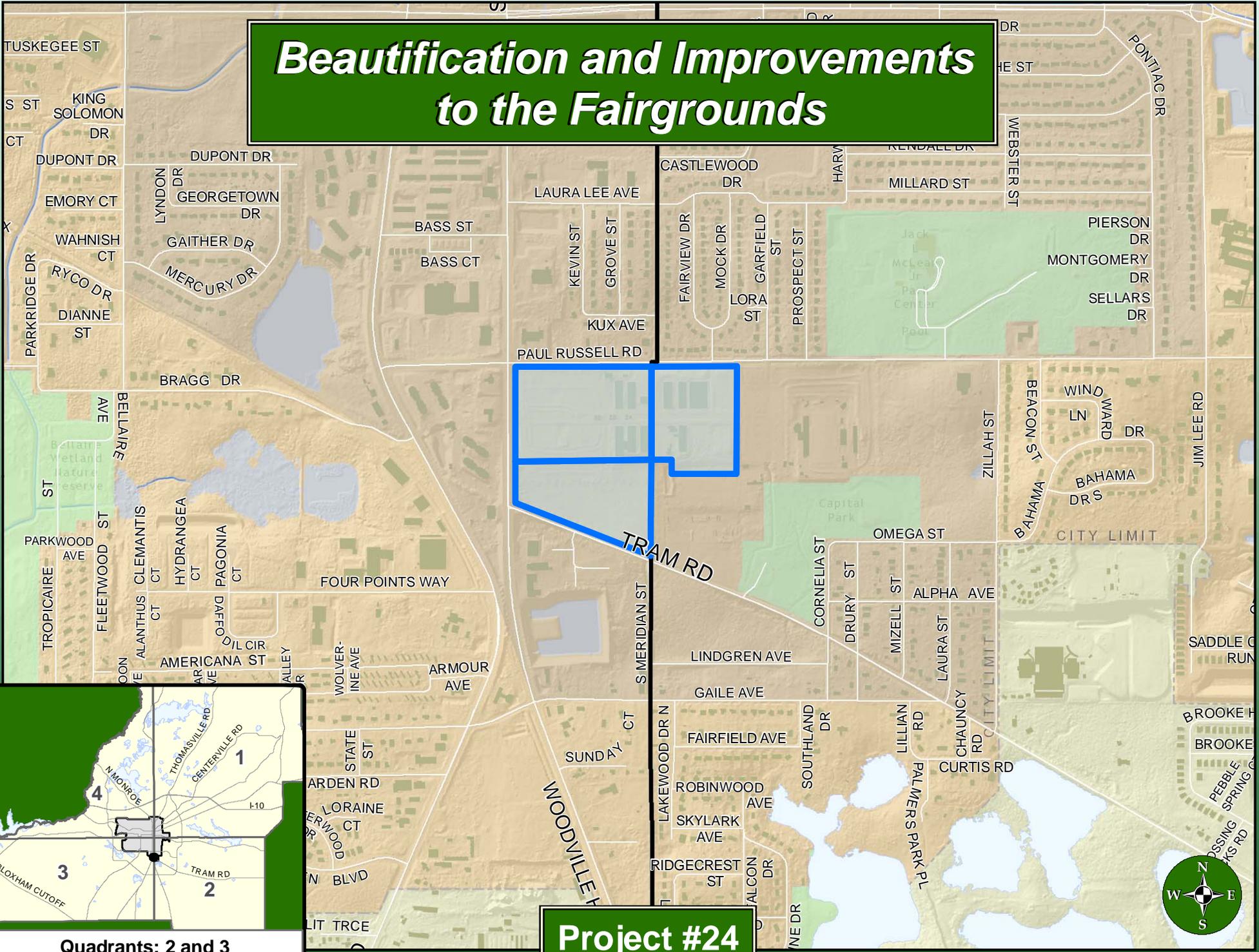
Detail Project Description

The project is proposed to being completed in two phases. Phase One of this project would be to reconfigure the fairgrounds on the site. Phase Two would be to incorporate the necessary infrastructure on the site that will allow for redevelopment activities to occur strategically as envisioned by the Board of County Commissioners in working with the surrounding neighborhoods.

Cost By Themes

	Regional Mobility/Transportation
\$12 million	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Beautification and Improvements to the Fairgrounds



Quadrants: 2 and 3

Project #24



Previous Project Information for:

Project #24

Beautification and Improvements to the Fairgrounds

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Beautification and Improvements to the Fairgrounds (*Previously Project #45*)

Proposed Sales Tax Projects by the Community

Project Name: Beautification and Improvements to the Fairgrounds (Project #45)

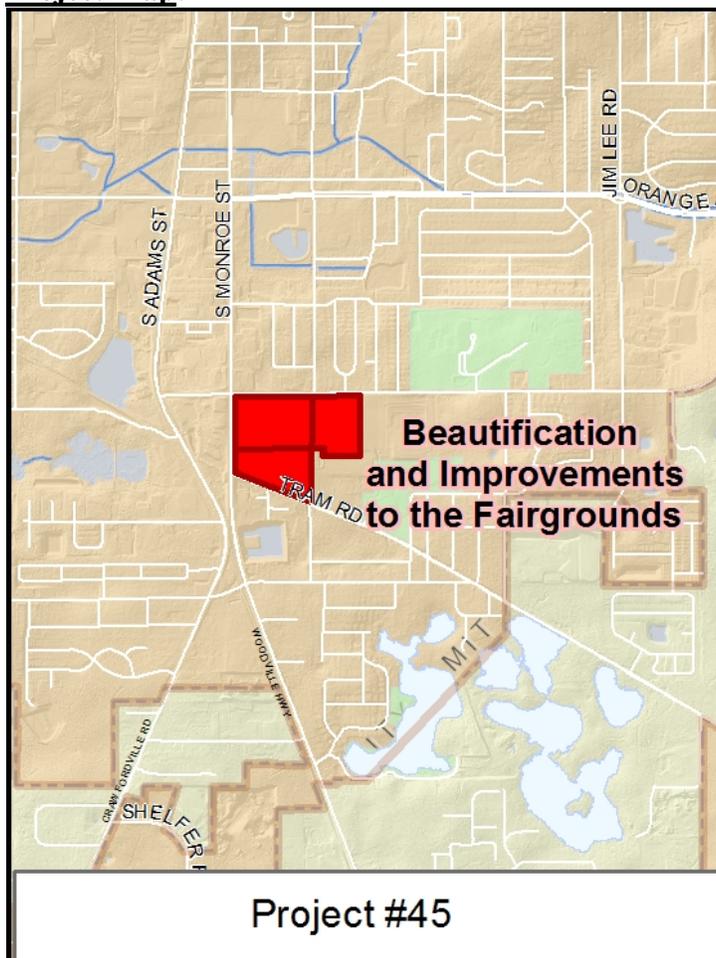
Project Themes:

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input checked="" type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Project Description:

Currently, the Fairgrounds site is significantly underutilized in terms of uses and intensities. This project intends to provide the necessary infrastructure that will allow the Fairgrounds activities to be repositioned on the site while allowing for a mixture of land uses and intensities. Phase One of the project would be to reconfigure the fairgrounds on the site. Phase Two would be to incorporate the necessary infrastructure on the site that will allow for redevelopment activities to occur strategically as envisioned by the Board of County Commissioners in working with the surrounding neighborhoods.

Project Map:



SALES TAX PROJECTS

Project Number: 25

Project Name: Tennessee Street Beautification and Pedestrian Safety Enhancement

Total Project Cost: TBD

Total Project Cost Notes

The cost estimate ranges from \$5M to over \$100M depending on the ultimate project scope. The current space available for improvements along Tennessee Street is extremely dense. At the low end of the estimate, the project scope would mirror what FDOT is currently proposing as part of the scheduled 2014 resurfacing of the roadway. The upper estimate, the project scope would entail the purchase of adjacent properties so as to accommodate the desired improvements of wider sidewalks, enhanced pedestrian crossings, enhanced transit service, bike facilities and landscape amenities along the Tennessee Street corridor between Ocala Road and Monroe Street.

Executive Project Summary

Tennessee Street is in the heart of the City's 18 square mile Mobility District, where the primary goal is to create a safe and desirable environment for pedestrian, cyclists, and transit users. A project to enhance the mobility along Tennessee Street can vary greatly in scope depending on how the different modes are accommodated. Existing Tennessee Street is a densely developed facility and very constrained by its right-of-way. Any improvements within this right-of-way would be limited to a reallocation of the existing space, thus greatly limiting the opportunity for beautifying the corridor.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

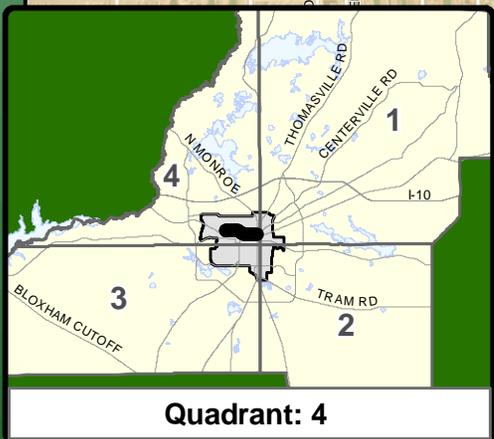
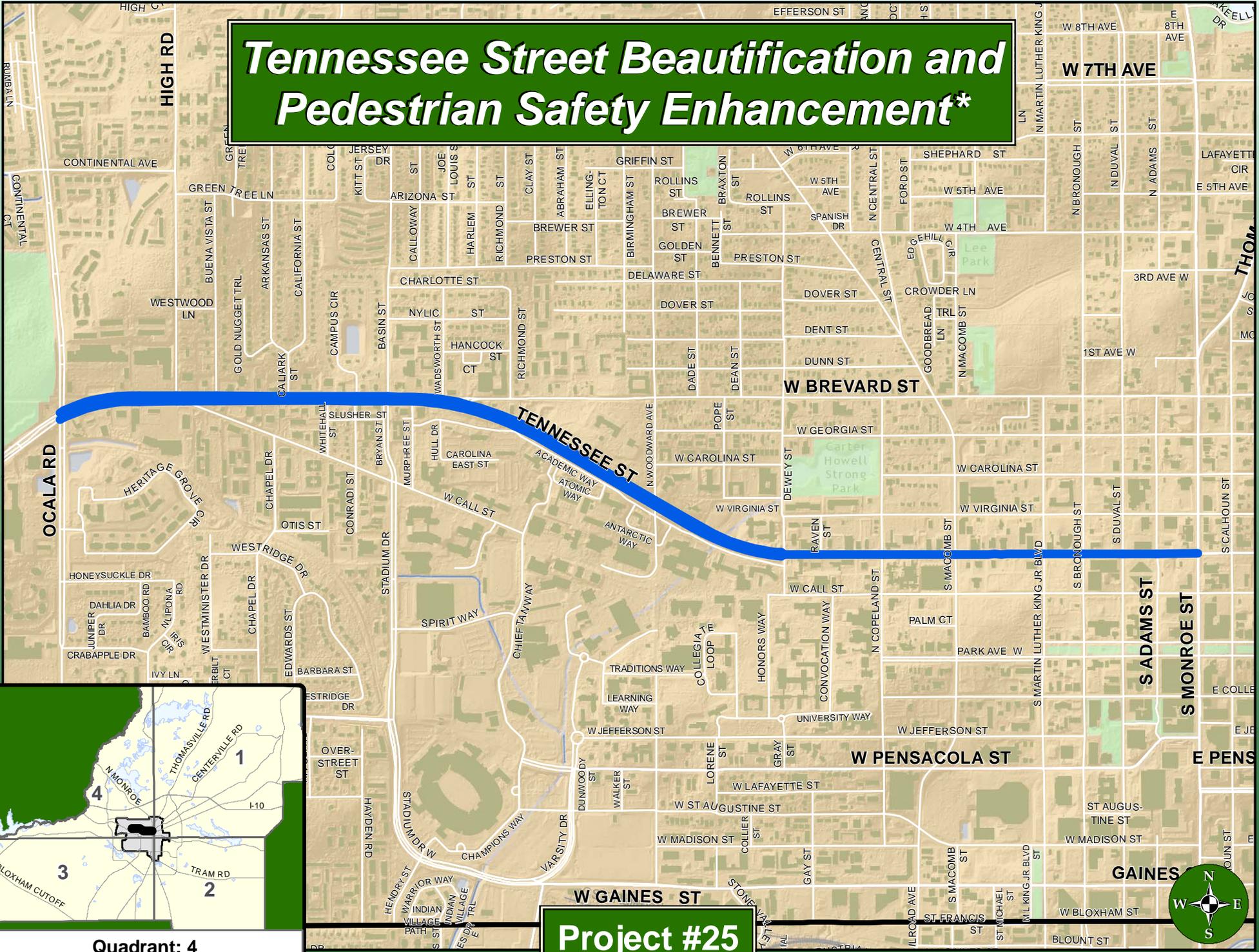
Detail Project Description

Tennessee Street is in the City's multi-modal district, a district within which modal choices are made more attractive and more readily available. The existing right-of-way width of Tennessee Street is such that it greatly restricts the space available for adding such amenities as wider sidewalks, bike lanes and street side landscaping (all of which avail and help promote alternative modes of transportation). The scope of the proposed project will vary greatly on whether additional properties are to be acquired to accommodate the desired amenities.

Cost By Themes

20%	Regional Mobility/Transportation
20%	Sense of Community
	Environmental/Water Quality
20%	Connectivity
20%	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
20%	Gateways

Tennessee Street Beautification and Pedestrian Safety Enhancement*



Quadrant: 4

Project #25



Previous Project Information for:

Project #25

Tennessee Street Beautification and Pedestrian Safety Enhancement

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Tennessee Beautification and Pedestrian Safety Enhancement
(*Previously Project #37*)

Proposed Sales Tax Projects by the Community

Project Name: Tennessee Beautification and Pedestrian Safety Enhancement (Project #37)

Project Themes:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Project Description:

The project consists of developing a program to beautify Tennessee Street (US 90), from Monroe Street to Ocala Road, and add pedestrian enhancements where suitable. Preliminary analysis by City staff finds the following:

- Tennessee Street (US 90), is owned and operated by the Florida Department of Transportation (FDOT) and the section between Monroe Street to Ocala Road lies within the City’s Multi-modal Transportation District (MMTD). The primary goal of the MMTD is to create a safe and desirable environment for pedestrians, cyclists, and transit users. Beautification of the corridor and enhancing the pedestrian experience are desirable within the MMTD.
- The MMTD is currently densely developed and includes institutional developments. Tennessee Street (US 90) has a limited right-of-way which currently accommodates narrow sidewalks and narrow travel lanes. Therefore, any beautification along this corridor within the existing right-of-way can only be accommodated at the expense of existing sidewalks and/or travel lanes. Any beautification efforts outside the existing right-of-way would require the participation and cooperation of the adjacent private property owners, as staff does not envision buying additional right-of-way to accommodate beautification enhancements.
- With regards to the pedestrian enhancements, FDOT has recently conducted a Roadside Safety Audit in advance of a proposed resurfacing project of Tennessee Street slated for 2014. That report identifies a need for additional pedestrian crossings of Tennessee Street. FDOT is currently determining the location and type of crossing to be provided.

It is staff’s recommendation that if this project is to advance, the program should focus on developing partnerships with adjacent property owners to beautify Tennessee Street.

Project Map:



SALES TAX PROJECTS

Project Number: 26
Project Name: Northeast Connector Corridor
Total Project Cost: \$36.3 million

Total Project Cost Notes

The project cost does include right-of-way costs for Bannerman Road and Meridian Trail.

Executive Project Summary

The Northeast Connector Corridor project encompasses a holistic approach to transportation capacity improvements, regional mobility, connectivity to existing/proposed amenities, and enhances the water quality of the Bradfordville area. This project includes the widening of Bannerman Road (Thomasville to Tekesta) to four lanes as well as constructs a multi-use path along the entire roadway to connect to regional amenities. This roadway has been identified as a significant benefits project. To date the County has spent in excess of \$1 million on the preliminary design study and reached out to area residents for their comments on the proposed corridor study.

In addition, the project constructs the northern portion of the Meridian Road Trail which will connect to the future Orchard Pond Road south to the Lake Overstreet property. This connection provides an efficient route between the Northeast to the Northwest existing pedestrian and bike networks.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

The Northeast Connector Corridor Project proposed improvements include the following:

- Widen the roadway between Tekesta and Thomasville to four lanes. This road serves a large concentration of residents and residential communities as well as a thriving commercial activity center of retail stores, restaurants, and other neighborhood services.

This project widens the section between Tekesta Drive and Thomasville Road to four lanes. This section will have paved shoulders, provisions for future transit, center landscaped median and landscaped between the edge of pavement and right of way with gentle swales for water quality enhancements and preservation of the more rural character of the area. The current level of service for between Tekesta Drive and Thomasville Road is Level E but is forecast to deteriorate to Level F by 2035 without the proposed improvements. The adopted level of service standard for this segment is D. Following construction, this would achieve a Level of service B.

- Construct multi-use path with sidewalk extensions to Killlearn Lakes and Bull Headley boat landing at Lake Iamonia. As well as, construct multi-use paths and swales along Bannerman between Thomasville Road and Meridian Road to the Meridian Road Trail as identified in the Greenway Master Plan.
- Construct sidewalk on Beech Ridge Trail between Kinhega and Lawton Chiles Drive to complete pedestrian connectivity between the region and Chiles High School. This project will link residential and commercial areas to the Bradfordville Community Center, the Northeast Library, Chiles High School, and

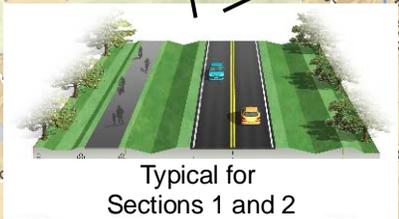
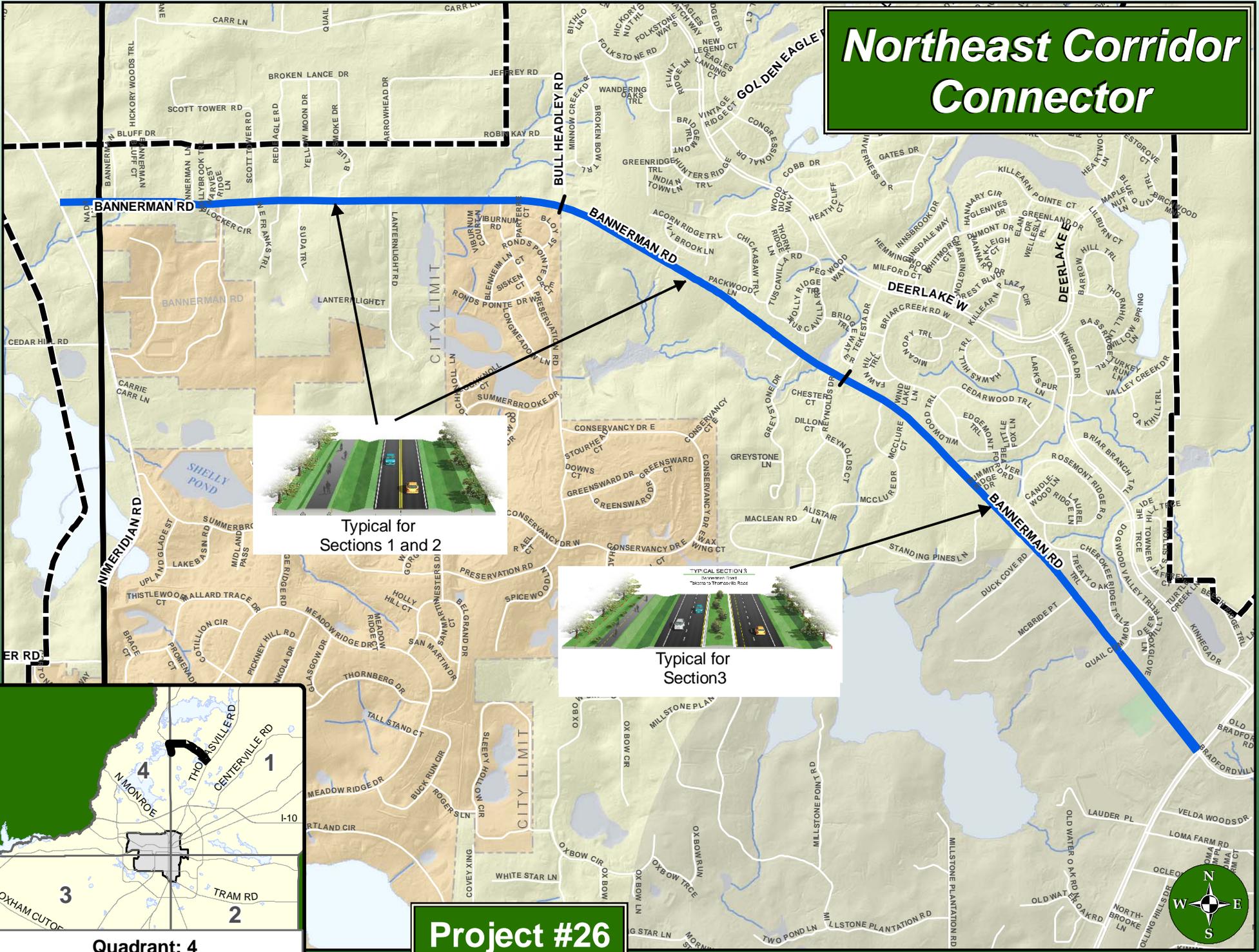
the Bull Headley Boat Landing for a comprehensive pedestrian and bicycle network.

- Interconnect existing internal neighborhood sidewalk network for maximum community connectivity.
- Construct Meridian Road Trail between Bannerman (and future Orchard Pond Road and bike routes) south to the Lake Overstreet property. The project provides interconnectivity for all modes from the proposed Orchard Pond toll road to the west, using the Meridian Road Trail and existing bike and sidewalk networks on Thomasville Road and Velda Dairy Road to the east. This will allow regional travel for bicycles and pedestrians and provide an efficient connection between the Northeast to the Northwest with interconnection to existing pedestrian and bike networks.
- Green Infrastructure enhancements include adding swales in the right of way to improve runoff water quality. The swales in the right of way increase water quality and preservation of the more rural character of the area.

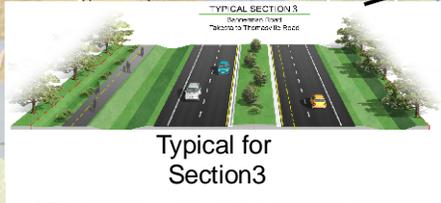
Cost By Themes

\$15.8 million	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
\$0.7 million	Connectivity
	Economic Vitality
\$19.8 million	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

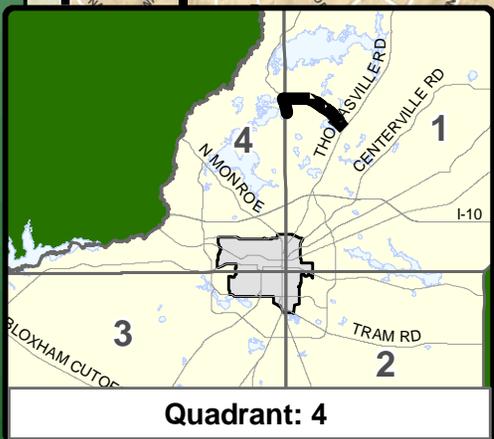
Northeast Corridor Connector



Typical for Sections 1 and 2



Typical for Section 3



Quadrant: 4

Project #26



Previous Project Information for:

Project #26

Northeast Connector Corridor

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Northeast Connector Corridor (*Previously Project #54*)

Ochlockonee River Basins: Stormwater Improvements,
Groundwater Protection, and Greenway Acquisition (*Previously
Project #7*)

Proposed Project for the Sales Tax Extension

Project #1: *Northeast Connector Corridor*

Estimated Project Cost: \$36.2 million

Project Criteria:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input checked="" type="checkbox"/> Water Quality Enhancements and Green Infrastructure |
| <input checked="" type="checkbox"/> Regional Mobility | <input type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input checked="" type="checkbox"/> Greenway Master Plan | <input checked="" type="checkbox"/> Transportation Capacity Improvements |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input type="checkbox"/> Complements BP2000 Project | |

Project Description:

The Northeast Connector Corridor project encompasses a holistic approach to transportation capacity improvements, regional mobility, connectivity to existing/proposed amenities, and enhances the water quality of the Bradfordville area. The project constructs the northern portion of the Meridian Road Trail which will connect to the future Orchard Pond Road. This connection provides an efficient route between the Northeast to the Northwest existing pedestrian and bike networks. This project includes the widening of Bannerman Road (Thomasville to Tekesta) to four lanes as well as constructs a multi-use path along the entire roadway to connect to regional amenities. This roadway has been identified as a significant benefits project. To date the County has spent in excess of \$1 million on the preliminary design study and reached out to area residents for their comments on the proposed corridor study.

The specific elements of the project include the following:

- Widen the roadway between Tekesta and Thomasville to four lanes
- Construct multi-use path with sidewalk extensions to Killlearn Lakes and Bull Headley boat landing at Lake Iamonia
- Construct multi-use paths and swales along Bannerman between Thomasville Road and Meridian Road
- Construct Meridian Road Trail and connect to multi-use path to Bannerman and future Orchard Pond Road and bike routes
- Interconnect existing internal neighborhood sidewalk network for maximum community connectivity
- Construct sidewalk on Beech Ridge Trail between Kinhega and Lawton Chiles Drive to complete pedestrian connectivity between the region and Chiles High School.
- Green Infrastructure enhancements include adding swales in the right of way to improve runoff water quality

Project Criteria Detail:

Comprehensive Plan: This project supports both the Mobility and Parks and Recreation elements of the Comprehensive Plan. The areas that the project complies with are listed below.

- Mobility:
 - Goal 1: Establishing and maintaining a safe, convenient, energy efficient, and environmental sound automobile, transit, bicycle, and pedestrian transportation system that is capable of moving people of all ages and abilities as well as goods.
 - Policy 1.2.2: Safe and convenient facilities for pedestrians, cyclists and transit users shall be evaluated for all new road and road widening projects. Specifically, all road projects, including resurfacing projects, shall be evaluated for the addition of bicycle lanes or paved shoulders, and transit shelters where they did not previously exist.

Project #1: Northeast Connector Corridor

Page 2

- Policy 1.3.4: Design, construct, and maintain transportation facilities to prevent flooding, minimize pollution, and maintain natural stormwater quantity, timing, rate, and direction of flow characteristics consistent with the adopted Stormwater Level of Service Standard.
- Objective 1.4: Connectivity and Access Management: Reduce vehicle trip demand, increase access and safety for cyclists and pedestrians, and preserve the integrity of the transportation system with effective connectivity and access management programs.
- Parks and Recreation Element Policy 1.5.3: The County shall help fund and develop a Greenways Trail System that provides the public opportunities to access a safe and convenient trail system in the unincorporated County. This system should incorporate public lands, right of way, easements on private lands, and open space designated lands to achieve this goal. The trail system will connect to the maximum extent possible existing and future residential areas to employment, education, and activity centers.

Regional Mobility Plan: This project supports the Capital Regional Transportation Authority's long range plan of facilitating the designation of an activity center (Bradfordville Activity Node) and promoting multimodal transportation and regional connectivity.

Greenway Master Plan: This project will connect multiuse paths along Bannerman to the Meridian Road Trail as identified in the Greenway Master Plan.

Connectivity: This project will link residential and commercial areas to the Bradfordville Community Center, the Northeast Library, Chiles High School, and the Bull Headley Boat Landing for a comprehensive pedestrian and bicycle network. The project provides interconnectivity for all modes from the proposed Orchard Pond toll road to the west, using the Meridian Road Trail and existing bike and sidewalk networks on Thomasville Road and Velda Dairy Road to the east. This will allow regional travel for bicycles and pedestrians and provide an efficient connection between the Northeast to the Northwest with interconnection to existing pedestrian and bike networks.

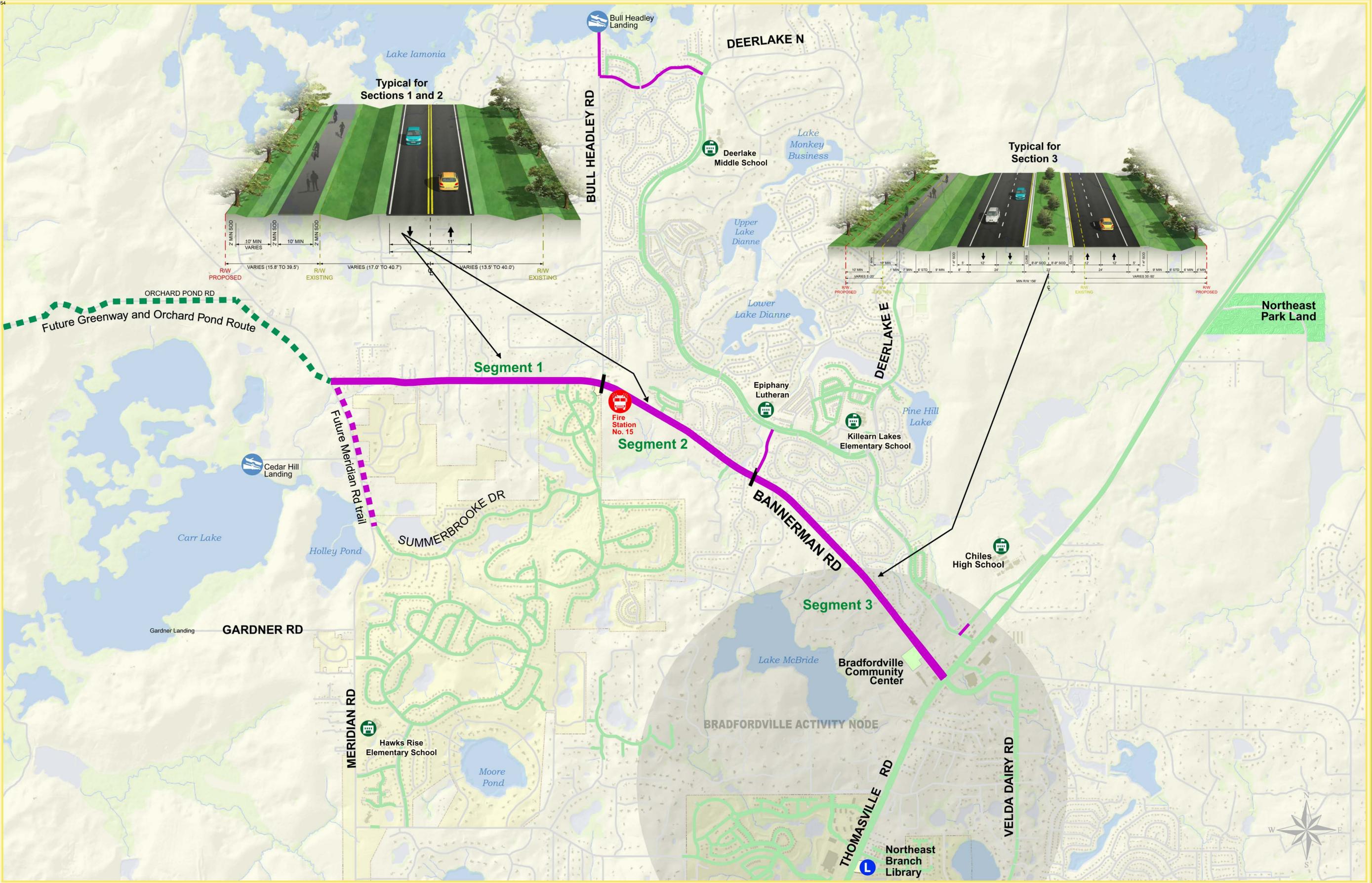
Complements Blueprint 2000 Project(s): There were not any Blueprint 2000 completed in the Northeast.

Water Quality Enhancements and Green Infrastructure: This project constructs the Meridian Road Trail, adds swales in the right of way of Bannerman Road, and creates multi-use paths. The swales in the right of way increase water quality and preservation of the more rural character of the area.

Stormwater/Sewer Capacity Improvements: N/A

Transportation Capacity Improvements: This road serves a large concentration of residents and residential communities as well as a thriving commercial activity center of retail stores, restaurants, and other neighborhood services.

This project widens the section between Tekesta Drive and Thomasville Road to four lanes. This section will have paved shoulders, provisions for future transit, center landscaped median and landscaped between the edge of pavement and right of way with gentle swales for water quality enhancements and preservation of the more rural character of the area. The current level of service for between Tekesta Drive and Thomasville Road is Level E but is forecast to deteriorate to Level F by 2035 without the proposed improvements. The adopted level of service standard for this segment is D. Following construction, this would achieve a Level of service B. Meridian Road to Bull Headley will remain at Level of Service C and Bull Headley to Tekesta Drive will remain at Level of Service D.



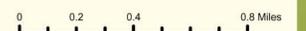
Northeast Connector Corridor

Legend

- Proposed Project
- Existing Sidewalks, Bike Routes
- Parks
- Major Lakes

Northeast Connector Corridor Project Criteria

<input checked="" type="checkbox"/> Comprehensive Plan	<input checked="" type="checkbox"/> Water Quality Enhancements and Green Infrastructure
<input checked="" type="checkbox"/> Regional Mobility	<input checked="" type="checkbox"/> Stormwater/Sewer Capacity Improvements
<input checked="" type="checkbox"/> Greenway Master Plan	<input checked="" type="checkbox"/> Transportation Capacity Improvements
<input checked="" type="checkbox"/> Connectivity	<input checked="" type="checkbox"/> Core Infrastructure
<input type="checkbox"/> Complements BP2000 Project	



This product has been compiled from the most accurate source data from Leon County and the City of Tallahassee. However, this product is for reference purposes only and is not to be construed as a legal document or survey instrument. Any reliance on the information contained herein is at the user's own risk. Leon County and the City of Tallahassee assume no responsibility for any use of the information contained herein or any loss resulting therefrom.

Blueprint 2000

Proposed Sales Tax Projects

Project Name: Ochlockonee River Basins: Stormwater Improvements, Groundwater Protection, and Greenway Acquisition (Blueprint 2000 Map 1)

Project Themes:

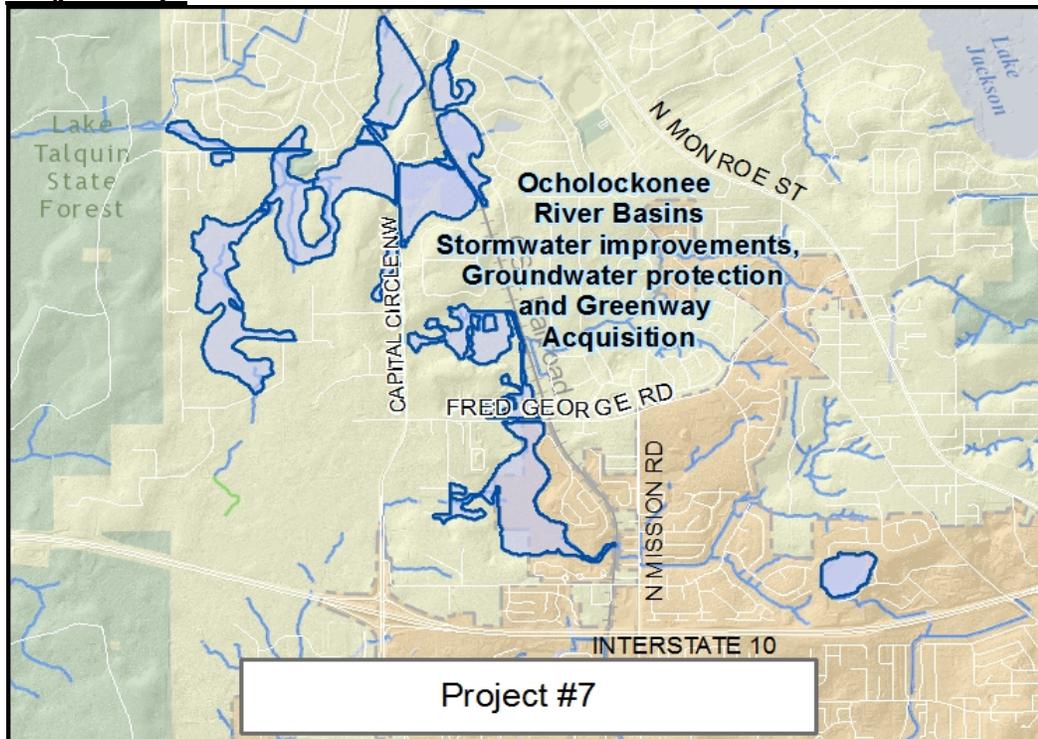
- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

The primary benefits of this project are greenway connections and protection of groundwater in the northwestern portion of Leon County. Greenway connections between the City’s Northwest Park, four County Parks (Fred George Park, Stoneler Road, Tower Road, and the Tower Road Ochlockonee River Landing) the Lake Talquin State Forest, and the Phipps-Overstreet greenbelt are proposed. This project will link northwestern Leon County to north Meridian Road via Old Bainbridge Road and the existing Orchard Pond Roadway. The project includes trail construction and trail crossings at roadways.

This project will improve ground water quality by restoring the hydrology of the Fred George Sink. Additionally, there are two flood prone areas in Map 1, Stoneler Road and Riverwood Road Swamps. Acquisition and restoration of the active, developed portions of these floodplains will improve water quality and provide passive storage and biological treatment of stormwater, minimizing costs for retrofit in the future. An environmentally sensitive area of old growth forest of about 75 acres off of Old Bainbridge Road is proposed for acquisition.

Project Map:



SALES TAX PROJECTS

Project Number: 27
Project Name: Orange Avenue Widening to Capital Circle SW
Total Project Cost: \$78.2 million

Total Project Cost Notes

In order create a corridor with consistent widths, this project begins widening Orange Avenue at Adams Street Intersection through Capital Circle Southwest (CCSW).

East Segment from Adams Street to Springhill Road (1.2 miles) cost = \$39.1 million. This segment has several commercial developments which would more than likely result in higher costs to acquire right of way.

West Segment from Springhill Rd. to CCSW (3.0 miles) cost = \$39.1 million.

Costs estimated by applying FDOT adjustment factor to 1998 PD&E cost.

Executive Project Summary

This project calls for the widening of Orange Avenue to four lanes between Adams Street and Capital Circle Southwest (CCSW), including bike lanes and sidewalks. In order create a corridor with consistent widths, this project begins widening Orange Avenue at Adams Street Intersection through CCSW. A multi-use path would run the length of the corridor, enhancing access to the St. Marks Bike Trail and the Capital Cascades Trail. The 1998 PD & E study would be updated to allow community input to the corridor design.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Detail Project Description

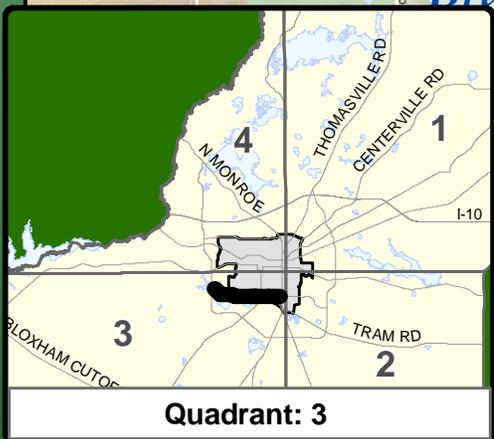
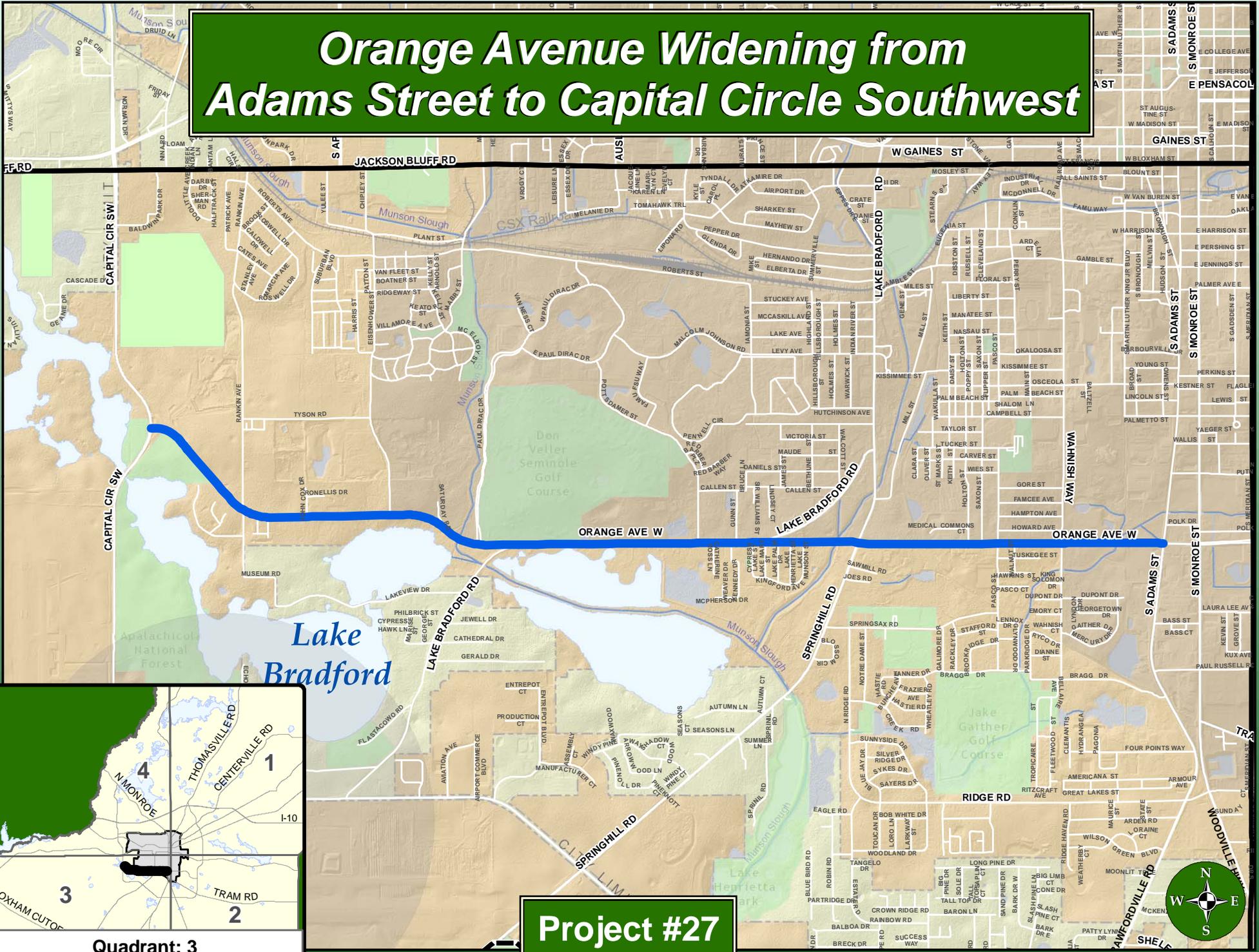
In addition to four-laning Orange Avenue, Lake Bradford Road would be realigned with Paul Dirac Drive (Innovation Park). New stormwater treatment facilities would be constructed to address existing roadway runoff. Multi-modal improvements will include transit, pedestrian and bicycle facilities. The 1998 PD & E study would be updated to address current conditions and design standards, as well as allow residents input to the proposed corridor design.

Orange Avenue east of Lake Bradford Road is designated a Gateway Route in the City Land Development Code.

Cost By Themes

\$78.2 million	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Orange Avenue Widening from Adams Street to Capital Circle Southwest



Project #27



Previous Project Information for:

Project #27
Orange Avenue Widening from Adams
Street to Capital Circle Southwest

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Orange Avenue Widening (*Previously Project #64*)

Proposed Sales Tax Projects by the Community

Project Name: Orange Avenue Widening (Project #64)

Project Themes:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

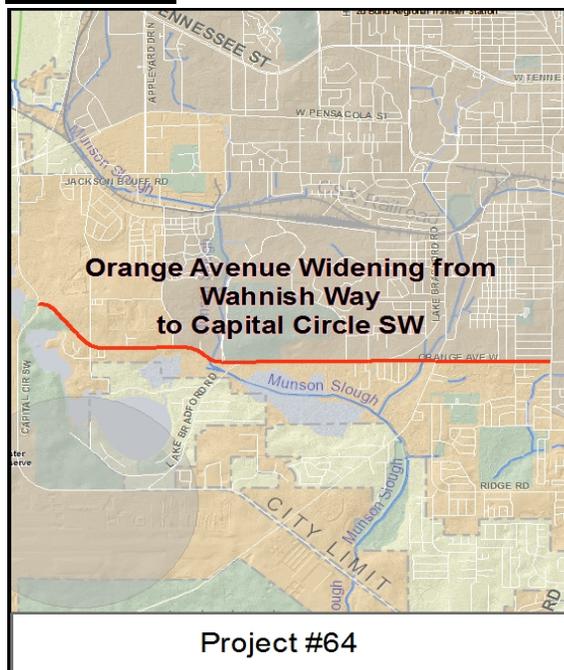
Project Description:

The Orange Avenue Widening project encompasses a holistic approach to transportation capacity improvements, regional mobility, connectivity to existing/proposed amenities, and enhances the water quality of the Southside area. The corridor will connect downtown commercial areas and FAMU to Capital Circle SW and Capital Circle SE. This connection also provides an efficient route between the existing pedestrian and bike networks, from Capital Circle SW to Capital Circle SE. This project widens Orange Avenue (Capital Circle SW to Wahnish Way) to four lanes and constructs a multi-use path along the entire roadway to connect to regional amenities. The Florida Department of Transportation completed a preliminary design study in 1998, which will require updating to address current conditions and design standards. At the time, the Department reached out to area residents for their comments on the proposed corridor study. The estimated cost to update the PD&E, complete engineering and permitting, acquire right-of-way, and construct the improvements is \$78.2 million.

The specific elements of the project include the following:

- Widen the roadway between Capital Circle SW and Wahnish Way to four lanes
- Realign Lake Bradford Road West to intersect with Paul Dirac Drive
- Construct multi-use paths along Orange Avenue between Capital Circle SW and Wahnish Way, enhancing access to the St. Marks Bike Trail and the Capital Cascades Trail
- Green Infrastructure enhancements include new stormwater facilities and swales in the right-of-way to improve runoff water quality

Project Map:



SALES TAX PROJECTS

Project Number: 28

Project Name: Welaunee Critical Area Plan Regional Infrastructure within the Canopy Project - Welaunee Boulevard and the Dove Pond Regional Stormwater Facility

Total Project Cost: \$18,500,000

Total Project Cost Notes

The total project cost will range between 16.5 and 20.5 million dollars. The projected costs (minus right-of-way) for a 4-lane Welaunee Boulevard within the Canopy project will range between 12 and 15 million dollars. The projected costs for constructing the Dove Pond Regional Stormwater Facility will range between 4.5 and 5.5 million dollars. Blueprint funds would be used to complete the construction of this facility. One-third of the project costs will be reimbursed as future construction occurs within the Dove Pond Basin of the Canopy project. As contemplated by the Welaunee Critical Area Plan, under this proposal two-thirds of the construction costs will be funded by local government via Blueprint funds.

Executive Project Summary

This project supports and facilitates a regional gateway in our community which has been planned and supported since the 1990's, and is currently being implemented. Welaunee Boulevard is a significant regional transportation facility and has been planned pursuant to the Tallahassee-Leon County Comprehensive Plan to connect to a future I-10 interchange and continue north to connect to other major roadways. The Dove Pond Regional Stormwater Facility has also been planned and supported since the 1990's, and has been designed and approved as a major flood control and water quality project and will prevent flooding downstream which in the past has negatively impacted infrastructure, property and homes. This proposal also includes an innovative, but voluntary, method for reimbursing Blueprint for the construction costs sales tax funding of these projects.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Detail Project Description

Background:

The Canopy project consists of approximately 505-acres and is subject to the Welaunee Critical Area Plan which was approved in 2002 by the City of Tallahassee and Leon County as part of the Land Use Element of the Tallahassee-Leon County Comprehensive Plan. (See attached Location Map). The Canopy project is subject to a Development Agreement with the City and is an approved PUD for the following major uses: 1,572 residential units; 162,927 square feet of office and retail; and 335,000 square feet of institutional. The Welaunee Critical Area Plan was a significant planning effort undertaken by the landowner, Powerhouse, Inc., and received broad public support through a citizens' Welaunee Community Advisory Group. The Welaunee property is 6,500 acres which in its entirety roughly forms the shape of a "Boot." (See attached Location Map). The western portion of the original Welaunee landholding lies south of I-10 and is approximately 937 acres. It is referred to as the Toe and includes the Canopy project, the City's project, and portions of the Miccosukee Canopy Road Greenway, which was acquired by the State of Florida in 1996 through a negotiated sale facilitated by the Trust for Public Land. (See attached Location Map). The eastern portion of Welaunee is the "Heel," fronting on U.S. 90, consists of approximately 903 acres and is also subject to the Welaunee Critical Area Plan. The portion of Welaunee north of I-10 and west of Miccosukee Road is the "Arch" which is approximately 4,660 acres. It is designated "Planned Development" under the current Tallahassee-Leon County Comprehensive Plan.

Welaunee Boulevard: A Critical Regional Roadway:

Welaunee Boulevard and the Dove Pond Regional Stormwater facility are planned regional infrastructure improvements and addressed in the Welaunee Critical Area Plan. The Critical Area Plan also provides for the connection of Welaunee Boulevard to a future I-10 interchange, which has been identified for more intensive study in the Florida Department of Transportation's recent update of its I-10 Master Plan. Welaunee Boulevard has been planned as a major regional roadway and is part of the approved Long Range Transportation Plan. The planned portion of Welaunee Boulevard in the Toe will provide significant traffic relief to both Centerville and Miccosukee designated canopy roads, as demonstrated by transportation studies conducted by Powerhouse during the critical area planning process and approved by the City and County in 2002.

The projected costs (minus right-of-way) for a 4-lane Welaunee Boulevard within the Canopy project will range between 12 and 15 million dollars. The construction of Welaunee Boulevard within and beyond the Canopy project will provide significant public benefits to this region of our community and further support the objective of our Comprehensive Plan to construct an I-10 interchange in order to create this important gateway. Blueprint funds would be used to front-end the building of this 4-lane segment of Welaunee Boulevard and would be reimbursed over time as development occurs in the Canopy project. The framework for accomplishing this proposal could be addressed through a development agreement or using a special district together with a negotiated agreement between the City, special district, and property owners. The conveyance of the right-of-way within the Canopy project has already been addressed in the form of a development agreement with the City. This project will ultimately result in no costs to the taxpayers as a result of reimbursement of the front-end funding over time as the Canopy project is developed. This concept could be enlarged to include the planned development on the City's adjacent property in the Toe, which was acquired from Powerhouse in 2006.

Dove Pond Regional Stormwater Facility:

Dove Pond is located within the Canopy project and within the City of Tallahassee. The Dove Pond Regional Stormwater Facility is a major regional flood control project which has been planned and studied since the late 1990's starting with the County-commissioned Tri-Basin Study dated April 1999. This regional stormwater project enjoyed broad support by the Welaunee Community Advisory Group and downstream neighborhoods. It was incorporated into the 2002 Welaunee Critical Area Plan, which provides that local government should fund two-thirds of the project costs because approximately two-thirds of the flood control benefits from the facility would accrue to off-site property in the unincorporated County. Over 320 developed acres (with no adequate stormwater management) in the unincorporated County upstream and north of Dove Pond drain through Dove Pond and to the downstream basin also located in the County, which has experienced historic negative flooding, including roads, property and homes. The Dove Pond Regional Stormwater Facility has been designed and approved as part of the Canopy project, and if constructed, will reduce downstream flooding. The facility will provide water quality and flood control benefits by taking on the post-development stormwater from the upstream 320 acres, the Canopy project (including Welaunee Boulevard), and pre-development stormwater within the remainder of the Toe located within the Dove Pond Basin. The Dove Pond Regional Stormwater Facility will also serve to facilitate a portion of the planned network of open space, parks and trails throughout the Canopy project and connecting to the Miccosukee Canopy Road Greenway.

The projected costs for constructing the Dove Pond Regional Stormwater Facility will range between 4.5 and 5.5 million dollars. Blueprint funds would be used to complete the construction of this facility. One-third of the project costs will be reimbursed as future construction occurs within the Dove Pond Basin of the Canopy project. As contemplated by the Welaunee Critical Area Plan, under this proposal two-thirds of the construction costs will be funded by local government via Blueprint funds. Therefore, this project will cost between \$2,999,700 and \$3,666,300 since one-third of these costs will ultimately be

repaid over time as the Canopy project is developed.

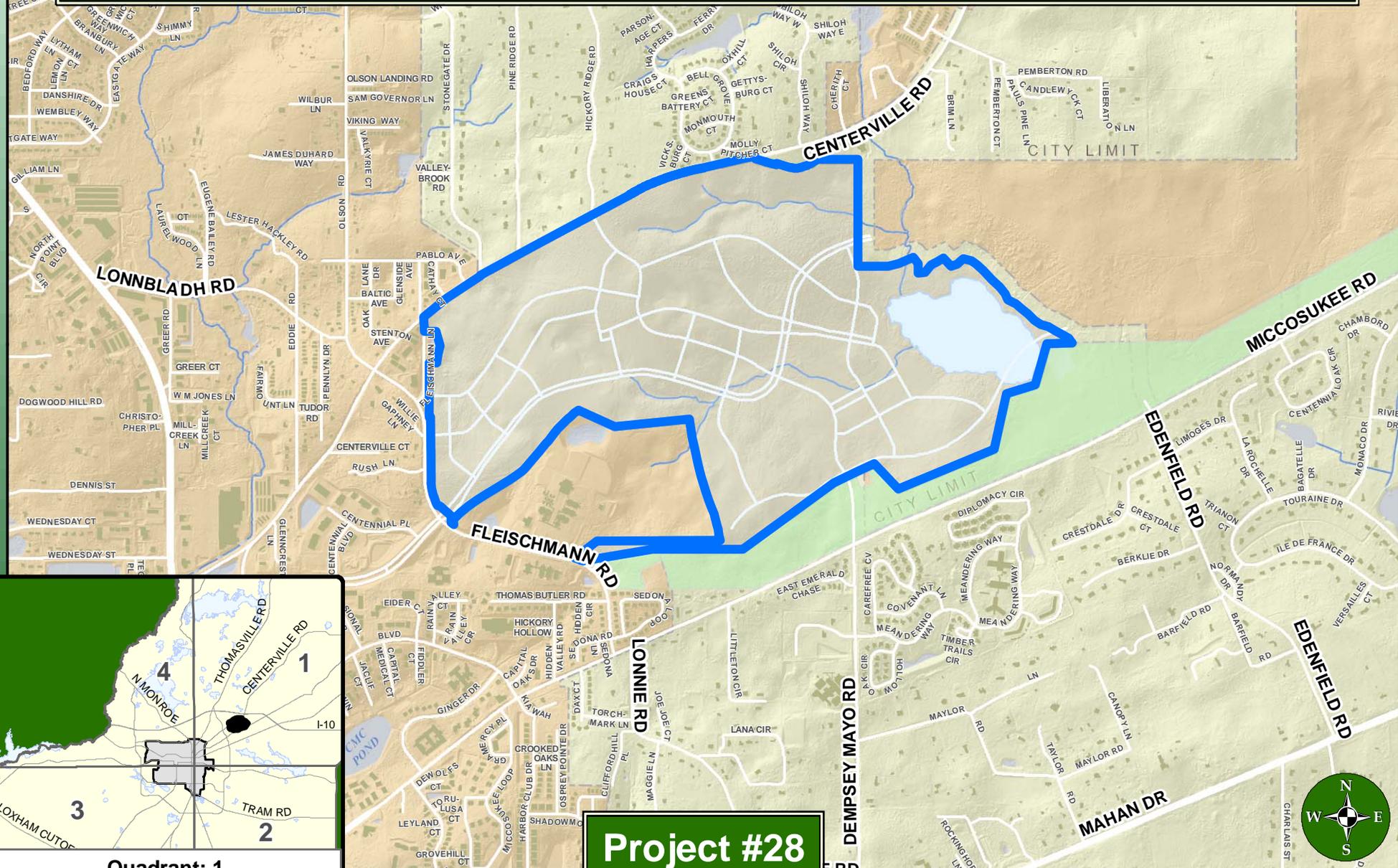
Conclusion:

In sum, this project, and the associated costs, furthers the Blueprint themes. Welaunee Boulevard supports regional mobility/transportation, connectivity, economic vitality, and gateways. The Dove Pond Regional Stormwater Facility promotes environmental and water quality, parks/ecotourism, and vertical infrastructure. This project supports and facilitates a regional gateway in our community which has been planned and supported since the 1990's.

Cost By Themes

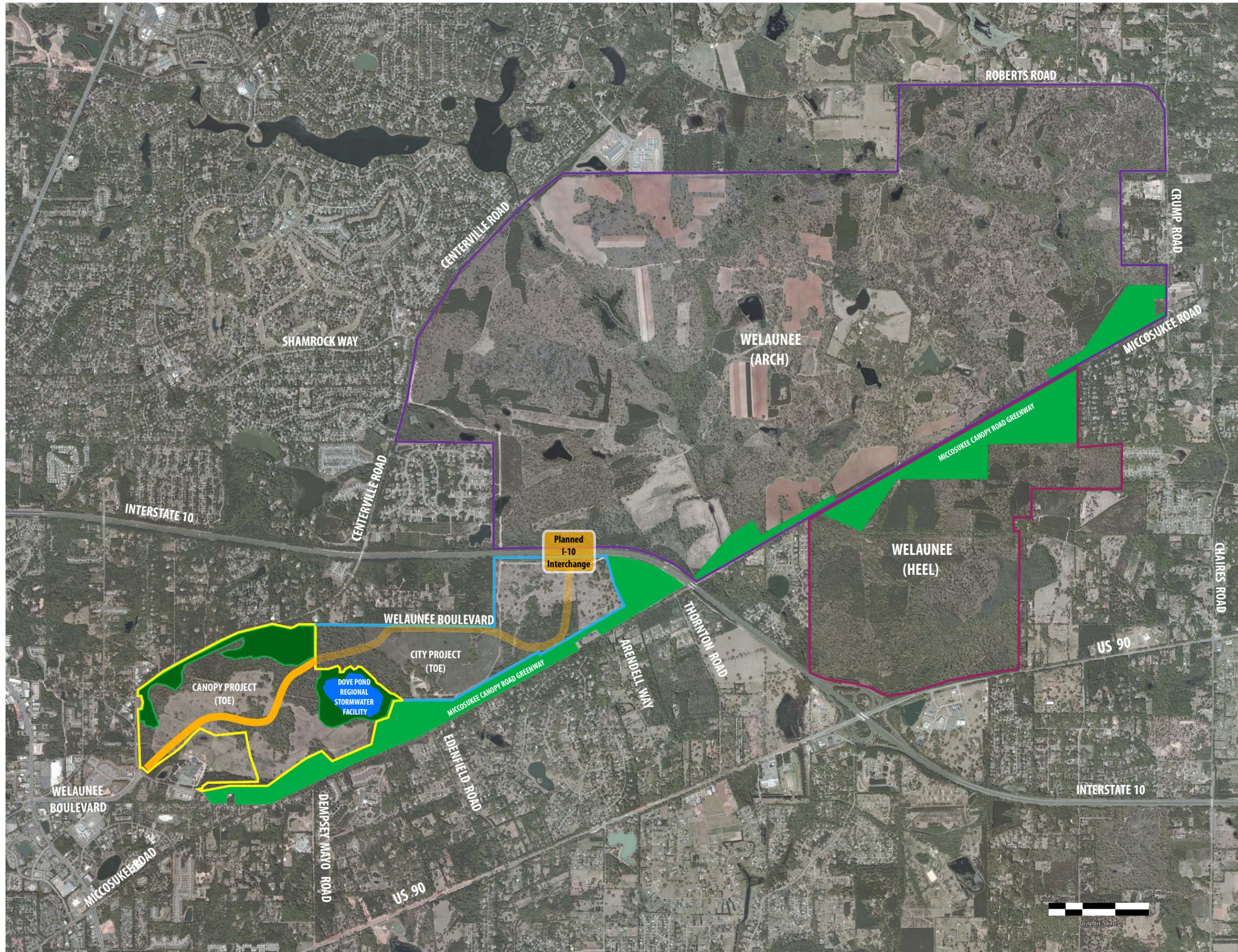
\$13,500,000	Regional Mobility/Transportation
	Sense of Community
\$5,000,000	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Welaunee Critical Area Plan Regional Infrastructure within the Canopy Road Project - Welaunee Boulevard and the Dove Regional Stormwater Facility



Project #28

Quadrant: 1



LEGEND

-  Canopy Project Boundary
-  City Project Boundary
-  Welaunee Arch Boundary
-  Welaunee Heel Boundary
-  Miccosukee Canopy Road Greenway (State of Florida)
-  Welaunee Boulevard (Canopy Project)
-  Welaunee Boulevard (City Project)
-  Proposed Connecting Open Space (Canopy Project)
-  Dove Pond Regional Stormwater Facility

LOCATION MAP

LEON COUNTY, FLORIDA

Date: April 10, 2013
 SOURCE: USGS
 DATE FLOWN: 2010



Previous Project Information for:

Project #28
Welaunee Critical Area Plan Regional
Infrastructure within the Canopy Project -
Welaunee Boulevard and the Dove Pond
Regional Stormwater Facility

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Canopy Planned Unit Development Infrastructure: Welaunee Boulevard (*Previously Project #36*)

Proposed Sales Tax Projects by the Community

Project Name: Canopy Planned Unit Development Infrastructure: Welaunee Boulevard (Project #36)

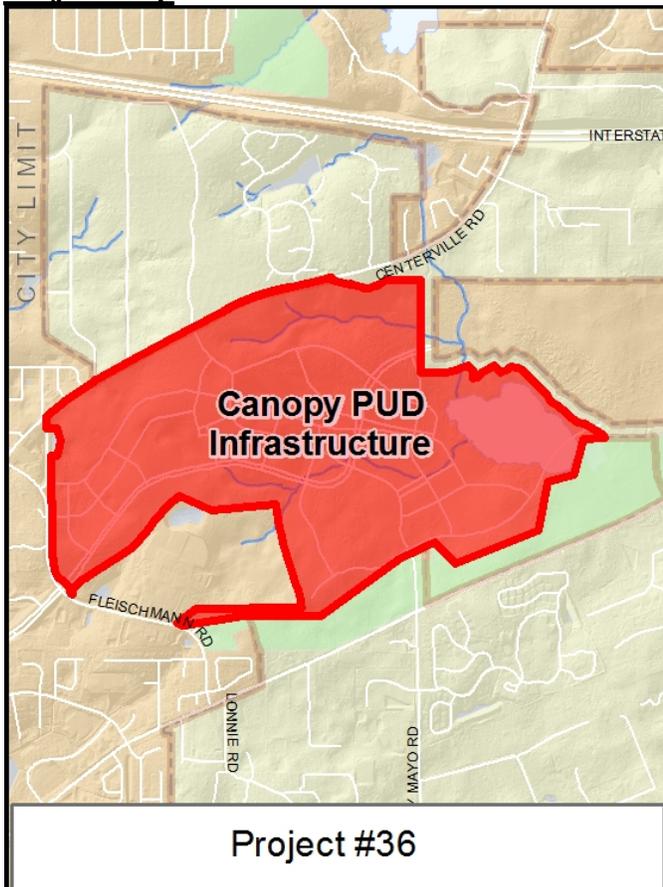
Project Themes:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

The Canopy Planned Unit Development (PUD) calls for the construction of Welaunee Boulevard, from Fleischman Road to the City owned property in the vicinity of Dove Pond. This project would consist of establishing a Special Improvement District (SID) for the construction of this portion of Welaunee Boulevard. The SID would fund the improvements with proceeds from the Sales Tax Extension and subsequently assess the PUD developer an equivalent amount over time. City staff recommends further exploring this project, as it represents an innovative financing approach to privately fund roadway construction.

Project Map:



SALES TAX PROJECTS

Project Number: 29

Project Name: StarMetro - Enhanced Infrastructure

Total Project Cost: 12,250,000

Total Project Cost Notes

Making every stop ADA Compliant - \$1,700,000

Benches, Shelters, or other infrastructure at every stop - \$7,550,000

3 SuperStops, placed where three or more routes intersect - \$3,000,000

Executive Project Summary

The decentralized route system is a success, with ridership projected to be up 8% on the non-campus routes in FY 13. Under the decentralized system, transfers and bus boarding spread throughout the community instead of at one location. Therefore, there is a desire to enhance the customer experience by providing an amenity (bench, shelter, or other structure) at every stop to encourage usage and provide an accessible, comfortable, and safe environment to wait for the bus. Adding to the enhanced infrastructure is the construction of SuperStops. These stops serve as a mini-transfer hub providing many of the amenities of a C.K. Steele Plaza but in a much smaller footprint, providing remote locations to transfer, use the restroom, and purchase fare.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input checked="" type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

The customer experience's on the transit system is enhanced when they are provided an accessible, comfortable, and safe waiting area for the bus. There are two components to improving the customer experience at this phase of the trip. StarMetro's first priority is universal bus stop accessibility by bringing all stops to full Americans with Disabilities Act (ADA) compliance. The second priority is enhanced customer comfort by adding bus stop amenities such as leaning rails, benches, and/or shelters. More detailed information on this project is listed in attachment #1.

ADA Accessibility

In an effort to promote universal accessibility and for the comfort of all transit users—including people in wheelchairs or using other mobility devices, parents with strollers, etc.—StarMetro produced an Accessibility Transition Plan as a guide for bringing all transit facilities into compliance with the Americans with Disabilities Act Accessibility Guide (ADAAG). An accessible stop will have: a boarding and alighting area that is 5'x8' perpendicular to the curb and composed of a firm, stable, slip resistant surface; a running slope no greater than 5 degrees and a cross slope no greater than 2 degrees; an adjacent sidewalk with curb cuts. Of the 917 stops in the inventory, 258 were found to be fully accessible.

Stop Amenities

Bus stop amenities provide enhanced levels of comfort for customers waiting for the bus. Such amenities may include a bus shelter, bench, trash can, solar lighting, or bike racks. Placement of amenities depends on many factors; such as minimum daily boardings, transfer locations, shopping centers, public offices, destinations for seniors and persons with disabilities, and areas with safety concerns. Based on boardings or other elements, StarMetro classifies bus stops into four categories.

Category 1: Bus Stops with 0 - 9 Boardings per day - these stops will be ADA compliant, have a trash can, and will have either a leaning rail or a canopy for customers to use. (569 stops upgraded)

Category 2: Bus Stops with 10 - 64 Boardings per day - these stops will be ADA compliant, have a trash can, and will have a shelter with a bench. (164 stops upgraded)

Category 3: Bus Stops with 64+ Boardings per day - these stops will be ADA compliant, have a trash can, and will have an oversized shelter with multiple benches. (47 stops upgraded)

Category 4: SuperStops - these are transfer points where three or more routes currently or potentially could intersect, creating a major transfer point in the system. These stops would be constructed with bus bays, covered walkways/seating areas, and possible other amenities such as a restroom and farebox vending machines, to facilitate the transfer between routes. (3 stops upgraded)

Cost By Themes

\$12,250,000	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

StarMetro – Enhanced Infrastructure (Project #29)

The customer’s experience on the transit system is enhanced when they are provided an accessible, comfortable, and safe waiting area for the bus. There are two components to improving the customer experience at this phase of the trip. StarMetro’s first priority is universal bus stop accessibility by bringing all stops to full Americans with Disabilities Act (ADA) compliance. The second priority is enhanced customer comfort by adding bus stop amenities such as benches, shelters, and SuperStops.

ADA Accessibility

In an effort to promote universal accessibility and for the comfort of all transit users—including people in wheelchairs or using other mobility devices, parents with strollers, etc.— StarMetro produced an Accessibility Transition Plan as a guide for bringing all transit facilities into compliance with the Americans with Disabilities Act Accessibility Guide (ADAAG). **An accessible stop will have: a boarding and alighting area that is 5’x8’ perpendicular to the curb and composed of a firm, stable, slip resistant surface; a running slope no greater than 5 degrees and a cross slope no greater than 2 degrees; an adjacent sidewalk with curb cuts.**

Of the 917 stops in the inventory 258 were found to be fully accessible. The 569 non-accessible stops have been divided into five categories as described below.

Priority 1 (120 stops) – These stops are those that need a narrow slab of concrete to connect the curb and sidewalk over the grassy strip known as the utility strip. Because the utility strip is always in the public right-of-way and the construction costs are low, category 1 are the easiest to upgrade; because there is a gap between curb and sidewalk wheelchair use is difficult, making them a high priority for upgrade.



Priority 2 (77stops) - These stops are easy to upgrade but are useable by a customer in a wheelchair as is because the concrete is contiguous from the curb to the back of the sidewalk. To be a category 2 right-of-way must be evident, usually shown by power poles as in the photo at right. The lack of clear right-of-way automatically moves a stop to category 3.



Priority 3 (181stops)- *These stops* are of medium difficulty, requiring more concrete than the two easy categories or having some other minor barrier including right-of-way needs or small hills and slopes that make construction slightly more problematic. The stop shown at right requires a 5’x6; connection to the sidewalk.



Priority 4 (167 stops) - These stops are difficult. They cannot reasonably be relocated, lack sidewalks or have a significant distance between the sidewalk and road (shown near right), have significant construction needs like retaining walls or culverts (shown far right), or require major reconstruction.



Priority 5 (24 stops) - These stops cannot be relocated and present such significant obstacles that they will remain not fully accessible until such time as the site is completely rebuilt.



Table 1: Stop categories, number in system, and description

Priority	# in system	Description
1	120	Small amount of concrete needed (less than 4'x5') to connect the sidewalk and the curb over the utility strip.
2	77	Small amount of concrete needed behind the sidewalk and right-of-way is available.
3	181	Between 4x5 and 5x8 feet of concrete, minor slopes and other minor difficulties. May not have right-of-way.
4	167	Stop cannot be relocated; large amounts of concrete (greater than 5'x8') needed; may have hills, slopes, obstructions and other surmountable obstacles. Major reconstruction may be required. Many of these lack sidewalks.
5	24	Stop cannot be relocated; there are insurmountable obstacles such as constructed private property (retaining walls).
Total	569	

The primary deficiency in StarMetro's stops is the boarding and alighting area. StarMetro is working closely with City Public Works department to add sidewalks along StarMetro routes. Sidewalk coverage is now 91% and deficiencies are largely along state and county portions of roads. The City and Leon County Public Works department are aware of the need for boarding and alighting areas, and the county has already added several to projects they are currently undertaking; the City now includes boarding and alighting areas on all sidewalk plans and requires developers include them in site plans for new developments.

The main task is in retrofitting existing facilities. City of Tallahassee Public Works will perform the work for StarMetro with this funding. As noted, private development and City sidewalk work will include upgrades where applicable. Complete ADA compliance could be completed with additional funds from the City or other funding sources.

Stop Amenities

Bus stop amenities provide enhanced levels of comfort for customers waiting for the bus. Such amenities may include a bus shelter, bench, trash can, solar lighting, or bike racks. Placement of amenities depends on many factors; such as minimum daily boardings, transfer locations, shopping centers, public offices, destinations for seniors and persons with disabilities, and areas with safety concerns. Currently, StarMetro has four categories of stops. All stops have an octogonal pole, system signage, and a braille plate (forthcoming). Depending on the category, other amenities are added.

- Category 1 stops have fewer than 10 boardings a day and are not located at transfer points or major destinations. A bus stop pole is only provided at these stops.
- Category 2 stops have 10+ boardings a day or located at a transfer point/major destination. Benches and/or shelters are provided at these stops.
- Category 3 stops boardings greater than 65 per day and the current shelter is not large enough to meet the needs of our customers.
- Category 4 stops are major transfer points where three or more routes currently do or could intersect.

With a desire to take the current stop above and beyond what would be ordinarily found in a typical transit system, staff identified additional amenities that could be added to bus stops across the City in order to improve the level of comfort for passengers.

Category 1

Category 1 stops would have a bench for passengers. Including installation, each bench costs approximately \$3,500.

Category 2

Category 2 stops offer a higher level of protection from the elements as well as greater comfort. Each stop would contain a full shelter with side panels and a bench. Many shelters may also contain solar lighting, trash cans, and bike racks. All these facilities cost \$23,000 to purchase, construct, and install.

Category 3

Category 3 stops are those are served by at least one route and have boarding in excess of 65 per day. At these stops, a standard bench would not provide enough seating capacity to meet the needs of the customers. A double or triple shelter would be installed at these locations.

Category 4

At least three locations in the StarMetro service area currently have or have the potential for three or more routes to intersect. It is at these locations additional amenities are needed to meet the needs of our customers. These mini-hubs would include bus bays, covered waiting areas, and may include restrooms and fare vending machines.

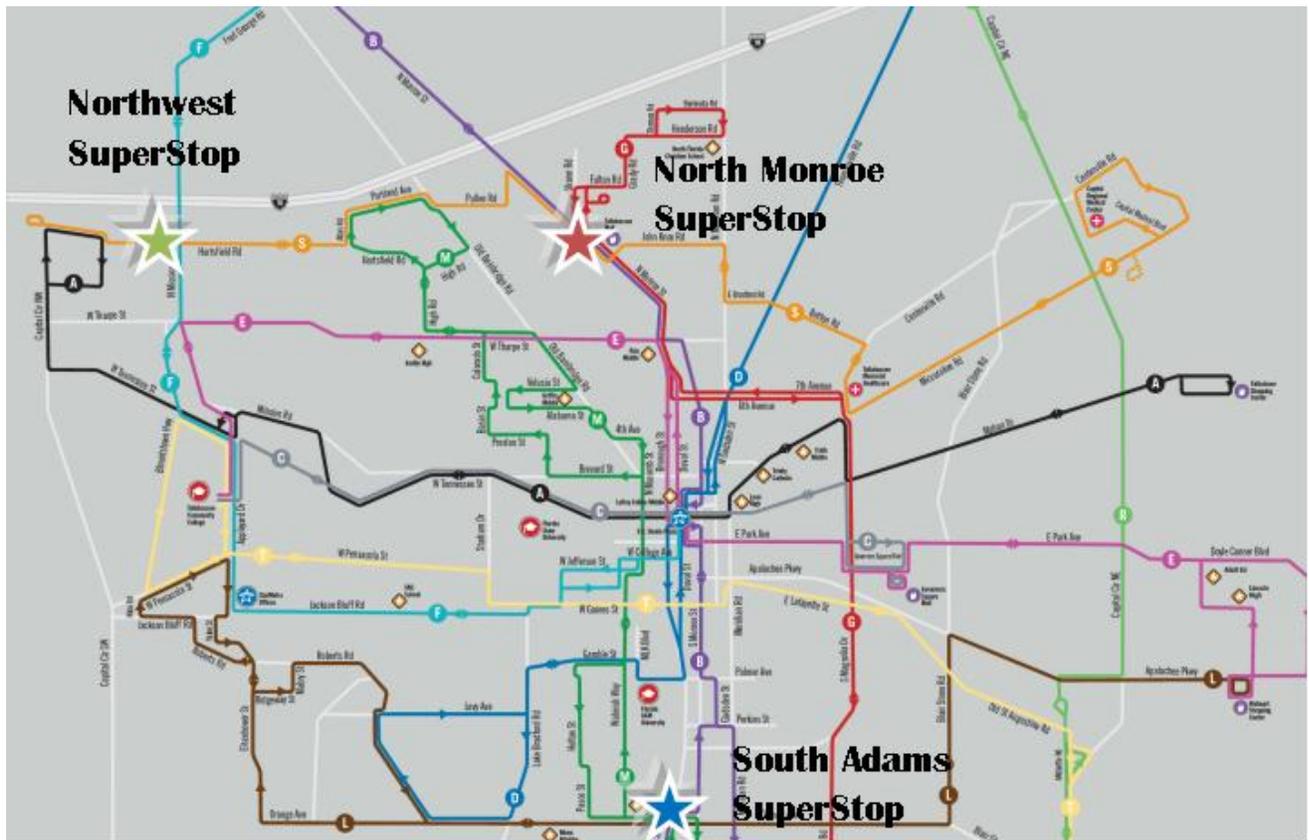


Table 2: Enhanced Infrastructure

	Boardings	Amenities	Number of Stops	Capital Costs	Annual Maintenance Costs*
Category 1	0-9	Bench	569	\$1,992,000	\$569,000
Category 2	10-64	Shelter with bench	164	\$3,800,000	\$164,000
Category 3	65+	Large shelter with multiple benches	47	\$2,350,000	\$47,000
Category 4	N/a; 3 or more intersecting routes	SuperStop	3	\$3,000,000	\$50,000
Total Cost				\$11,200,000	\$830,000
Amenity Coverage					100%

*Assuming \$1,000 annual maintenance costs for stops with any amenity.

StarMetro - Enhanced Infrastructure

Northwest SuperStop



Super Stop



Super Stop

North Monroe SuperStop

South Adams SuperStop

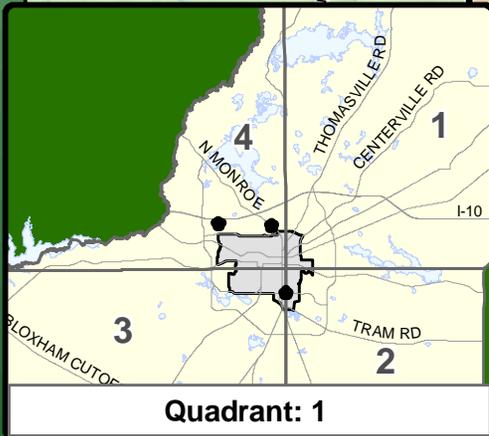
**Shown under #23
(Orange Meridian Placemaking)**



Super Stop



**Bus Stop Enhancements
and ADA compliance**



Quadrant: 1

Project #29



Previous Project Information for:

Project #29

StarMetro

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Moving People: Redevelop C.K. Steele Plaza (*Previously Project #35*)

Moving People: Expand the Facility and Fleet (*Previously Project #34*)

Moving People: Build SuperStops (*Previously Project #33*)

StarMetro: Bus Stop Enhancements (*Previously Project #31*)

Program
2a

Moving People: Redevelop C.K. Steele Plaza

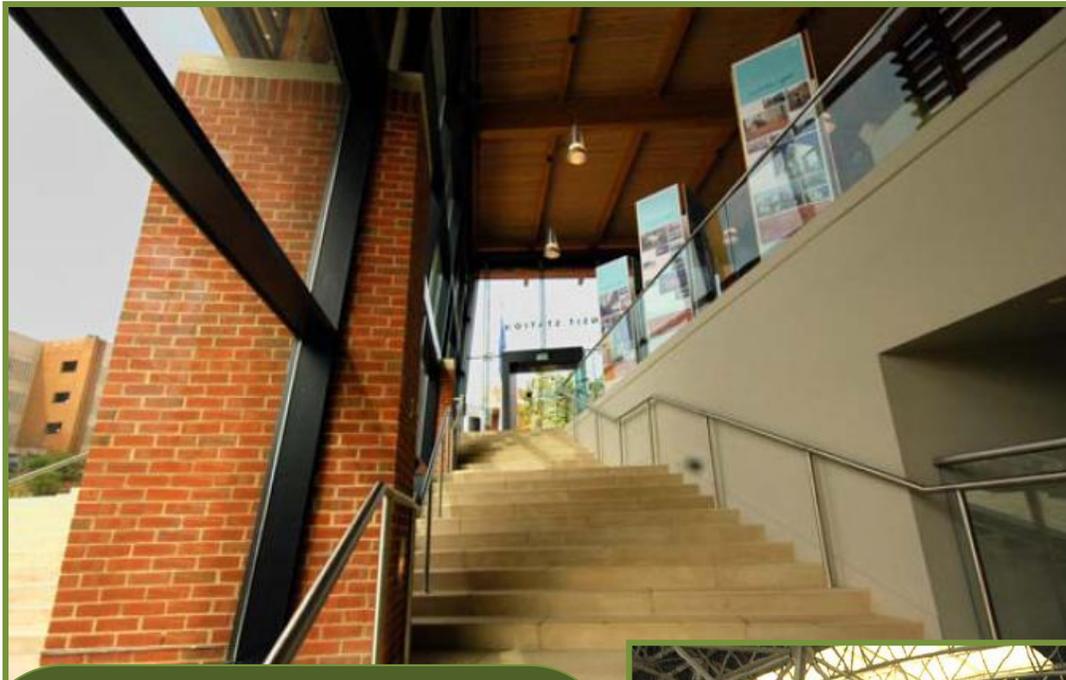
Estimated Cost:
\$25 million

WHAT IS IT?

C.K. Steele Plaza was completed in 1985, and while it was recently renovated, the facility is beginning to show its age. In addition, the plaza is also single-story single-use property which could be put to a much higher use in an area where multiuse projects are encouraged and foot traffic is high. Therefore, StarMetro has proposed redevelopment of the property into a multi-story/multi-use facility incorporating bus bays and other transit amenities indicative of a site important to transit operations. The site could include both commercial and office leasable space, and would be the hub of premium transit Downtown, as well as an intersecting stop for the Gaines to Midtown Trolley.

Evaluation Criteria:

Provides Regional Impact / Geographic Equity	✓	Benefits the Environment	✓
Enhances Mobility	✓	Supports Economic Development / Revitalization	✓
Supports Recreation and Quality of Life	✓	On an Existing Master Plan or Blueprint List	✓
		Leverages Other Funds	✓



At left is the entrance to the Downtown Transit Center in Charlottesville, Virginia

Below is the Charlotte Transit Center in Charlotte, North Carolina.

A state of the art redevelopment of C.K. Steel Plaza would be a hub for premium transit Downtown, but would also include StarMetro offices (currently located on Appleyard Drive) as well as leasable office and retail space. Relocating StarMetro offices would provide sorely needed opportunities to expand the Appleyard Maintenance Facilities.



WHY IS IT IMPORTANT?

Benefits the Environment – As a single-use facility, Steele Plaza does not encourage a variety of uses, but a redeveloped Steele Plaza would provide several walkable destinations that can reduce the number of trips people make by car.

Provides Regional Impact / Geographic Equity – People from all over the city currently transfer at the plaza and any improvements to the site would be enjoyed by a broad group of citizens.

Enhances Mobility – The inclusion of a day care and/or a grocery would vastly increase the mobility of StarMetro users because these uses would consolidate a trip that would otherwise take extra time.

Supports Recreation and Quality of Life – C.K. Steele Plaza is currently something of a blank space in the urban environment. Redeveloping it into an attractive, mixed-use facility would improve the vibrancy and quality of life for all residents and users of Downtown.

Supports Economic Development / Revitalization – Rebuilding the dated, single-use, and unattractive Steele Plaza would bring more energy and investment by directly employing more people in the leasable space, as well as by making the area more aesthetically appealing.

On an Existing Master Plan or Blueprint List – Redeveloping Steele Plaza has been on StarMetro's Transit Development Plan for nearly 20 years.

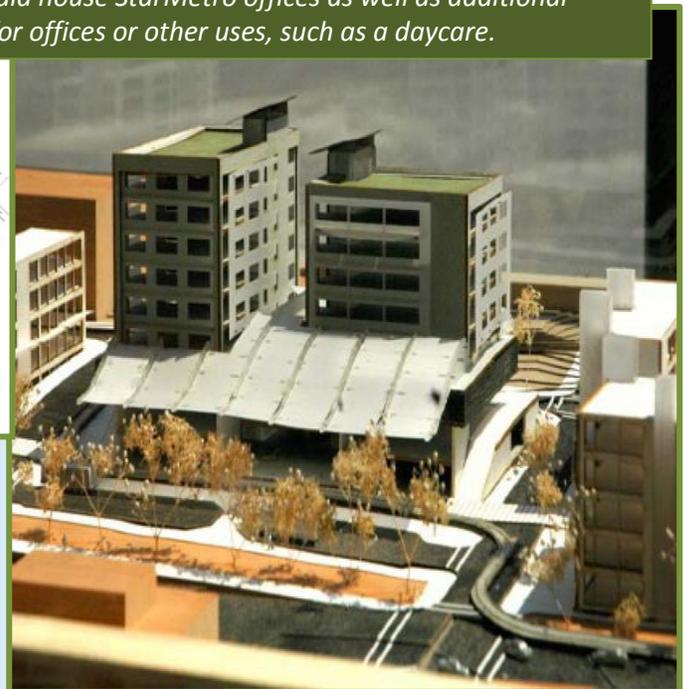
Leverages Other Funds – Under the current federal transportation bill (MAP-21), additional funding could be leveraged under the Federal Transit Administration (FTA) Section 5339 Bus and Bus Facility program for this project, provided a local match is identified.

Below, a three dimensional model of a potential C.K. Steele Redevelopment as it would appear from Tennessee Street.

Bottom is an elevation as the structure would appear from Adams St.

At left, an aerial rendering of the proposed redeveloped plaza.

On the ground floor would be the transit hub and retail space. The upper floors would house StarMetro offices as well as additional leasable space for offices or other uses, such as a daycare.



Program
2b

Moving People: Expand the Facility and Fleet

Estimated Cost:
\$13.3 million

WHAT IS IT?

To make transit a viable option for people on tight schedules, service must be provided frequently and have hours of operation that allow flexible schedules. This means constantly seeking to expand the number of transit vehicles serving each route as well as extending the hours each route operates. The current administrative facility on Appleyard Drive, however, is at capacity and there is insufficient bus storage, office, garage, and parking space to increase the fleet beyond the current 70 fixed-route buses. It would cost approximately \$10,000,000 to expand the facility in order to add more vehicles. Additionally, six buses and four vans are requested to increase StarMetro's fleet. These additional vehicles and the additional operating funds would allow StarMetro to have a bus frequency of 30 minutes or better on most routes. Each bus costs approximately \$450,000 and each van costs \$150,000. The estimated capital cost for the new vehicles is \$3,300,000.

Evaluation Criteria:

Provides Regional Impact / Geographic Equity ✓
Enhances Mobility ✓
Supports Recreation and Quality of Life ✓

Benefits the Environment ✓
Supports Economic Development / Revitalization ✓
On an Existing Master Plan or Blueprint List ✓
Leverages Other Funds ✓



WHY IS IT IMPORTANT?

Benefits the Environment – Additional bus capacity would add flexibility to the system and therefore encourage more people to use transit, reducing need for as many car trips and resulting emissions. New buses also have improved fuel economy and cleaner emissions.

Provides Regional Impact / Geographic Equity – The investment would increase frequencies and provide more travel options for all city residents.

Enhances Mobility – Travel times would be reduced as more vehicles enter service.

Supports Recreation and Quality of Life – An expanded fleet could provide additional service hours during nights and weekends, allowing access to more recreational sites and events.

Supports Economic Development / Revitalization – Additional bus capacity and higher frequencies would encourage high-density, mixed-use development near StarStops. This can already be seen in the new student housing built along the Seminole Express routes.

Leverages Other Funds – Increasing transit service and ridership overtime increases StarMetro's funding from FDOT and FTA. Under the current federal transportation bill (MAP-21), additional funding could be leveraged under the Federal Transit Administration (FTA) Section 5339 Bus and Bus Facility program for this project provided a local match is identified.



The existing maintenance facility at Appleyard Drive is at capacity. In order to expand the service to include more buses per hour and to offer more night and weekend routes, both facility expansion and more vehicles are needed. Provided service with shorter wait times and longer hours are key to making transit a flexible, desirable alternative to driving.



Program
2c

Moving People: Build SuperStops

Estimated Cost:
\$9 million

WHAT IS IT?

Under the new decentralized system, transfers are no longer limited to a single point, but can occur anywhere routes intersect. Places where three or more routes intersect are ideal for SuperStops, which provide bus pull-ins, larger covered areas, restrooms and other amenities. StarMetro has planned three of these SuperStops at an estimated cost of \$9,000,000 (\$3,000,000 each). Each Super Stop fills a unique need. The Northwest stop provides a much-needed layover (restroom) point for coach operators as well as the customer amenities at a high-traffic location. The North Monroe stop safely facilitates transfers at a location where traffic patterns make transferring difficult, and the Orange Avenue stop will provide passenger comfort and a safety at a place with extraordinarily high ridership.

Evaluation Criteria:

Provides Regional Impact / Geographic Equity	✓	Benefits the Environment	✓
Enhances Mobility	✓	Supports Economic Development / Revitalization	✓
Supports Recreation and Quality of Life	✓	On an Existing Master Plan or Blueprint List	✓
		Leverages Other Funds	✓



Super Stops are designed to provide safe and pleasant facilities as customers wait to catch connecting routes. Each one includes such things as sheltered bus bays (top), covered walkways (bottom left) and restrooms (bottom right).



WHY IS IT IMPORTANT?

Benefits the Environment – SuperStops could promote walkable, high-density, mixed-use centers that encourage transit use over the automobile, which results in less fuel use, fewer emissions, and better air quality.

Provides Regional Impact / Geographic Equity – C.K. Steele Plaza in Downtown Tallahassee is currently the only place that has the amenities that would be included in a SuperStop. Satellite facilities with restrooms, covered waiting areas, and bus bays would support the new decentralized transit system.

Enhances Mobility – Additional route and transfer options would be available at nodes along the periphery of the city.

Supports Recreation and Quality of Life – SuperStops would improve quality of life by providing a comfortable waiting area with restrooms, a cashier's window, bike racks, etc.

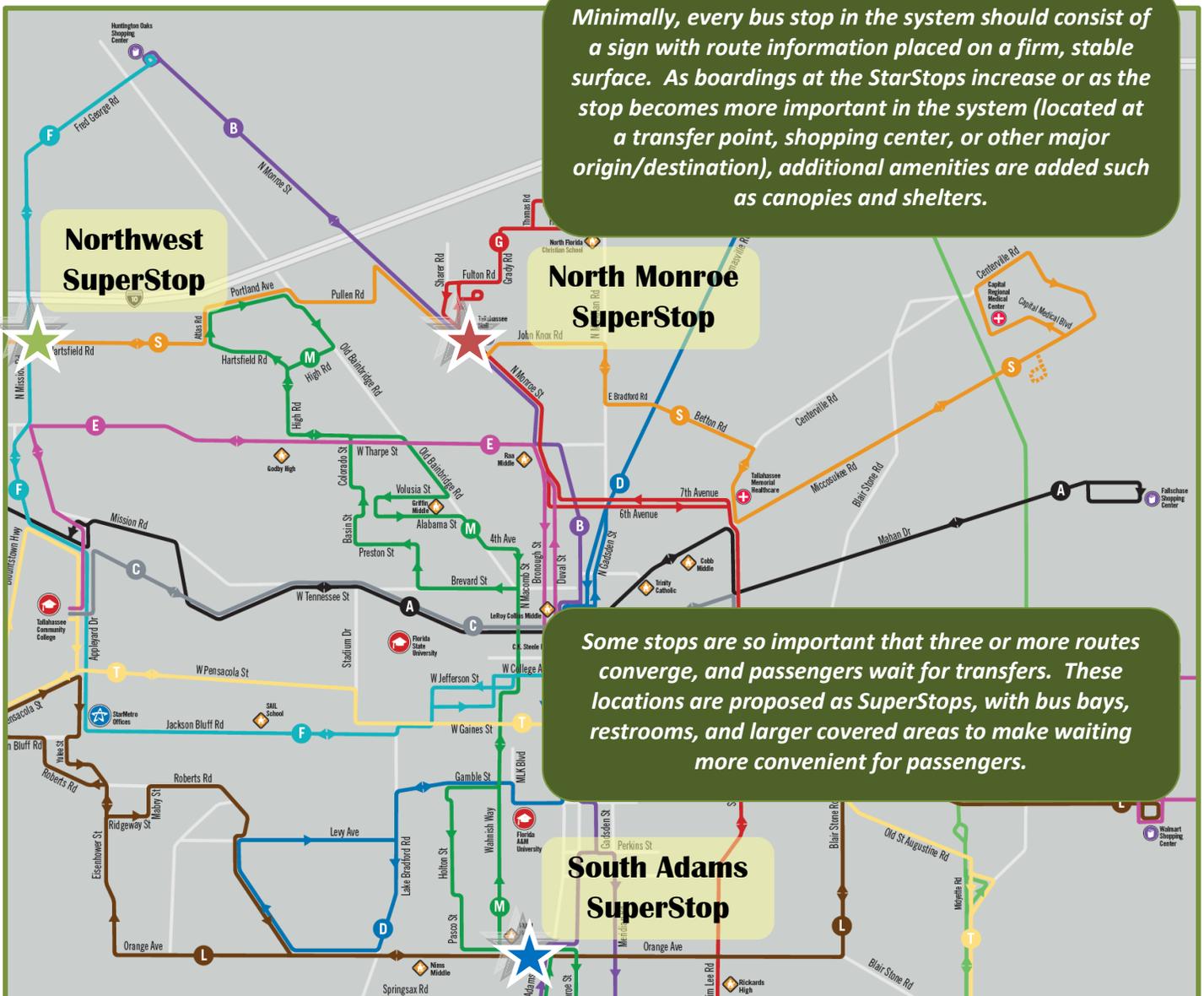
Supports Economic Development / Revitalization – SuperStops could attract development around each station.

On an Existing Master Plan or Blueprint List – SuperStops are included in StarMetro's current Transit Development Plan.

Leverages Other Funds – Under the current federal transportation bill (MAP-21), additional funding could be leveraged under the Federal Transit Administration (FTA) Section 5339 Bus and Bus Facility program for this project if a local match is identified. StarMetro will use its existing resources, grants, and partnerships to make sure all other stops in the system meet at least minimum standards and will seek to place shelters or other covering for at least 25% of the stops in the system.

Minimally, every bus stop in the system should consist of a sign with route information placed on a firm, stable surface. As boardings at the StarStops increase or as the stop becomes more important in the system (located at a transfer point, shopping center, or other major origin/destination), additional amenities are added such as canopies and shelters.

Some stops are so important that three or more routes converge, and passengers wait for transfers. These locations are proposed as SuperStops, with bus bays, restrooms, and larger covered areas to make waiting more convenient for passengers.



Proposed Sales Tax Projects by the Community

Project Name: StarMetro: Bus Stop Enhancements (Project #31)

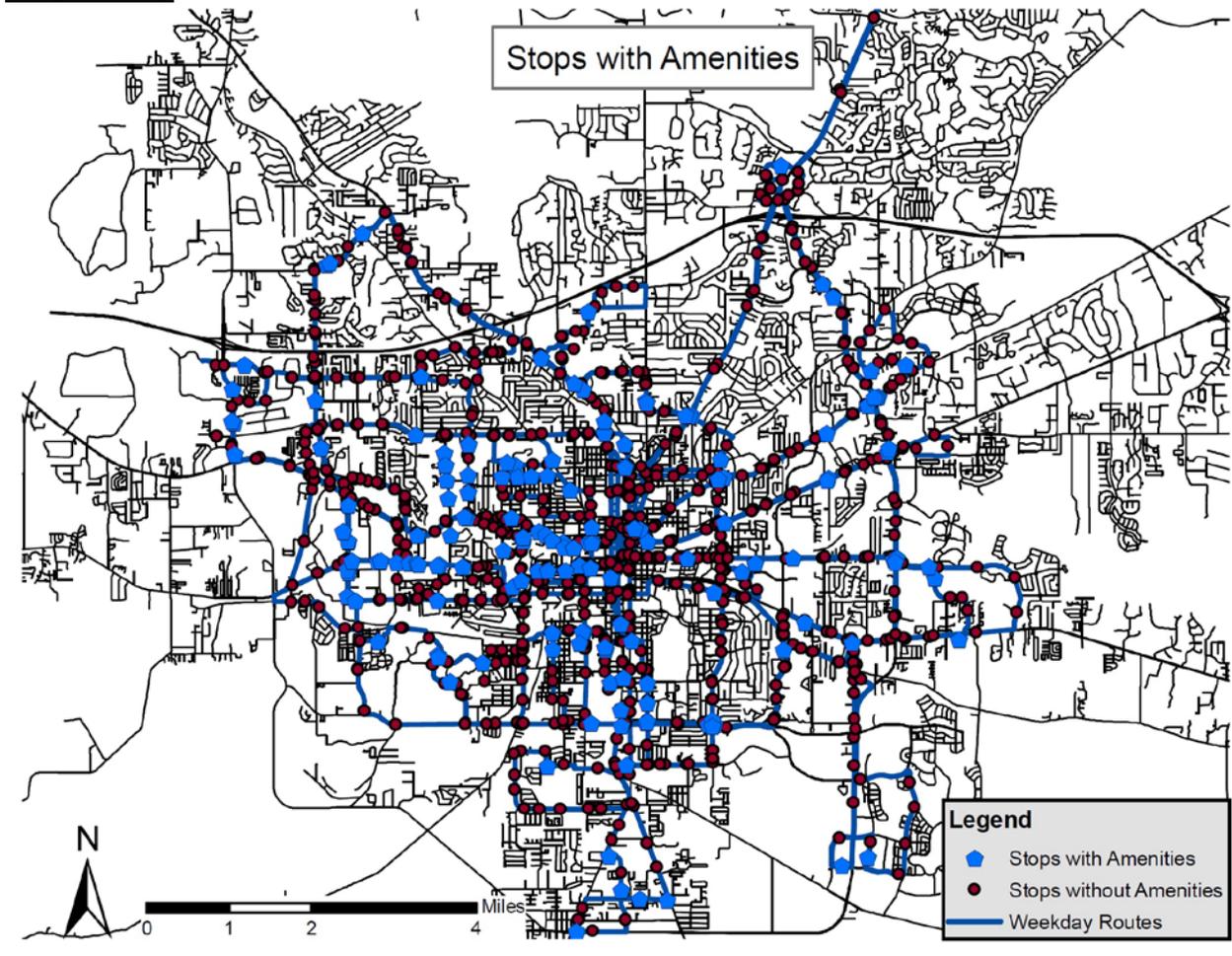
Project Themes:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

The City of Tallahassee is currently developing a capital plan to add more bus stop amenities throughout the system. Bus stop amenities provide enhanced levels of comfort for customers waiting for the bus. Such amenities may include a bus shelter, bench, trash can, solar lighting, or bike racks. Placement of amenities depends on many factors: such as minimum daily boardings, transfer locations, shopping centers, public offices, destinations for seniors and persons with disabilities, and areas with safety concerns. Currently, StarMetro has 917 bus stops with 202 (22% of stops) with or scheduled to have a shelter or a bench. The City recommends increasing the number of stops with a bench or a shelter to at least 50% of the system.

Project Map:



SALES TAX PROJECTS

Project Number: 30
Project Name: Weems Road Flood Control

Total Project Cost: \$7,800,000

Total Project Cost Notes
NA

Executive Project Summary

This project calls for the reconstruction of 0.80-miles of Weems Road from Mahan Drive to Easterwood as an urban street with curb and gutters, sidewalks, and bike lanes to meet current pedestrian and street safety standards. In addition, water quality enhancement will be accomplished through the elevation of Weems Road to prevent flooding and culvert enlargement in order to preserve and protect Lake Lafayette.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

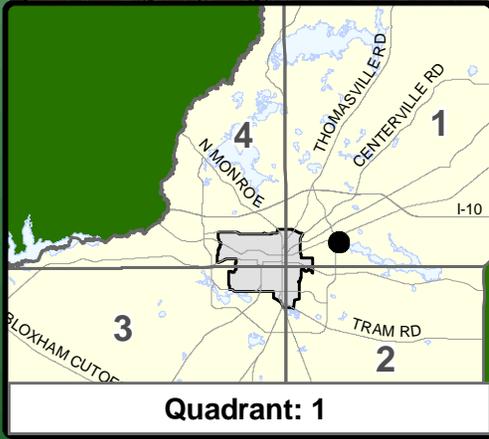
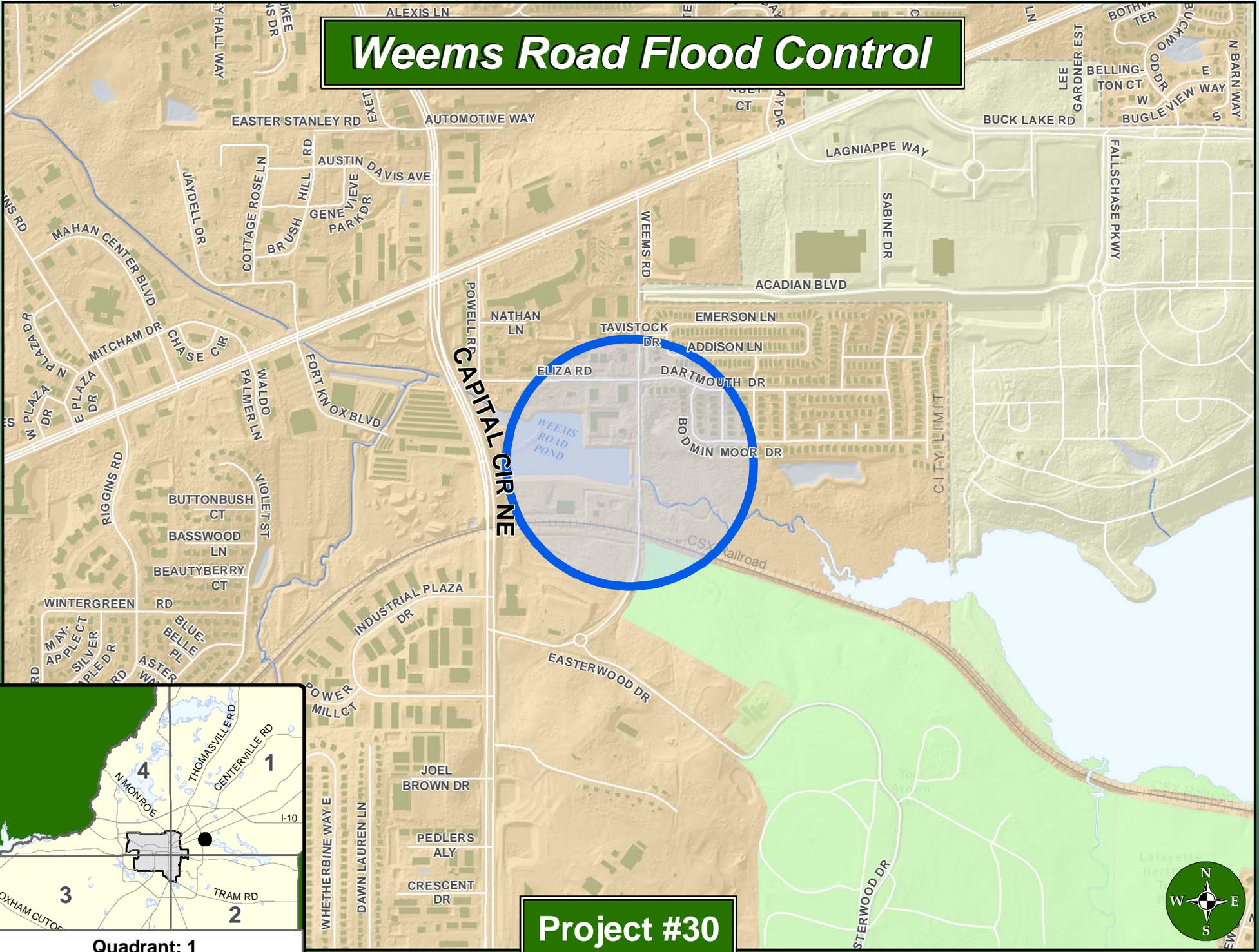
This project calls for the reconstruction of 0.80-miles of Weems Road from Mahan Drive to Easterwood as an urban street with curb and gutters, sidewalks, and bike lanes to meet current pedestrian and street safety standards. In addition, water quality enhancement will be accomplished through the elevation of Weems Road to prevent flooding and culvert enlargement in order to preserve and protect Lake Lafayette.

The sidewalks and bike lanes will allow non-motorized connections from the area's residential neighborhoods to the Commercial centers on Mahan Drive to the north and the recreational opportunities associated with Tom Brown Park to the south. As such, the enhancements will improve the multi-modal efficiency in the area including and surrounding the intersection of Weems Rd. and Mahan Drive, a state-owned road.

Cost By Themes

\$7,800,000	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Weems Road Flood Control



Project #30



Previous Project Information for:

Project #30

Weems Road Flood Control

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Lake Lafayette Basin Stormwater Improvements and Trailhead Developments (*Previously Project #6*)

Blueprint 2000

Proposed Sales Tax Projects

Project Name: Lake Lafayette Basin Stormwater Improvements and Trailhead Developments
(Blueprint 2000 Map 6)

Project Themes:

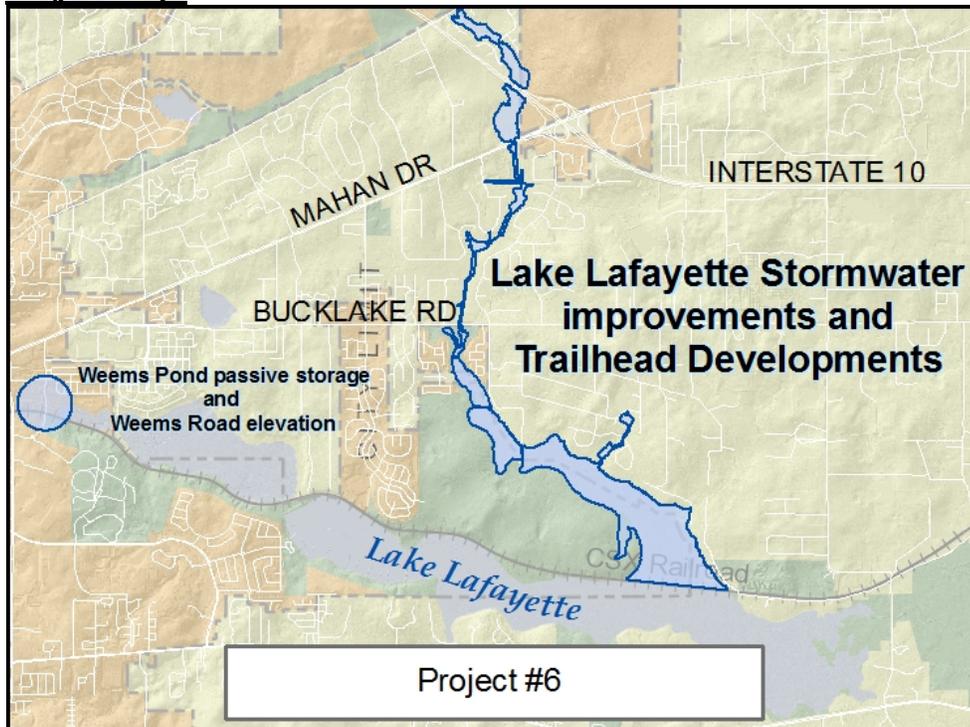
- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

The primary benefits of this project are significant additions to the regional greenway network and stormwater retrofit to enhance water quality in the Lake Lafayette Basin. The greenway system within this map will include new connections between Goose Pond Trail, Lake Lafayette Heritage Trail and Alford Arm Properties and the Miccosukee Canopy Road Greenway. The stormwater retrofit will primarily occur in Tom Brown Park.

Currently the Goose Pond Trail shared use path ends east of the Mahan Drive/Capital Circle NE intersection. The project proposes to extend the shared-use path to Dempsey Mayo Road where the current shared use path along Mahan Drive begins. This plan also proposes the extension of the Mahan Drive shared use path eastward from Vineland Drive to Walden Road. Connection north to the Miccosukee Greenway from Mahan Drive can be accomplished through a shared use path along Thornton Road. Connection south from Mahan Drive to the JR Alford Greenway can be made via Pedrick Road and Buck Lake Road to the Goose Creek County Park. Trail crossings at roadways may need either to be established or enhanced. Weems Road will need to be elevated to prevent flooding and culverts will need to be enlarged in order to preserve and protect Lake Lafayette

Project Map:



SALES TAX PROJECTS

Project Number: 31
Project Name: Intersection and Safety Improvement
Total Project Cost: \$5.8 million

Total Project Cost Notes
The project cost does include right-of-way costs.

Executive Project Summary

This project is for the improvement of intersections throughout the County in order to maintain safe and efficient operations. Intersection improvements can also have significant impacts on the capacity of road sections as it relates to concurrency management. Occasionally, improvements can be coordinated with improvements being performed by other governmental agencies such as the Florida Department of Transportation and the City of Tallahassee.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

Attachment #1 details the proposed projects within the Intersection and Safety Improvements project.

Cost By Themes

\$5.8 million	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Intersection and Safety Improvements		
Location	Description	Cost
Blair Stone Road at Old St. Augustine Road	Lengthen turn lanes	400,000
Oak Ridge Road at Wakulla Springs Highway	Construct westbound turn lane with 175 feet of storage on C.R. 2204 (Oak Ridge Road)	150,000
Old Bainbridge Road at CCNW	Realign Old Bainbridge Road at CCNW with Gateway Drive to improve safety of intersection (may be a part of the Orchard Pond Toll Road project)	825,000
Old Bainbridge Road at Raa Avenue	Severe drop-offs at returns, enclose ditch north and south of Raa and improve intersection 450FT	125,000
Miles Johnson Road at Miccosukee Road	Realign Miles Johnson Road and shift Miccosukee Road to the north to improve the sight distance	400,000
Meridian Road at Rhoden Cove Road	Left turn lane for Meridian northbound	250,000
Aenon Church Road at State Road 20	Right turn lane for Aenon Church to west	1,000,000
Geddie Road at State Road 20	Right turn lane for Geddie to west	400,000
Crowder Road at US 27 North	Realign intersection at US 27 with turn lane addition and sidewalk	1,100,000
Buck Lake Road at Chaires Crossroad	Realign Buck Lake Road	250,000
Gadsden Street at Pensacola Street	Signalization (joint project with City)	150,000
Tharpe Street at Maryellen Street	Signalization and left turn lanes for Tharpe Street (joint project with City)	400,000
Tharpe Street at Trimble Road	Signalization and left turn lanes for Tharpe Street (joint project with City)	400,000
Intersection Total		\$ 5,850,000

Intersection and Safety Improvements



Old Bainbridge Rd @ CCNW
- Realign Old Bainbridge and Gateway

Meridian Rd @ Rhoden Cove Road
- Left turn lane for Meridian

Crowder Rd @ US 27 North
- Realignment and lengthen turn lane

Realign Miles Johnson & shift Micosukee to north

Tharpe St @ Maryellen
- Signalization and left turn lanes

Enclose drainage and widen lanes
Tharpe Street @ Colorado St
- Signalization

Buck Lake Road @ Chaires Crossroad
- Realignment

Geddie Rd @ SR 20
- Right turn lane on Geddie to west
BLOUNTSTOWN HWY

Tharpe St @ Trimble
- Signalization and left turn lanes

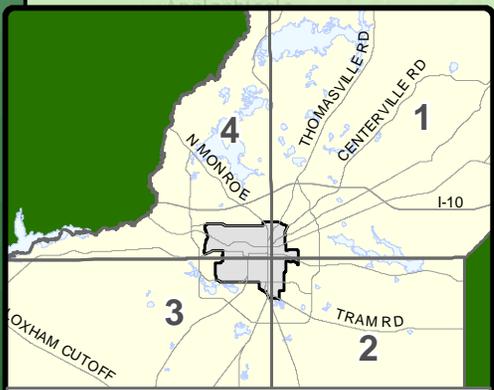
Gadsden Street @ Pensacola - Signalization

Aeon Church Rd @ SR 20
- Right turn lane on Aeon Church to west

Blairstone Rd @ Old St. Augustine Rd
- Lengthen turn lanes

Tram Rd @ Gaile Avenue
- Realignment

Oak Ridge Rd @ Wakulla Springs Rd
- Right turn lane for Oak Ridge to north



Quadrants: All

Project #31



Previous Project Information for:

Project #31

Intersection Safety and Improvements

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Intersection Safety and Improvements (*Previously Project #50*)

Proposed Project for the Sales Tax Extension

Project #13: Intersection Safety and Improvements

Estimated Project Cost: \$5.8 million

Project Criteria:

- | | |
|--|--|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input type="checkbox"/> Water Quality Enhancements & Green Infrastructure |
| <input checked="" type="checkbox"/> Regional Mobility | <input type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input type="checkbox"/> Greenway Master Plan | <input type="checkbox"/> Transportation Capacity Improvements |
| <input type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input checked="" type="checkbox"/> Complements BP2000 Project | |

Project Description:

This project is for the improvement of intersections throughout the County in order to maintain safe and efficient operations. Intersection improvements can also have significant impacts on the capacity of road sections as it relates to concurrency management. Occasionally, improvements can be coordinated with improvements being performed by other governmental agencies such as the Florida Department of Transportation and the City of Tallahassee.

Project Criteria Detail:

Comprehensive Plan: This project supports the Mobility element of the Comprehensive Plan as listed below.

- Objective 1.5: Improve the safety and preserve the integrity of the arterial and collector street system with an effective access management and traffic signal control program and with the use of traffic operations features to maximize the capacity of the existing street system.

Regional Mobility Plan: The project includes several intersections identified in the regional mobility 'Needs Plan'.

Greenway Master Plan: N/A

Connectivity: N/A

Complements Blueprint 2000 Project(s): This project improves intersections that may complement several Blueprint 2000 Projects.

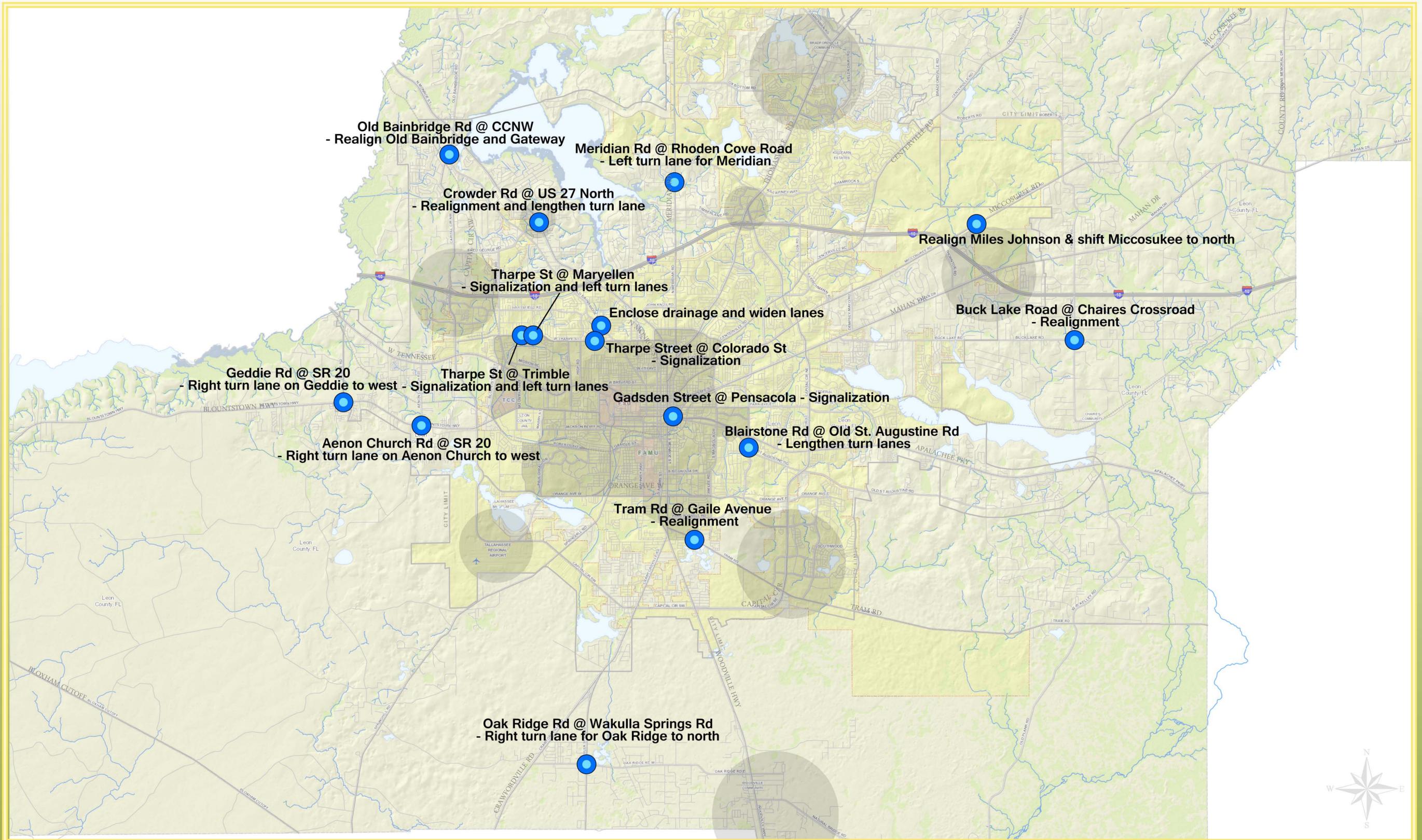
Water Quality Enhancements and Green Infrastructure: N/A

Stormwater/Sewer Capacity Improvements: N/A

Transportation Capacity Improvements: N/A

Core Infrastructure: As major component of the regional mobility plan, intersection improvements are considered core infrastructure projects which maintain safe and efficient operations of the roadways.

Intersection and Safety Improvements		
Location	Description	Cost
Blair Stone Road at Old St. Augustine Road	Lengthen turn lanes	400,000
Oak Ridge Road at Wakulla Springs Highway	Construct westbound turn lane with 175 feet of storage on C.R. 2204 (Oak Ridge Road)	150,000
Old Bainbridge Road at CCNW	Realign Old Bainbridge Road at CCNW with Gateway Drive to improve safety of intersection (may be a part of the Orchard Pond Toll Road project)	825,000
Old Bainbridge Road at Raa Avenue	Severe drop-offs at returns, enclose ditch north and south of Raa and improve intersection 450FT	125,000
Miles Johnson Road at Miccosukee Road	Realign Miles Johnson Road and shift Miccosukee Road to the north to improve the sight distance	400,000
Meridian Road at Rhoden Cove Road	Left turn lane for Meridian northbound	250,000
Aenon Church Road at State Road 20	Right turn lane for Aenon Church to west	1,000,000
Geddie Road at State Road 20	Right turn lane for Geddie to west	400,000
Crowder Road at US 27 North	Realign intersection at US 27 with turn lane addition and sidewalk	1,100,000
Buck Lake Road at Chaires Crossroad	Realign Buck Lake Road	250,000
Gadsden Street at Pensacola Street	Signalization (joint project with City)	150,000
Tharpe Street at Maryellen Street	Signalization and left turn lanes for Tharpe Street (joint project with City)	400,000
Tharpe Street at Trimble Road	Signalization and left turn lanes for Tharpe Street (joint project with City)	400,000
Intersection Total		\$ 5,850,000



Old Bainbridge Rd @ CCNW
- Realign Old Bainbridge and Gateway

Meridian Rd @ Rhoden Cove Road
- Left turn lane for Meridian

Crowder Rd @ US 27 North
- Realign and lengthen turn lane

Realign Miles Johnson & shift Micosukee to north

Tharpe St @ Maryellen
- Signalization and left turn lanes

Enclose drainage and widen lanes

Buck Lake Road @ Chaires Crossroad
- Realignment

Geddie Rd @ SR 20
- Right turn lane on Geddie to west

Tharpe St @ Trimble
- Signalization and left turn lanes

Tharpe Street @ Colorado St
- Signalization

Gadsden Street @ Pensacola - Signalization

Aeon Church Rd @ SR 20
- Right turn lane on Aeon Church to west

Blairstone Rd @ Old St. Augustine Rd
- Lengthen turn lanes

Tram Rd @ Gaile Avenue
- Realignment

Oak Ridge Rd @ Wakulla Springs Rd
- Right turn lane for Oak Ridge to north

County Intersection Safety Improvements

Legend

-  Intersection Improvements
-  Mobility District/ActivityNodes
-  Urban Service Area
-  Parks

Intersection Safety and Improvements Project Criteria:

- | | |
|--|--|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input type="checkbox"/> Water Quality Enhancements & Green Infrastructure |
| <input checked="" type="checkbox"/> Regional Mobility | <input type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input checked="" type="checkbox"/> Greenway Master Plan | <input type="checkbox"/> Transportation Capacity Improvements |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input checked="" type="checkbox"/> Complements BP2000 Project | |



This product has been compiled from the most accurate source data from Leon County and the City of Tallahassee. However, this product is for reference purposes only and is not to be construed as a legal document or survey instrument. Any reliance on the information contained herein is at the user's own risk. Leon County and the City of Tallahassee assume no responsibility for any use of the information contained herein or any loss resulting therefrom.

SALES TAX PROJECTS

Project Number: 32
Project Name: Tram Road
Total Project Cost: \$41,900,000

Total Project Cost Notes
NA

Executive Project Summary

The focus of this project is to provide mobility choices and to promote economic development in the southern part of the community. The purpose of this project is to develop Tram Road as an alternative transportation corridor linking downtown with Southwood Plantation and the Capital Circle Office Center and facilitating greenway linkages. This project also includes protection and/or restoration of environmentally significant features to facilitate the creation of greenway linkages and protect native wetland species.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Detail Project Description

The focus of this project is to provide mobility choices and to promote economic development in the southern part of the community. The purpose of this project is to develop Tram Road as an alternative transportation corridor linking downtown with Southwood Plantation and the Capital Circle Office Center and facilitating greenway linkages. This project also includes protection and/or restoration of environmentally significant features to facilitate the creation of greenway linkages and protect native wetland species.

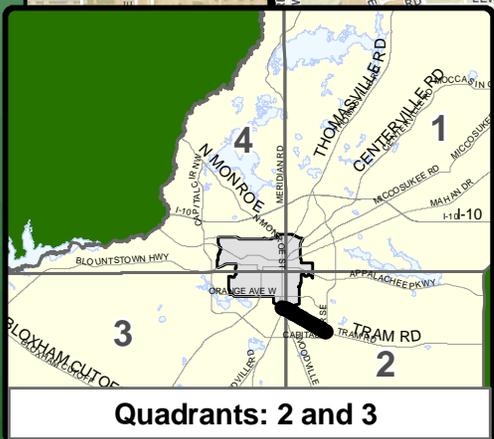
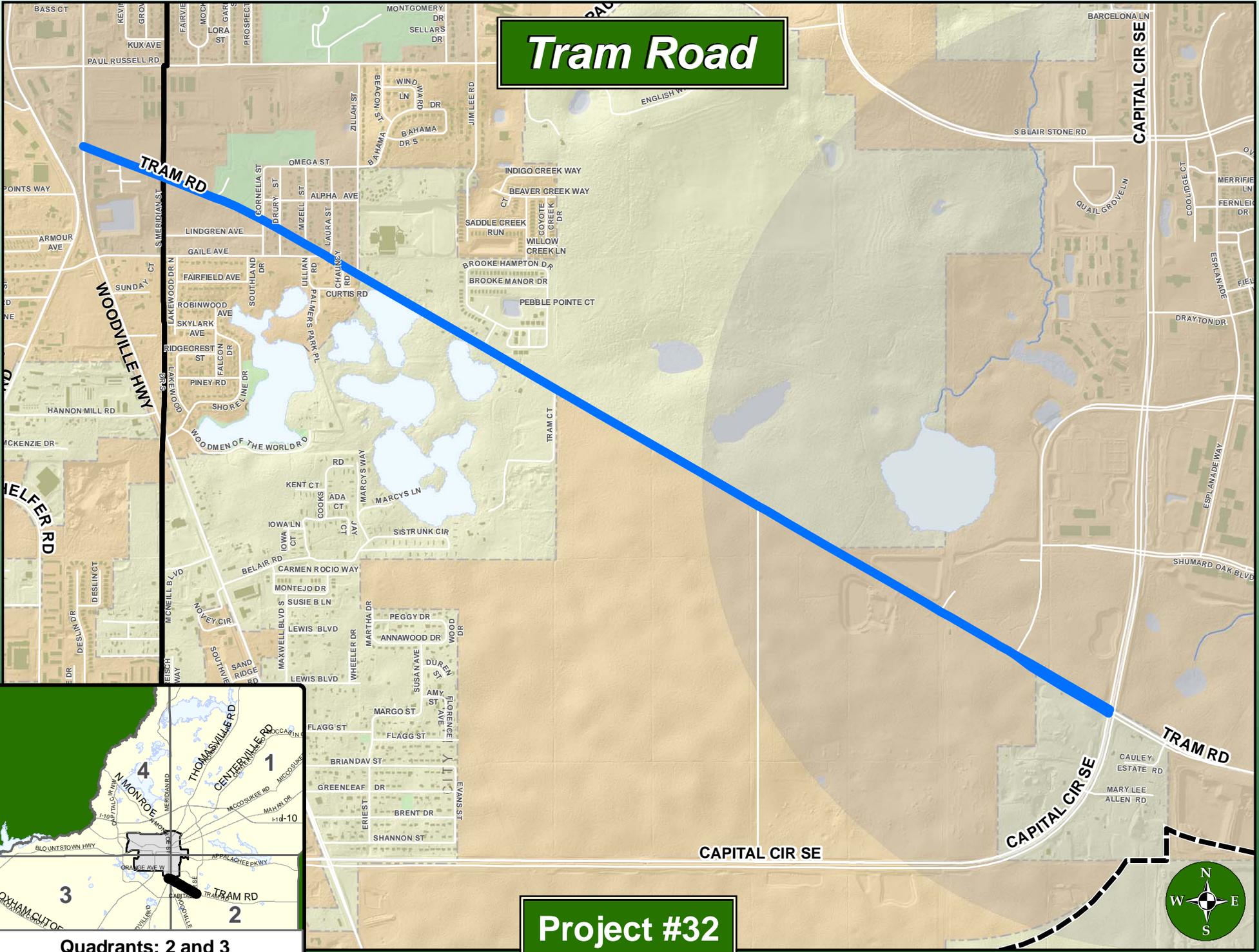
It is expected that additional capacity will be needed on Tram Road to serve Southwood Plantation and the Capital Circle Office Center. Since there is only 60 feet of right-of-way on Tram Road, additional right-of-way should be acquired early. Tram Road should be designed as a future transportation corridor to accommodate vehicles, transit, bicyclists, and pedestrians. Tram Road and Woodville Highway intersection improvements should be planned to incorporate north-south gateway elements as this intersection is in the northern portion of the Woodville Highway Southside Gateway. Additionally, Tram Road improvements should maximize redevelopment opportunities at the North Florida Fairground property.

Greenway linkages will be along transportation corridors connecting the Southwood Greenway system with the larger greenway network north and west, including the existing Campbell Connector Trail to Goose Pond Greenway and the future Tram Road Greenway. Provisions for the construction of trail crossings should be planned.

Cost By Themes

\$41,900,000	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Tram Road



Quadrants: 2 and 3

Project #32



Previous Project Information for:

Project #32

Tram Road

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Tram Road ROW and Construction Interchange at Crawfordville (*Previously Project #12*)

Blueprint 2000

Proposed Sales Tax Projects

Project Name: Tram Road ROW and Construction Interchange at Crawfordville (Blueprint 2000 Map 4)

Project Themes:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Project Description:

The focus this project is to provide mobility choices and to promote economic development in the southern part of the community. The purpose of this project is to develop Tram Road as an alternative transportation corridor linking downtown with Southwood Plantation and the Capital Circle Office Center and facilitating greenway linkages. This project also includes protection and/or restoration of environmentally significant features to facilitate the creation of greenway linkages and protect native wetland species.

It is expected that additional capacity will be needed on Tram Road to serve Southwood Plantation and the Capital Circle Office Center. Since there is only 60 feet of right-of-way on Tram Road, additional right-of-way should be acquired early. Tram Road should be designed as a future transportation corridor to accommodate vehicles, transit, bicyclists, and pedestrians. Tram Road and Woodville Highway intersection improvements should be planned to incorporate north-south gateway elements as this intersection is in the northern portion of the Woodville Highway Southside Gateway. Additionally, Tram Road improvements should maximize redevelopment opportunities at the North Florida Fairground property.

Greenway linkages will be along transportation corridors connecting the Southwood Greenway system with the larger greenway network north and west, including the existing Campbell Connector Trail to Goose Pond Greenway and the future Tram Road Greenway. Provisions for the construction of trail crossings should be planned.

Project Map:



SALES TAX PROJECTS

Project Number: 33
Project Name: Extension of Jackson Bluff Road

Total Project Cost: \$26,000,000

Total Project Cost Notes
NA

Executive Project Summary

The Jackson Bluff Extension project is a Blueprint 2000 Tier II project that extended Jackson Bluff Road westward from Appleyard Drive to Capital Circle Southwest. This project is inconsistent with both the Regional Mobility Plan and the Comprehensive Plan. It was suggested prior to the completion of several other projects which has satisfied connectivity and capacity issues that once affected this roadway.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

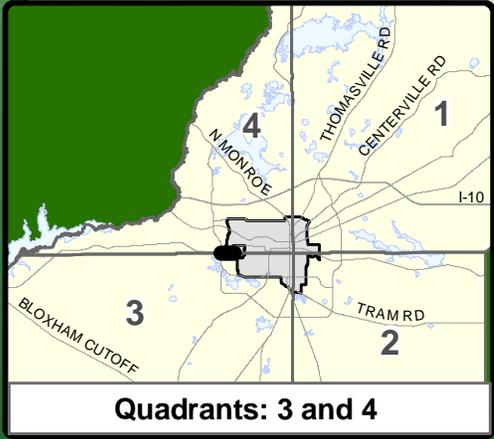
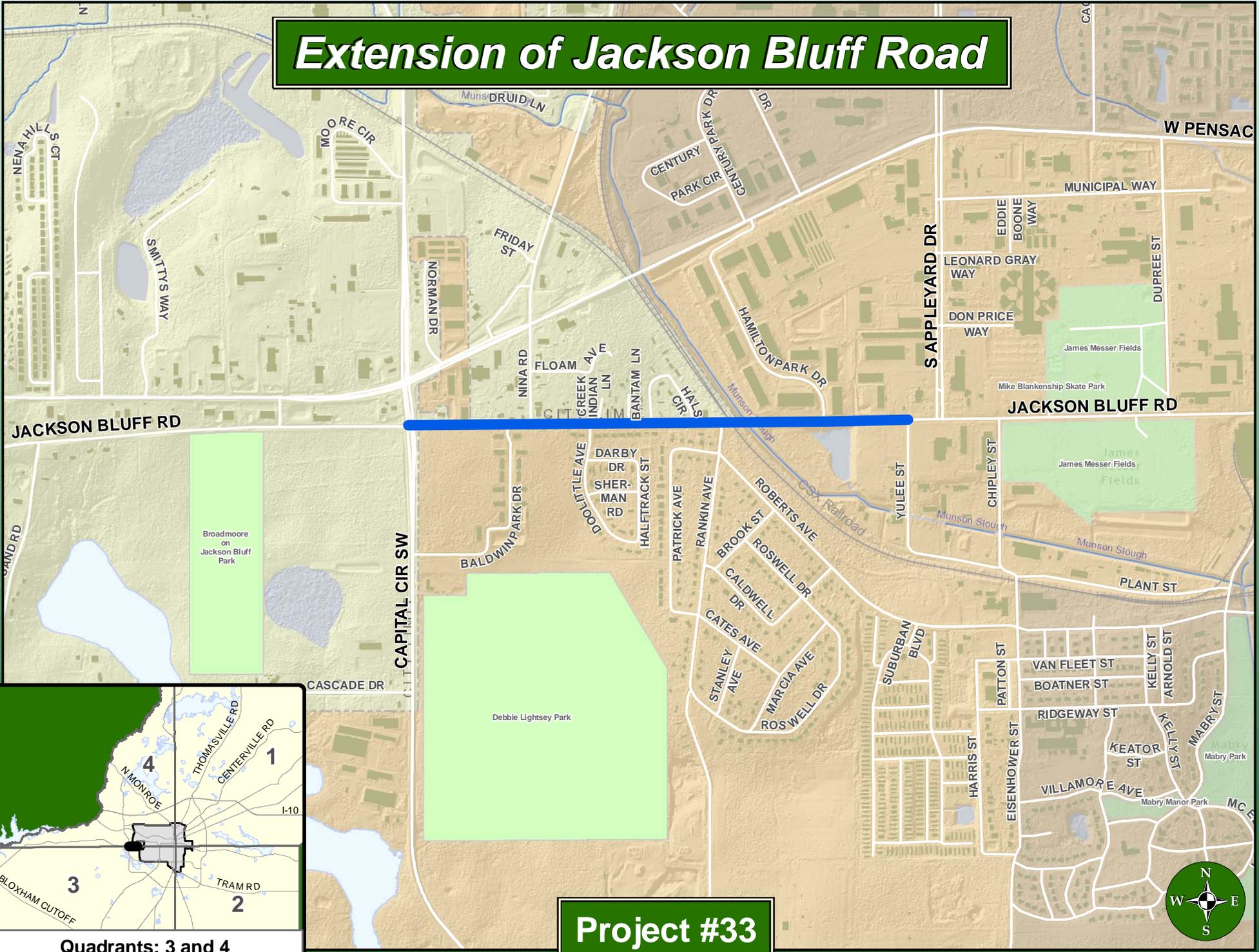
Detail Project Description

The Jackson Bluff Extension project is a Blueprint 2000 Tier II project that extended Jackson Bluff Road westward from Appleyard Drive to Capital Circle Southwest. This project is inconsistent with both the Regional Mobility Plan and the Comprehensive Plan. It was suggested prior to the completion of several other projects which has satisfied connectivity and capacity issues that once affected this roadway.

Cost By Themes

\$26,000,000	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Extension of Jackson Bluff Road



Project #33

Quadrants: 3 and 4

Previous Project Information for:

Project #33

Extension of Jackson Bluff Road

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Extension of Jackson Bluff Road (Previously Project #3)

Blueprint 2000 Proposed Sales Tax Projects

Project Name: Extension of Jackson Bluff Road

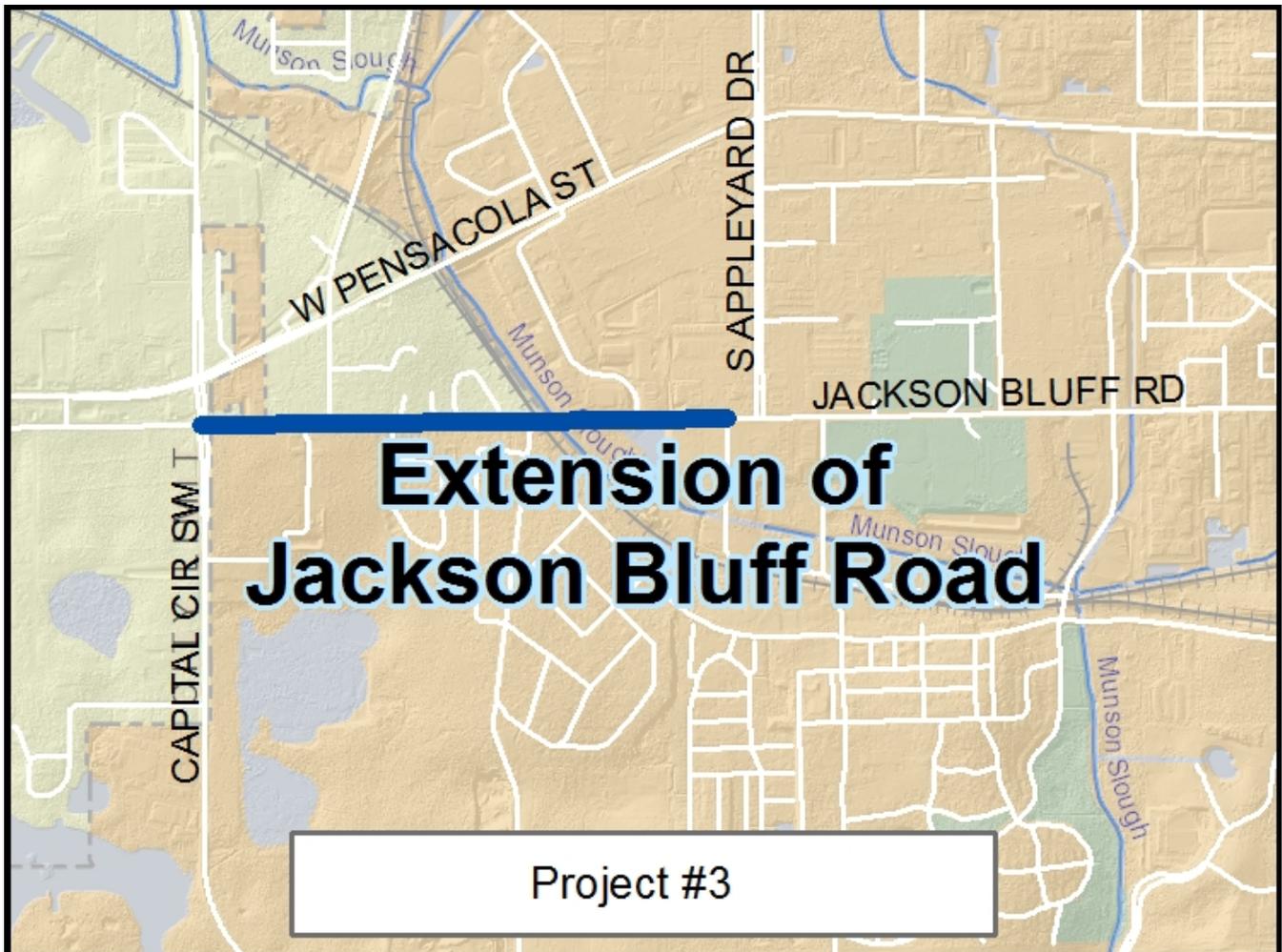
Project Themes:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

The Jackson Bluff Extension project is a Blueprint 2000 Tier II project that extended Jackson Bluff Road westward from Appleyard Drive to Capital Circle Southwest. This project was suggested prior to the completion of several other projects which has satisfied connectivity and capacity issues that once affected this roadway. At this time, no other details are available for this project.

Project Map:



SALES TAX PROJECTS

Project Number: 34
Project Name: Service Road (Capital Circle NW)
Total Project Cost: \$41,800,000

Total Project Cost Notes
NA

Executive Project Summary

The Capital Circle Northwest (CCNW) service roads are the “build-out” scenario identified in the original 2000 Blueprint Project Definitions Report for the CCNW widening project completed in 2007. The proposed service roads would be located in the rear of parcels fronting Capital Circle NW to serve site access requirements in the segment between I-10 and US 90. The design of the completed roadway and the current and forecasted conditions cause the service roads to be both impractical and inconsistent with both the Regional Mobility Plan and the Comprehensive Plan.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

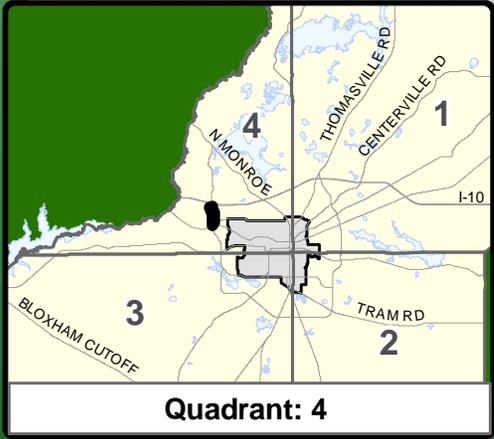
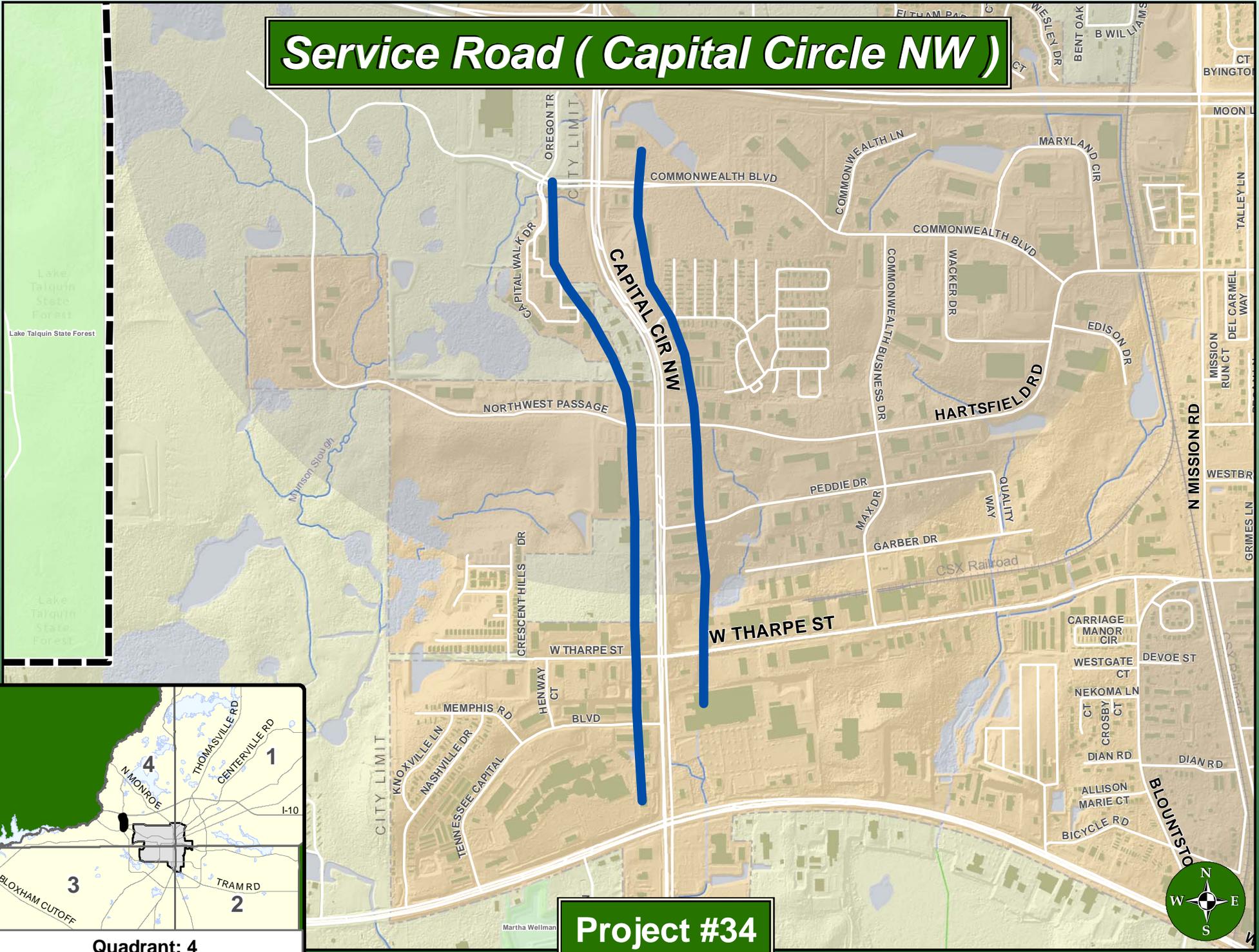
The Capital Circle, NW widening project from I-10 to US 90 (W. Tennessee Street) was completed in 2007. The original Economic and Environmental Consensus Committee proposed enhancements to follow at a later date based on a BP2000/TLCPD revised “build out” master plan for the roadway section. This “build out” scenario was based on a controlled access roadway with an elevated intersection at US 90 and contiguous right-turn lanes on both sides of a six lane road. The roadway configuration included a set of parallel service roads located in the rear of parcels fronting Capital Circle NW to serve site access requirements. Service roads are impractical and inconsistent with both the Regional Mobility Plan and the Comprehensive Plan.

The original Blueprint CCNW project also includes a greenway adjacent to Capital Circle that was never constructed. However, bike lanes and sidewalks on both sides of the road along this portion of Capital Circle (I-10-US 90) were constructed. Factors to consider in greenway construction for this segment include negative environmental impacts to Gum Swamp and that there are almost no significant parks and large residential areas in this area in which connectivity would be enhanced. Staff is not recommending this greenway be a part of the proposed Greenways Master Plan.

Cost By Themes

\$41,800,000	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Service Road (Capital Circle NW)



Project #34



Previous Project Information for:

Project #34

Service Road (Capital Circle NW)

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Capital Circle Northwest Service Road (*Previously Project #10*)

Blueprint 2000 Proposed Sales Tax Projects

Project Name: Capital Circle Northwest Service Road (Blueprint 2000 Map 2A)

Project Themes:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

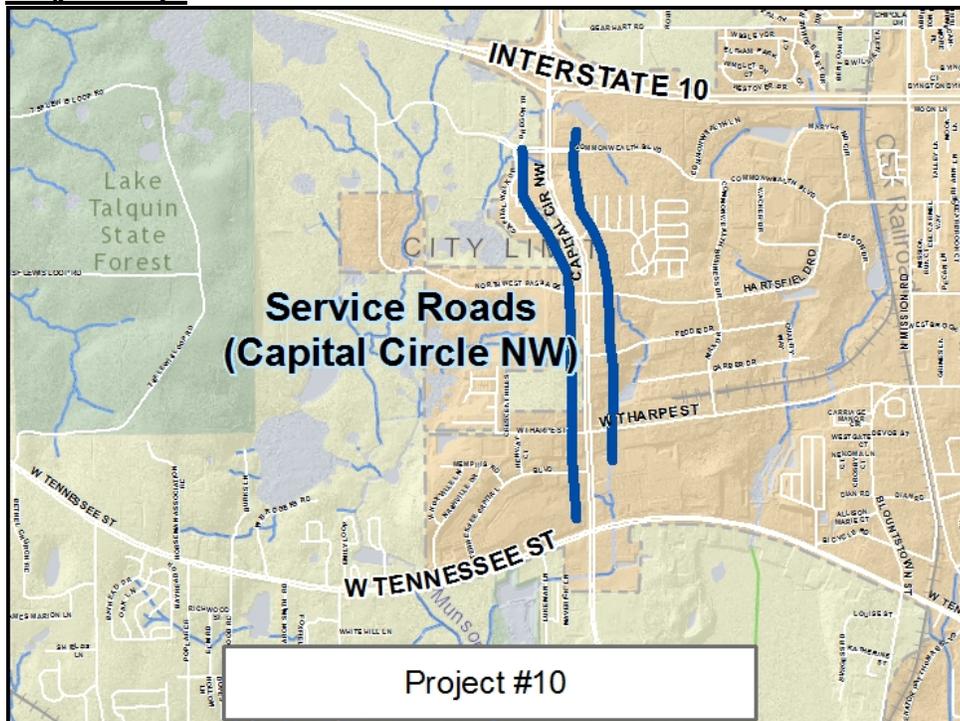
Project Description:

The Capital Circle, NW widening project from I-10 to US 90 (W. Tennessee Street) was completed in 2007. The EECC proposed enhancements to follow at a later date based on a BP2000/TLCDP revised “build out” master plan for the roadway section. This “build out” scenario would require the addition of a set of parallel service roads to serve site access requirements. Service roads located in the rear of parcels fronting Capital Circle, NW would provide primary site access and therefore limit driveways.

This project also proposes a greenway along Capital Circle NW linking the Lake Talquin State Forest to the multiuse trail on Capital Circle SW. Greenways are to be accomplished via acquisition and restoration of the remainder of Gum Swamp, west of Capital Circle, as well as a section north of Tennessee Street along the north branch of Gum Creek. A greenway connection to the Cascade Chain and Lake Munson is proposed along the West Drainage Ditch.

This project also includes development of a trail network from San Luis Mission Park to Tallahassee Community College. This would be accomplished through the proposed San Luis Greenway that is intended to provide a trail connection from residential areas north of Tennessee Street to Appleyard Drive for bicyclists and pedestrians.

Project Map:



SALES TAX PROJECTS

Project Number: 35
Project Name: Phase II: Water Quality Program

Total Project Cost: \$50,000,000

Total Project Cost Notes
NA

Executive Project Summary

This program provides funding from the Blueprint 2000 80% share of the sales tax extension for stormwater and water quality retrofit to be split 50/50 between City and County.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

This program provides funding from the Blueprint 2000 80% share of the sales tax extension for stormwater and water quality retrofit to be split 50/50 between City and County. Phase I of the Water Quality Program set aside \$50,000,000. Phase II of this program will continue to address water quality issues as they are identified in City and the County for stormwater and water quality retrofit.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
\$50,000,000	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Previous Project Information for:

Project #35

Phase II: Water Quality Program

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Phase II: Water Quality Program (Previously Project #9)

Blueprint 2000 Proposed Sales Tax Projects

Project Name: Phase II: Water Quality Program

Project Themes:

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

This program provides funding from the Blueprint 2000 80% share of the sales tax extension for stormwater and water quality retrofit to be split 50/50 between City and County. Phase I of the Water Quality Program set aside \$50,000,000. Phase II of this program will continue to address water quality issues as they are identified in City and the County for stormwater and water quality retrofit.

Project Map:

There is no map for this project.

SALES TAX PROJECTS

Project Number: 36
Project Name: Killlearn Estates Freshwater Restoration Project
Total Project Cost: \$10 million
Total Project Cost Notes

Executive Project Summary

Killlearn Homeowners Association (KHA) has requested \$10M from the Sales Tax Committee for the City of Tallahassee to restore and protect Lake Killlearn and Lake Hancock. COT has a MS4 permit from State to utilize these lakes, which are privately owned by KHA, for stormwater. The two specific issues that surround these water bodies pertain to the lakes' aesthetics and water quality. KHA is interested in resolving both issues with the requested \$10M.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

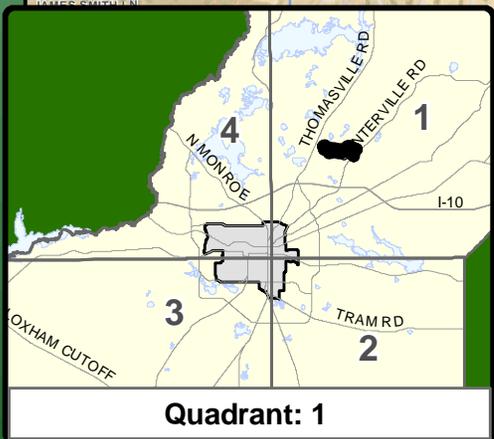
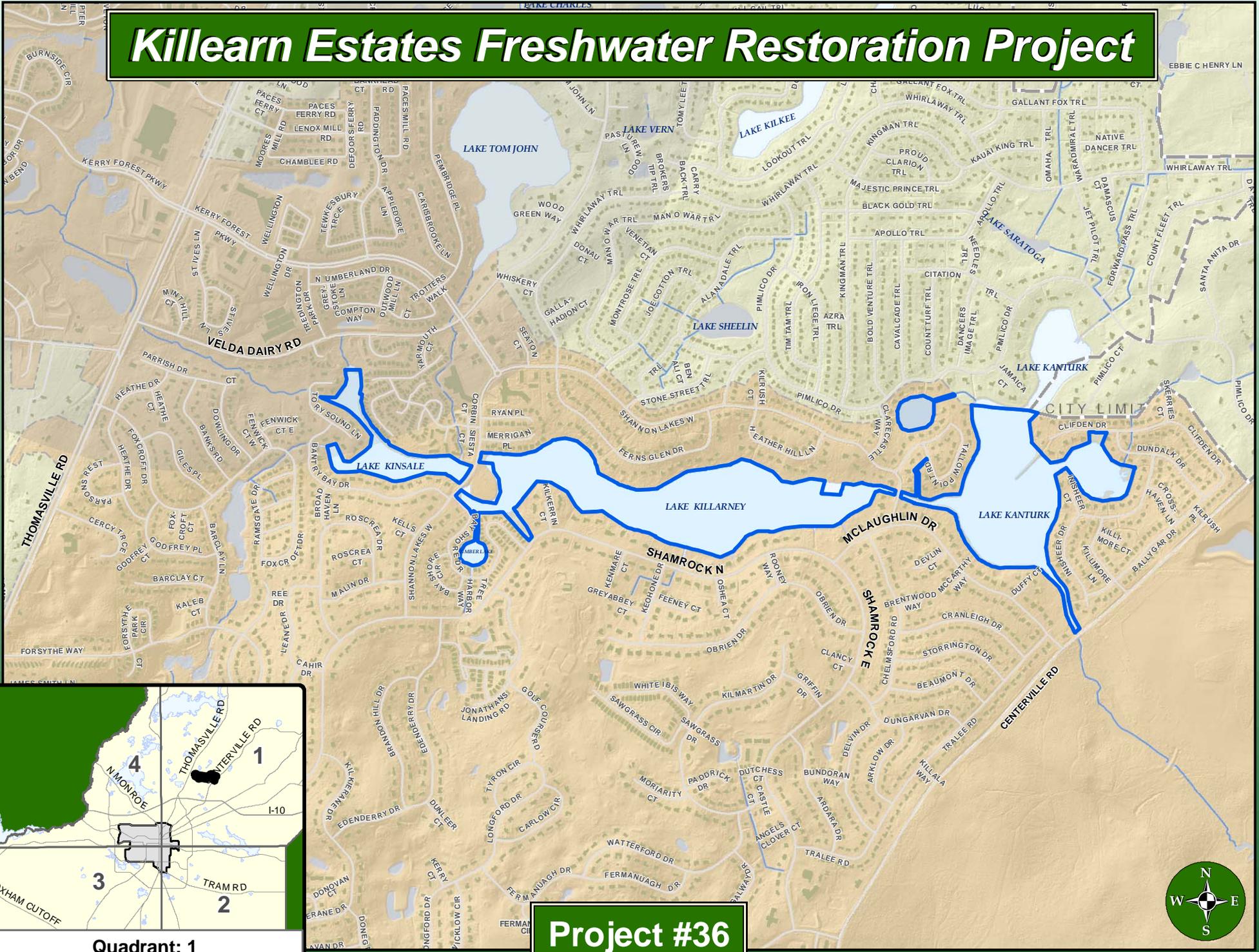
The proposed project would consist of building upstream treatment ponds as necessary to improve the water quality, while also removing sediments and over-excavating these lakes so as to create a permanent pool of water. A draft report from Florida Department of Environmental Protection (FDEP) has preliminarily identified these lakes as Impaired Water Bodies (water bodies of high pollutant levels). The City of Tallahassee is currently disputing that classification and working closely with FDEP to properly identify the pollutant levels and determining the appropriate corrective action, if any, needed to ensure the ponds' water quality.

The City staff does not support this request because the ponds are privately owned and the objectionable fluctuation in the pool levels within these ponds is hydrologic in nature. Therefore, any aesthetic improvements are the responsibility of the private owner. In addition, regarding the ponds' water quality, the City is currently working with FDEP to determine the appropriate levels of pollution within the ponds and to identify any corrective action needed.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
\$10 million	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Killarney Estates Freshwater Restoration Project



Quadrant: 1

Project #36



Previous Project Information for:

Project #36

Killearn Estates Freshwater Restoration Project

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Killearn Estates Stormwater Pond Management (*Previously Project #28*)

Proposed Sales Tax Projects by the Community

Project Name: Killlearn Estates Stormwater Pond Management (Project #28)

Project Themes:

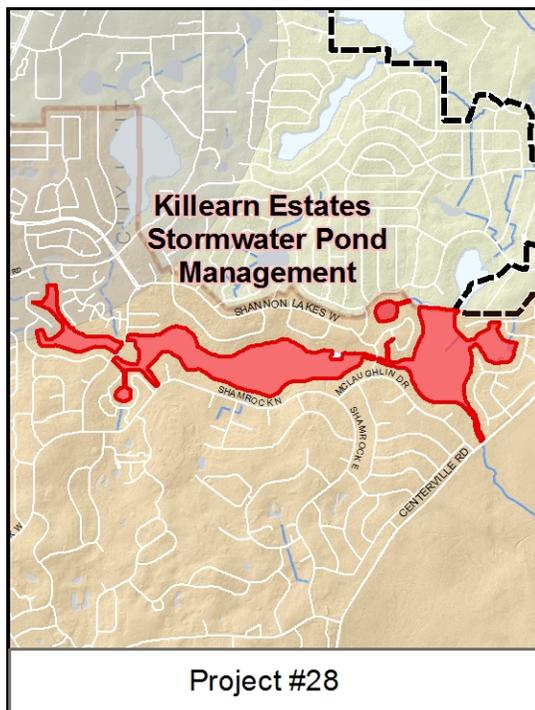
- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

Killlearn Homeowners Association (KHA) has requested \$10M from the Sales Tax Committee for the City of Tallahassee to restore and protect Lake Killlearn and Lake Hancock. COT has a MS4 permit from State to utilize these lakes, which are privately owned by KHA, for stormwater. The two specific issues that surround these water bodies pertain to the lakes' aesthetics and water quality. KHA is interested in resolving both issues with the requested \$10M. The proposed project would consist of building upstream treatment ponds as necessary to improve the water quality, while also removing sediments and over-excavating these lakes so as to create a permanent pool of water. A draft report from Florida Department of Environmental Protection (FDEP) has preliminarily identified these lakes as Impaired Water Bodies (water bodies of high pollutant levels). The City of Tallahassee is currently disputing that classification and working closely with FDEP to properly identify the pollutant levels and determining the appropriate corrective action, if any, needed to ensure the ponds' water quality.

The City staff does not support this request because the ponds are privately owned and the objectionable fluctuation in the pool levels within these ponds is hydrologic in nature. Therefore, any aesthetic improvements are the responsibility of the private owner. In addition, regarding the ponds' water quality, the City is currently working with FDEP to determine the appropriate levels of pollution within the ponds and to identify any corrective action needed.

Project Map:



SALES TAX PROJECTS

Project Number: 37
Project Name: Lake Lafayette Basin Improvements/Floodplain Protection and Eastgate Neighborhood Park/Greenspace
Total Project Cost: \$36,300,000
Total Project Cost Notes
NA

Executive Project Summary

The primary benefits of this project are the completion of a core-in-town greenway, significant flood control improvements, and recreational opportunities for the Eastgate community. The greenway system will provide linkages between Maclay State Gardens and Tom Brown Park. Flood control improvements include enhancement of the northeast drainage ditch and adjoining areas and restoration of Goose Pond.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

The primary benefits of this project are the completion of a core-in-town greenway, significant flood control improvements and recreational opportunities for the Eastgate community.

The greenway system will provide bicycle and pedestrian linkages between Timberlane Ravine and Goose Pond, extending to Tom Brown Park via Weems Pond at Mahan Drive and Capital Circle, NE. An additional greenway element is proposed to provide an alternative route from Tom Brown Park via Goose Pond to Maclay State Gardens. The development of trail crossings will be required.

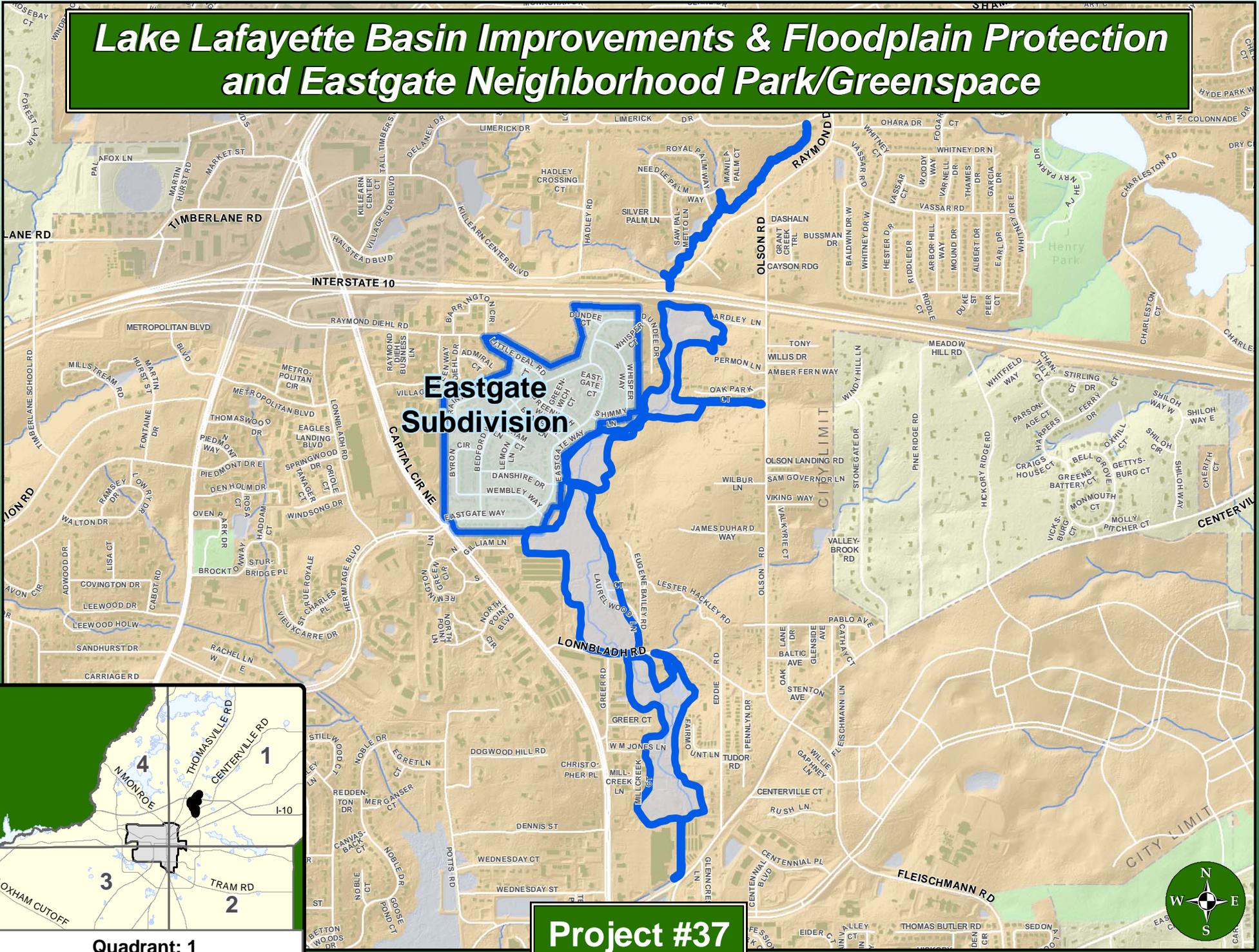
Enhancements are proposed for much of the northeast drainage ditch, including swales, culvert improvements, and limited in-stream storage (ponds) for flood control. On-line ponds are recommended north of Raymond Diehl Road and east of the Eastgate neighborhood, if feasible. Goose Pond should be excavated and restored. The floodplain adjoining the Northeast Ditch in the area east of the Eastgate neighborhood should be acquired to protect the resource, provide new greenway connections, and provide stormwater benefits.

This project provides greenspace and recreational opportunities for the Eastgate community. The Eastgate community currently does not have a greenspace or park in walking distance, which means that children and transportation disadvantaged citizens have no park access. The nearest park is AJ Henry, which although it is approximately one mile away, in fact does not service this neighborhood at all due to the barrier presented by Interstate 10.

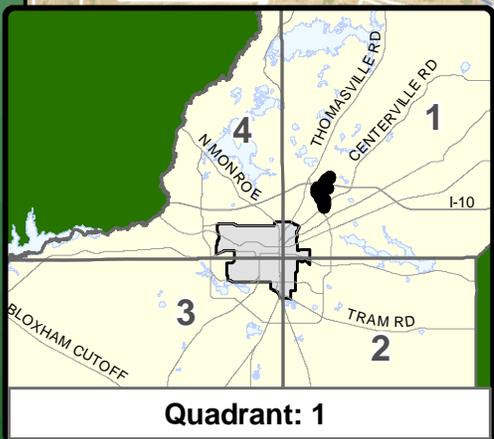
Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
\$35,800,000	Environmental/Water Quality
	Connectivity
	Economic Vitality
\$500,000	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Lake Lafayette Basin Improvements & Floodplain Protection and Eastgate Neighborhood Park/Greenpace



Eastgate Subdivision



Project #37



Previous Project Information for:

Project #37

Lake Lafayette Basin Improvements and Floodplain Protection and Eastgate Neighborhood Park/Greenspace

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Lake Lafayette Basin Improvements and Floodplain Protection
(Previously Project #5)

Eastgate Neighborhood Park/Green Space *(Previously Project
#21)*

Blueprint 2000

Proposed Sales Tax Projects

Project Name: Lake Lafayette Basin Improvements and Floodplain Protection (Blueprint 2000 Map 5B)

Project Themes:

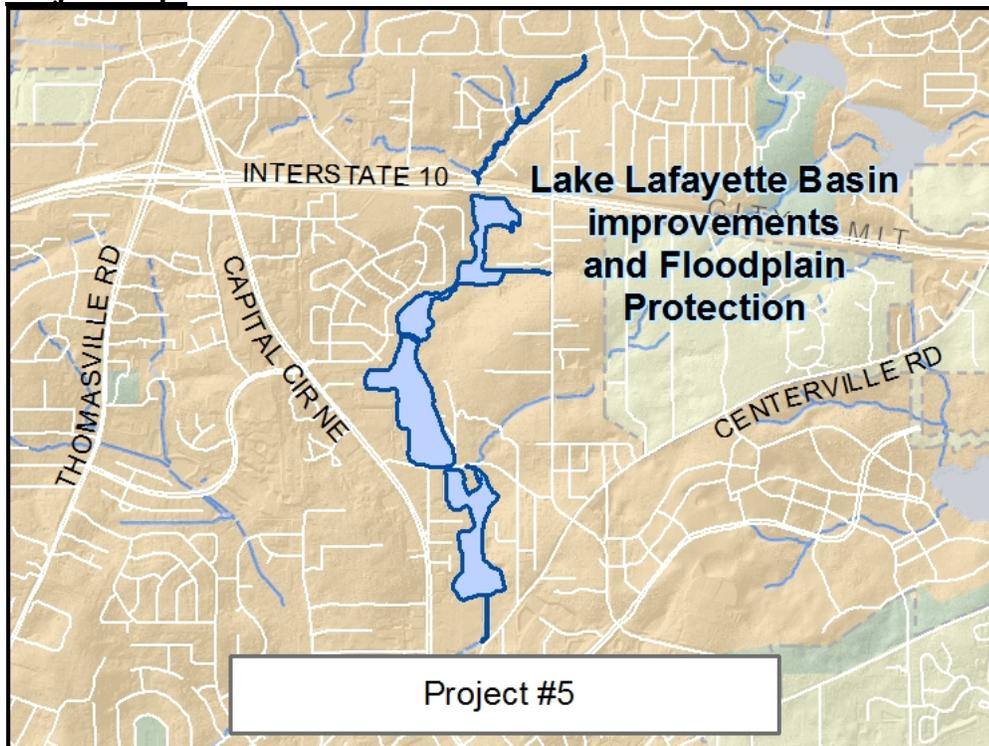
- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

The primary benefits of this project are the completion of a core-in-town greenway and significant flood control improvements. The greenway system will provide bicycle and pedestrian linkages between Timberlane Ravine and Goose Pond, extending to Tom Brown Park via Weems Pond at Mahan Drive and Capital Circle, NE. An additional greenway element is proposed to provide an alternative route from Tom Brown Park via Goose Pond to Maclay State Gardens. The development of trail crossings will be required.

Enhancements are proposed for much of the northeast drainage ditch, including swales, culvert improvements and limited in-stream storage (ponds) for flood control. On-line ponds are recommended north of Raymond Diehl Road and east of the Eastgate neighborhood, if feasible. Goose Pond should be excavated and restored. The floodplain adjoining the Northeast Ditch in the area east of the Eastgate neighborhood should be acquired to protect the resource, provide new greenway connections, and provide stormwater benefits.

Project Map:



Proposed Sales Tax Projects by the Community

Project Name: Eastgate Neighborhood Park / Green Space (Project #21)

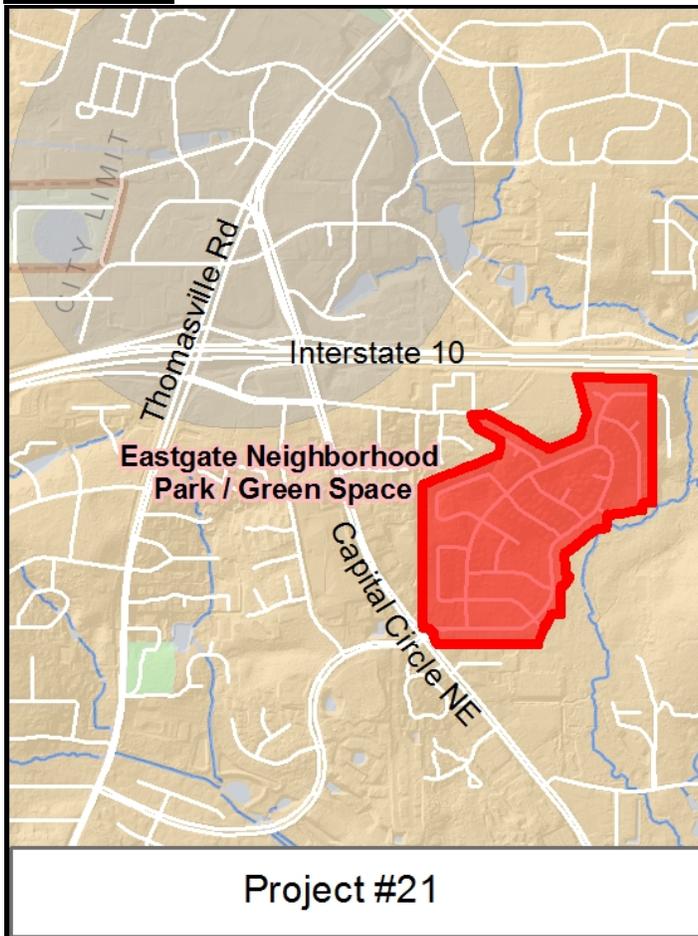
Project Themes:

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

This project provides greenspace and recreational opportunities for the Eastgate community. The Eastgate community currently does not have a greenspace or park in walking distance, which means that children and transportation disadvantaged citizens have no park access. The nearest park is AJ Henry, which although it is approximately one mile away, in fact does not service this neighborhood at all due to barrier presented by Interstate 10.

Project Map:



SALES TAX PROJECTS

Project Number: 38
Project Name: Downtown Stormwater Improvements
Total Project Cost: \$25 million
Total Project Cost Notes

Executive Project Summary

Although the Lake Elberta Regional Stormwater Facility has ample reserve capacity, there is an inadequate upstream conveyance system preventing potential development from tapping into that capacity. The proposed improvements to the stormwater conveyance system would College Avenue, Stadium, and Lake Bradford Road areas and allow future development to take advantage of the full capacity of the Lake Elberta Facility. This would further encourage redevelopment of the urban core. This project supports the goals of the College Avenue Placemaking Plan.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

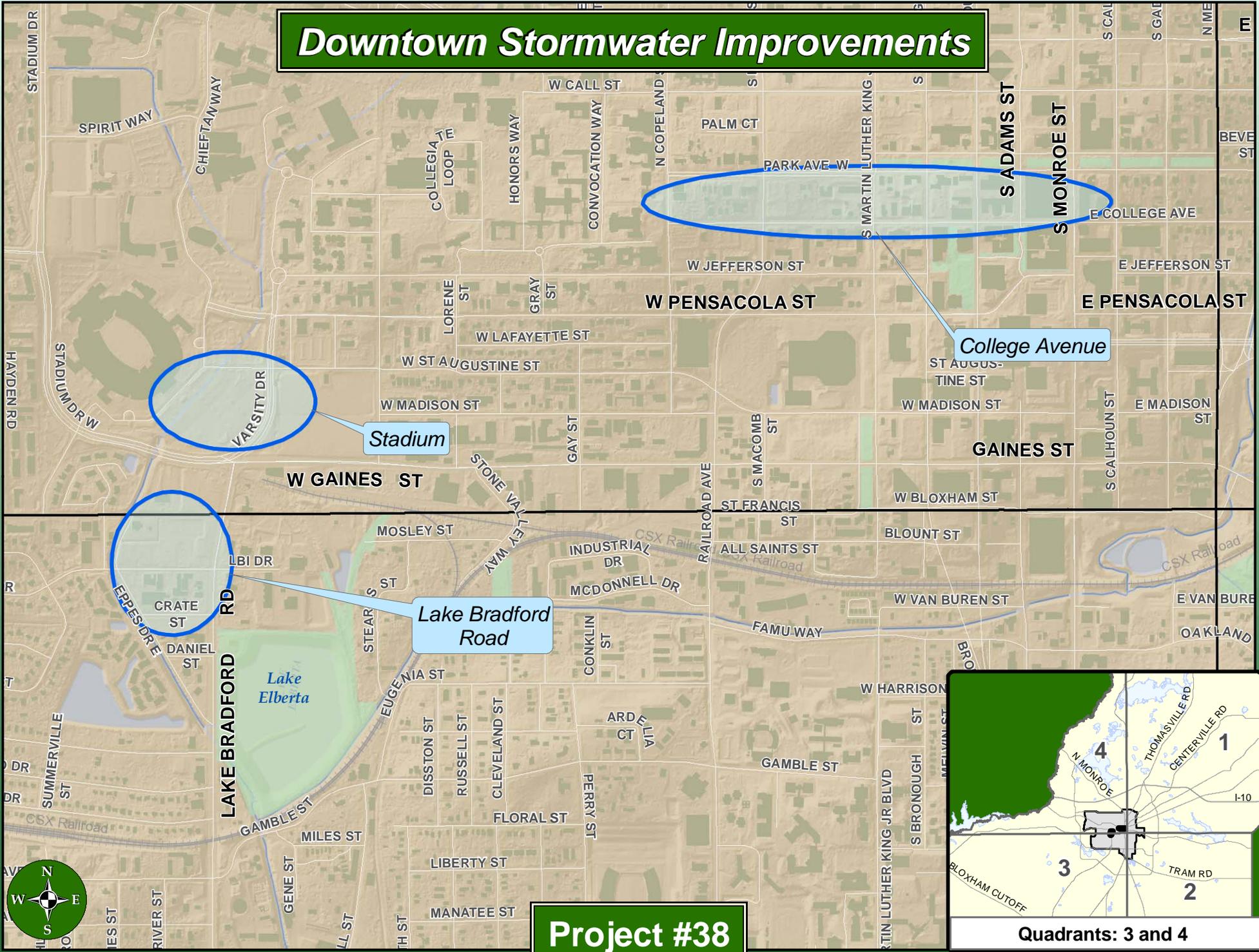
Detail Project Description

These projects would involve placing culverts underground to convey stormwater to Lake Elberta. This would allow redevelopment of the benefiting properties because land would no longer be needed onsite for stormwater ponds. For example, at right is construction of the Franklin Avenue conveyance system.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
\$25 million	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Downtown Stormwater Improvements

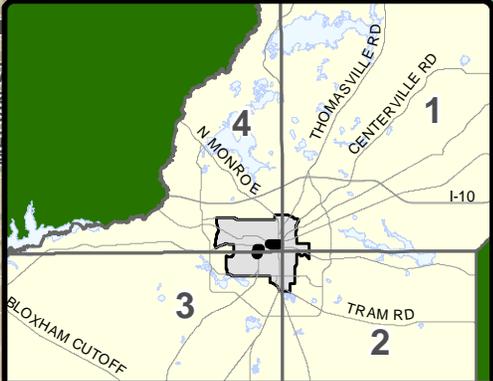


Stadium

College Avenue

Lake Bradford Road

Project #38



Quadrants: 3 and 4



Previous Project Information for:

Project #38

Downtown Stormwater Improvements

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: **Building the Core: Stormwater Improvements** (*Previously Project #20*)

Program
1b

Building the Core: Stormwater Improvements

Estimated Cost:
\$39 million

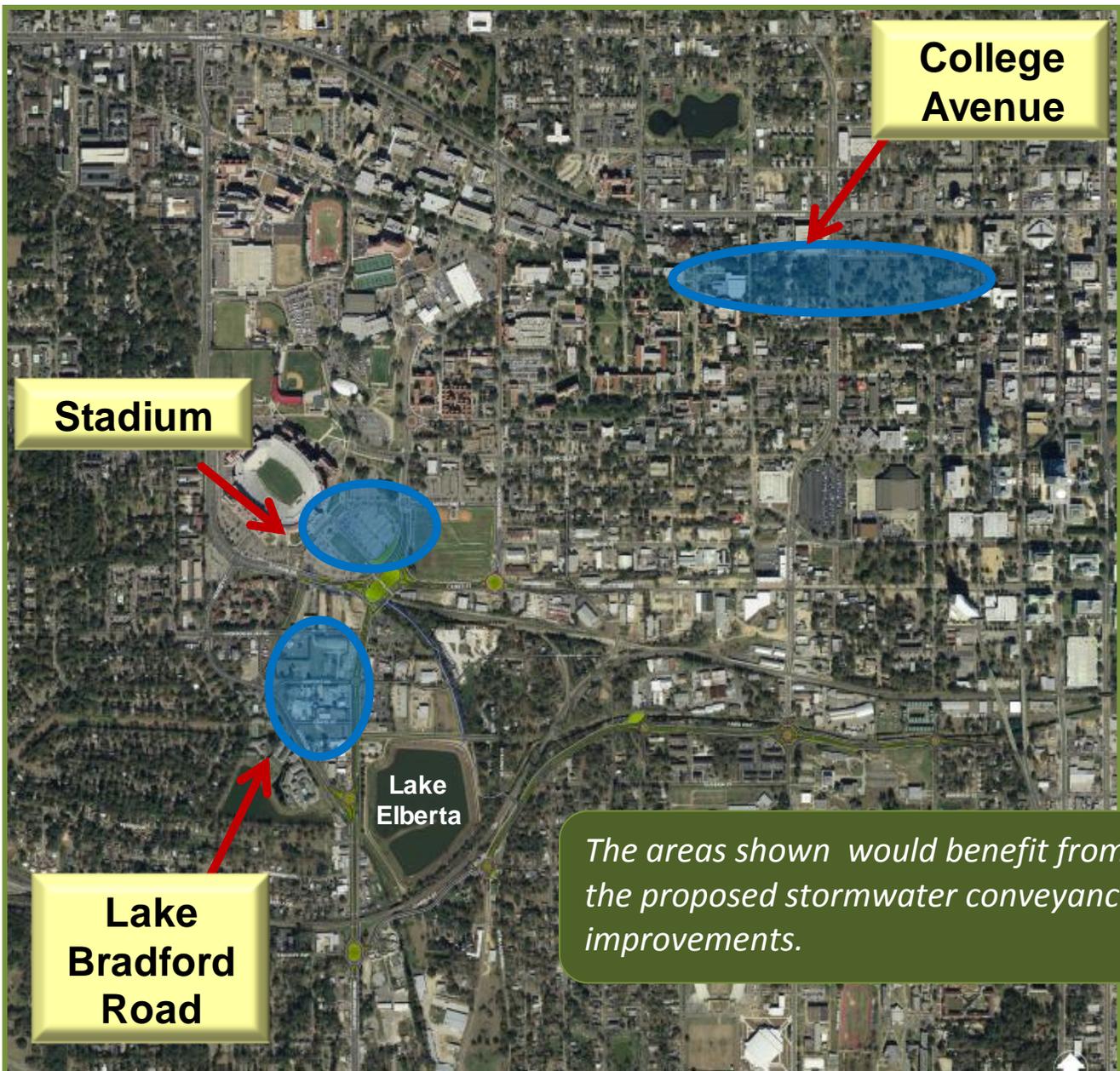
WHAT IS IT?

Although the Lake Elberta Regional Stormwater Facility has ample reserve capacity, there is an inadequate upstream conveyance system preventing potential development from tapping into that capacity. The proposed improvements to the stormwater conveyance system would College Avenue, Stadium, and Lake Bradford Road areas and allow future development to take advantage of the full capacity of the Lake Elberta Facility. This would further encourage redevelopment of the urban core.

Evaluation Criteria:

Provides Regional Impact / Geographic Equity ✓
Enhances Mobility
Supports Recreation and Quality of Life

Benefits the Environment ✓
Supports Economic Development / Revitalization ✓
On an Existing Master Plan or Blueprint List ✓
Leverages Other Funds ✓



WHY IS IT IMPORTANT?

Benefits the Environment – The project would benefit the environment by helping to ensure the adequate treatment of stormwater and urban runoff.

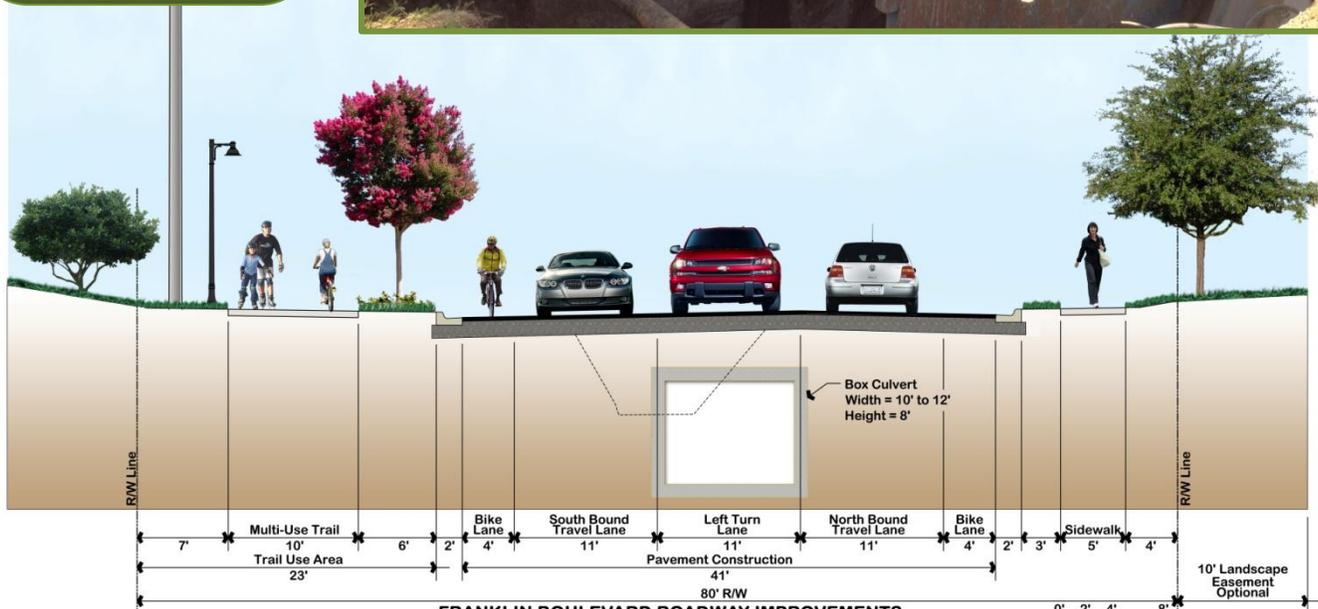
Provides Regional Impact / Geographic Equity – The project would allow the whole area to benefit from previous investments in stormwater capacity and reductions in flooding.

Supports Economic Development / Revitalization – The installation of underground conveyance facilities will allow development/redevelopment of infill sites currently limited by stormwater runoff.

On an Existing Master Plan or Blueprint List – The need for this project has been identified in the City's stormwater plan for several years.

Leverages Other Funds – This project leverages the investments made in the Lake Elberta Stormwater Facility by allowing the capacity in that facility to be used by upstream properties.

These projects would involve placing culverts underground to convey stormwater to Lake Elberta. This would allow redevelopment of the benefiting properties because land would no longer be needed onsite for stormwater ponds. For example, at right is construction of the Franklin Avenue conveyance system.



FRANKLIN BOULEVARD ROADWAY IMPROVEMENTS OPTION C - TYPICAL SECTION at Left Turn Lane

- 5' Sidewalk (East Side Only)
- Street Lighting TBD
- 10' Multi-Use Trail
- Electric Lines Remain Overhead
- 4' North and South Bike Lanes
- Left Turn Lane at Park Avenue
- Southbound Left Turn Lane at Jefferson Street

SALES TAX PROJECTS

Project Number: 39

Project Name: Alternative Sewer Solutions: Creating Comprehensive Wastewater Management for Leon County Unincorporated Area

Total Project Cost: \$67 million

Total Project Cost Notes

This project consists of three components, which are listed below:

Part I: Wastewater Management Plan and Execution (\$2.8 million)

Part II: Wastewater Treatment Facilities Plan Execution (\$50 million – \$62.2 million)

Part III: Connection Assistance Funding for existing Sewer (\$2.0 million)

Executive Project Summary

This project involves developing alternative methods of domestic wastewater treatment and disposal, rather than on-site sewage treatment and disposal systems (septic tanks) or connection to the City of Tallahassee central sewer system. Implementation of cluster disposal systems or package treatment and disposal facilities requires modification to the Tallahassee/Leon County Comprehensive Plan as directed by the Board of County Commissioners.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

On November 29, 2012, the Water Resources Committee (WRC) and the Wakulla Springs Alliance presented options on the alternatives to central sewer to the Committee. Subsequently, Robert Scanlon, the Chair of the WRC distributed a project proposal to the Committee for consideration. In the January 31, 2013 letter, Mr. Scanlon notes that the document has not been approved by the entire WRC but that the proposed project does represent a restatement of the previous presentation to the Committee. This project consists of three components, which are listed below:

Part I: Wastewater Management Plan and Execution (\$2.8 million)

Part II: Wastewater Treatment Facilities Plan Execution (\$50 million – \$62.2 million)

Part III: Connection Assistance Funding for existing Sewer (\$2.0 million)

Note: During the Leon County Board of County Commissioner's January 29, 2013 workshop on "Septic System Management Options," the Board provided the following direction to staff, which was ratified by the Board on during the February 12, 2013 meeting:

1. Direct staff to continue to pursue proposed sales tax extension project #10, Woodville Water Quality.
2. Direct staff to bring back a proposed amendment of the Code of Laws to establish an Advanced Wastewater Treatment nitrogen standard for new construction within the PSPZ, with means for managing those systems not on central sewer.
3. Direct staff to remain actively engaged in the BMAP process for the Upper Wakulla River and support further sampling to identify sources' relative nitrate loads, particularly from Inflow north of the Cody Scarp and at the state line, the scope of which will be established as part of the BMAP process.
4. Direct staff to include \$50,000 in funding in the FY 2013/14 budget for the Leon County Health Department to complete an inventory of all septic systems within Leon County, in coordination with TLC-GIS.
5. Direct staff to take no action at this time to institute a county-wide level 4 or 5 RME or Wastewater/Nutrient Management Utility.

6. Direct staff to pursue an amendment of the Code of Laws to require a county-wide 24-inch separation, between the bottom of the drainfield and wettest season water table, for the repair of failing septic systems.

7. Direct staff to initiate a Comprehensive Plan amendment in the 2014-1 cycle which maintains the goal of spring protection, but removes the technical specificity by which this can be achieved with respect to wastewater treatment.

In addition, on March 12, 2013, the Board approved modifications to amend the 2030 Sewer Masterplan, which deletes the requirement to include the additional area west of the Woodville Rural Community. The Board's priority/focus has been the development of the Woodville Rural Community node as a vibrant economic center. Central sewer is a key factor to allow the crucial density to occur. This area has been designated in the City's Sewer Masterplan for decades for connection to its system. The Woodville Urban Fringe area, due to its more rural character, has not historically been included in the Masterplan, and staff recommends that the City not be required to amend its Masterplan to include this more rural area. The removal of the request to amend the Sewer Masterplan to include the Woodville Urban Fringe area is consistent with all of the Board's actions to date. The Sales Tax Project #10 had three components, flood study, Woodville Rural Node Sewer, Woodville Urban Fringe Sewer. The project description is consistent with the approach being recommended in this agenda item to remove the urban fringe area from the City's 2030 Sewer Masterplan: provide central sewer to the Rural Community node and allow other AWT alternatives to be considered for the urban fringe area. This project can be found on page 36 of the Leon County Proposed Sales Tax Projects booklet.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
\$67 million	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Previous Project Information for:

Project #39
**Alternative Sewer Solutions: Creating
Comprehensive Wastewater Management for
Leon County Unincorporated Area (Nitrogen
Reduction and Infill Development)**

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Alternative Sewer Solutions (*Previously Project #43*)

Proposed Sales Tax Projects by the Community

Project Name: Alternative Sewer Solutions (Project #43)

Project Themes:

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

This project involves developing alternative methods of domestic wastewater treatment and disposal, rather than on-site sewage treatment and disposal systems (septic tanks) or connection to the City of Tallahassee central sewer system. Implementation of cluster disposal systems or package treatment and disposal facilities requires modification to the Tallahassee/Leon County Comprehensive Plan as directed by the Board of County Commissioners.

Description of Alternative Sewer Solutions:

Cluster Disposal Systems:

Cluster systems are where the discharge sewage from several individual homes is piped to a single larger septic tank and drainfield located separately from the homeowners' property. The septic tank and drainfield are sized to accommodate the number of homes connected. The goal would be to have economies of scale for several connected systems in order to provide a higher level of nitrogen treatment. While many studies are underway, there is currently no septic system that has been certified by the Department of Health to meet the 3mg/l treatment standard for nitrogen. Due to property acquisition and other concerns, cluster systems are not considered a good retrofit option, but could be considered for new development where the land for the common tank/drainfield can be designed into the overall layout of the development.

Package Treatment and Disposal Facilities:

Package plants are small centralized sewer systems where the sizing is customized specifically for a development or a small area. These plants are permitted by either the Department of Health or Florida Department of Environmental Protection depending on the capacity. The plants tend to have a small footprint, are capable of treating to advanced wastewater treatment (3 mg/l nitrogen) standards. They must be actively managed and maintained to achieve the required treatment levels.

Water Resources Committee:

On November 29, 2012, the Water Resources Committee (WRC) and the Wakulla Springs Alliance presented options on the alternatives to central sewer to the Committee. Subsequently, Robert Scanlon, the Chair of the WRC distributed a project proposal to the Committee for consideration. In the January 31, 2013 letter, Mr. Scanlon notes that the document has not been approved by the entire WRC but that the proposed project does represent a restatement of the previous presentation to the Committee. This project consists of three components, which are listed below (attachment #1):

- Part I: Wastewater Management Plan and Execution (\$2.8 million)
- Part II: Wastewater Treatment Facilities Plan Execution (\$50 million – \$62.2 million)
- Part III: Connection Assistance Funding for existing Sewer (\$2.0 million)

Note: During the Leon County Board of County Commissioner's January 29, 2013 workshop on "Septic System Management Options," the Board provided the following direction to staff, which was ratified by the Board on during the February 12, 2013 meeting:

Project #43: Alternatives to Central Sewer

Page 2

1. *Direct staff to continue to pursue proposed sales tax extension project #10, Woodville Water Quality.*
2. *Direct staff to bring back a proposed amendment of the Code of Laws to establish an Advanced Wastewater Treatment nitrogen standard for new construction within the PSPZ, with means for managing those systems not on central sewer.*
3. *Direct staff to remain actively engaged in the BMAP process for the Upper Wakulla River and support further sampling to identify sources' relative nitrate loads, particularly from Inflow north of the Cody Scarp and at the state line, the scope of which will be established as part of the BMAP process.*
4. *Direct staff to include \$50,000 in funding in the FY 2013/14 budget for the Leon County Health Department to complete an inventory of all septic systems within Leon County, in coordination with TLC-GIS.*
5. *Direct staff to take no action at this time to institute a county-wide level 4 or 5 RME or Wastewater/Nutrient Management Utility.*
6. *Direct staff to pursue an amendment of the Code of Laws to require a county-wide 24-inch separation, between the bottom of the drainfield and wettest season water table, for the repair of failing septic systems.*
7. *Direct staff to initiate a Comprehensive Plan amendment in the 2014-1 cycle which maintains the goal of spring protection, but removes the technical specificity by which this can be achieved with respect to wastewater treatment.*

In addition, on March 12, 2013, the Board approved modifications to amend the 2030 Sewer Masterplan, which deletes the requirement to include the additional area west of the Woodville Rural Community. The Board's priority/focus has been the development of the Woodville Rural Community node as a vibrant economic center. Central sewer is a key factor to allow the crucial density to occur. This area has been designated in the City's Sewer Masterplan for decades for connection to its system. The Woodville Urban Fringe area, due to its more rural character, has not historically been included in the Masterplan, and staff recommends that the City not be required to amend its Masterplan to include this more rural area. The removal of the request to amend the Sewer Masterplan to include the Woodville Urban Fringe area is consistent with all of the Board's actions to date. The Sales Tax Project #10 had three components, flood study, Woodville Rural Node Sewer, Woodville Urban Fringe Sewer. The project description is consistent with the approach being recommended in this agenda item to remove the urban fringe area from the City's 2030 Sewer Masterplan: provide central sewer to the Rural Community node and allow other AWT alternatives to be considered for the urban fringe area. This project can be found on page 36 of the Leon County Proposed Sales Tax Projects booklet.

Project Map:

N/A

January 31, 2013

Dear Members of the Leon County Sales Tax Committee:

The Leon County Water Resources Committee would like to thank you for allowing us to present a wastewater management and facility project for your consideration to be part of the next sales tax project list.

Due to time constraints, this specific document has not been reviewed and approved by the entire Water Resources Committee. It does however, represent a restatement of our previous presentation to you and the County Commission, the contents of which were reviewed and approved by the committee. We apologize for not being able to present you with a fully vetted document at this time. We hope that this will suffice for your discussions.

Sincerely,

Robert Scanlon
Chair, Leon County Water Resources Committee

Attachments:

1. Evaluation of County sewer proposals
2. Summary slide presented at the Sales Tax Committee
3. Summary slides presented at the County Commission workshop

Proposal: **Creating Comprehensive Wastewater Management for Leon County Unincorporated Area: Nitrogen Reduction and Infill Development**

Summary:

Cost	Component
\$2.8 M	Part I: Wastewater Management Plan and Execution
	Part II: Wastewater Treatment Facilities Plan Execution
\$2.2 M	Demonstration Cluster Facility
\$50-60 M	Other facilities for retrofit and urban infill
\$2.0 M	Part III: Connection Assistance Funding for existing Sewer

Part I: Wastewater Management Plan and Execution

Cost: \$2,800,000

This component starts with a study to determine the scope, responsibilities and funding for a Responsible Management Entity (RME) that would be tailored to the specific needs of the unincorporated portion of Leon County that is not served by a sewer utility. We have stated priorities as we believe are reflected in the Comprehensive Plan and other County policies.

1. Study options for establishment of a management entity that has characteristics of an EPA Level 4 or 5 of entity including:
 - Scope and Responsibilities
 - Oversight and Administrative Structure
 - Planning and Financing of RME

EPA Level 4 RME holds the permits for facilities, takes responsibility for aspects of operations and maintenance but does not own the facilities.

EPA Level 5 RME is both responsible for the facilities and owner thereof.

2. Adopt preferred options and establish the Responsible Management Entity
3. Establish Regulations following BCC directives for:
 - Wastewater Treatment Standard
 - Requirement for connection to facilities when available
4. Wastewater Treatment Facilities Plan
 - Develop an engineering study for the Primary Springshed Protection Zone (PSPZ) to determine appropriate scale, capacity and location of facilities: sewer, cluster and septic tanks

- Engineering standards for privately built facilities
5. Facilities Financing Plan
 - Facilities and large cost items: grants, sales tax
 - Establish user fees for operations and maintenance policy with specific values to be determined as facilities are built
 6. Woodville Basin Stormwater Treatment and Flood Study
 - Determine the engineering standards for stormwater treatment standards that support the concerns of nitrogen reduction in the PZPS
 - Determine the flooding pattern within the basin to provide information for development permitting and stormwater facilities.

Part II: Wastewater Treatment Facilities Plan Execution

Cost: Demonstration Cluster Facility: \$2,200,000

**Cost: Significant headway on total PSPZ nitrogen reduction and urban infill need:
\$50,000,000 to \$60,000,000**

This component calculates costs based on the estimates provided by the Lombardo Associates report (2011). According to this report, the estimates do not differ significantly among sewer, cluster or septic systems that meet AWT nitrogen treatment standards. The total cost about \$22,000 / unit, retrofit or new regardless of technology used: sewer, cluster or septic systems

At the County Commission workshop presentation we suggested that this large sum of money be divided as follows:

1. **Demonstration cluster facility:**
 - ~100 parcels, in PSPZ, Land Use with greatest potential for economic development and private need for capacity increase. The total cost would be \$2.2M assuming \$22,000 for each parcel served. The actual cost or number of parcels served would depend upon the distribution of capital costs between the builder of the facility and the users.
2. **Other facilities:**
 - residential retrofit or new residential development, approximately \$22,000 per unit (residential or commercial)
 - \$50,000,000 to \$60,000,000 for

The Lombardo report estimates the entire cost of meeting the TMDL for Wakulla Springs will, most likely, require AWT nitrogen standards for all wastewater systems in the PSPZ. This requires substantial retrofit of the approximate 7800 septic tanks, the vast majority of which are in Leon County. The total cost was estimated to be \$221 million.

Therefore, the Water Resources Committee proposes that a large amount of money be provided to make significant headway in the reduction of the nitrogen contribution

through the use of sewer, cluster and retrofit of septic tanks (as allowed by State law). In review of the County Sales Tax Projects that included four sewer projects, we indicated the importance of balancing nitrogen reduction, existing land use, future development potential and the capacity of wastewater facilities. The table we used to summarize our review is attached.

Part III: Connection Assistance Funding

Cost: \$2,000,000

This fund is for assistance to connecting owners to existing sewer in both the City of Tallahassee of in the unincorporated area. The form of the assistance, e.g. loan, grant, etc. is to be determined.

The connection costs for new facilities or extension of COT sewer is included in the project costs of the Wastewater Treatment Facilities Plan. The actual total cost of these projects or the capacity of the facilities will depend upon how the capital costs are shared. Connection costs for new development are the responsibility of the owners.

SALES TAX PROJECTS

Project Number: 40
Project Name: Woodville Water Quality
Total Project Cost: \$25.8 million

Total Project Cost Notes

This project cost is only for the sanitary sewer mainline construction only. Funding regarding landowner assistance for sewer hookups is discussed in Project #45.

Executive Project Summary

This project implements direction provided in the Comprehensive Plan and external reports on the protection of Wakulla Springs. Septic systems in the Woodville area of Leon County have been identified as a major source of nitrate loading to the groundwater and Wakulla Springs. Wakulla Springs is a first magnitude spring and is the longest and deepest known submerged freshwater cave system in the world. Located just five miles south of the Leon County line, Wakulla Springs is an important part of the regional culture and recreational economy.

The proposed project includes three components to help reduce existing nitrate loading and manage flooding issues for future development in the area. This project involves two major features:

- 1) The Woodville Recharge Basin Flood Study to document areas susceptible to closed depression flooding and to protect these areas from further development.
- 2) Construct sanitary sewer lines in the Woodville Rural Community to provide access to nitrogen-reducing central sewer facilities. Access to central sewer will address high nitrogen loads from existing septic tanks as well as allow for higher density new development, redevelopment, and expanded commercial opportunities in Woodville Rural Community.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

1. Woodville Recharge Basin Flood Study

The flood study will evaluate closed-depression flooding throughout the Woodville Recharge Basin following high-volume storm events to ensure that future development occurs in appropriate locations. Leon County's experience in 1994 was that smaller closed depression areas were overwhelmed during tropical storms and began cascading to adjacent depressions. The product of this study will be a map indicating high water elevations and susceptible areas to be protected from development.

2. Woodville Rural Community Advanced Wastewater Treatment Service

The Woodville Rural Community begins three miles south of Capital Circle Southeast and includes approximately 2,600 acres. The proposed project will provide the rural community with access to sewer facilities designed to achieve Advanced Wastewater Treatment standards. This project would allow for higher density new development, redevelopment, and expanded commercial opportunities. Additionally, Advanced Wastewater Treatment reduces potential nitrate loading to the groundwater by approximately a factor of ten. Reducing the nitrate load helps to protect groundwater and achieve restoration goals for Wakulla Springs.

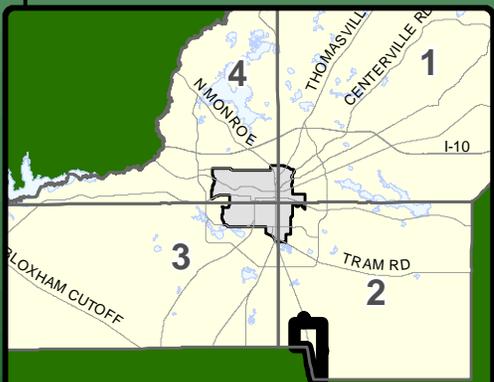
The County and City may choose to develop a landowner connection incentive program and financing option to address the financial burden and allow for sufficient connections to ensure that maximum benefits are achieved from the public capital investment in the sewer system. This incentive program is described in Project 45.

The Urban Fringe area west of the Rural Community will be evaluated for onsite nitrogen-reducing technology in lieu of central sewer due to the reduced development density.

Cost By Themes

[Redacted]	Regional Mobility/Transportation
[Redacted]	Sense of Community
\$25.8 million	Environmental/Water Quality
[Redacted]	Connectivity
[Redacted]	Economic Vitality
[Redacted]	Ecotourism/Parks
[Redacted]	Vertical Infrastructure
[Redacted]	Gateways

Woodville Water Quality*



Quadrant: 2

Project #40



Previous Project Information for:

Project #40

Woodville Water Quality

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Woodville Water Quality (*Previously Project #63*)

Proposed Projects for the Sales Tax Extension

Project #10: Woodville Water Quality

Estimated Project Cost: \$59.2 Million

Project Criteria:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input checked="" type="checkbox"/> Water Quality Enhancements and Green Infrastructure |
| <input checked="" type="checkbox"/> Regional Mobility | <input checked="" type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input type="checkbox"/> Greenway Master Plan | <input type="checkbox"/> Transportation Capacity Improvements |
| <input type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input type="checkbox"/> Complements BP2000 Project | |

Project Description:

This project implements direction provided in the Comprehensive Plan and external reports on the protection of Wakulla Springs. Septic systems in the Woodville area of Leon County have been identified as a major source of nitrate loading to the groundwater and Wakulla Springs. Wakulla Springs is a first magnitude spring and is the longest and deepest known submerged freshwater cave system in the world. Located just five miles south of the Leon County line, Wakulla Springs is an important part of the regional culture and recreational economy. This project may also include the acquisition of the Chason Wood property for environmental preservation. However, the Intergovernmental Agency may wish to acquire this property through the existing Blueprint 2000's Sensitive Lands account.

The proposed project includes three components to help reduce existing nitrate loading and manage flooding issues for future development in the area. These components are detailed in the Stormwater/Sewer Capacity Improvements under the Project Criteria details.

1. *Woodville Recharge Basin Flood Study (\$1.3 million):* An evaluation necessary to document areas susceptible to closed depression flooding and to plan for protection.
2. *Woodville Rural Community Advanced Wastewater Treatment Service (\$24.5 million):* Provide access to nitrogen reducing central sewer facilities and allow for higher density new development, redevelopment, and expanded commercial opportunities in Woodville Rural Community.
3. *Urban Fringe West of Woodville Advanced Wastewater Treatment Service (\$33.4 million):* Provide access to nitrogen reducing wastewater facilities.

(Note: This estimate does not include the individual landowner sewer connection costs. The County and City may choose to develop an incentive program and financing option to address the financial burden and allow for sufficient connections to ensure that maximum benefits are achieved from the public capital investment in the sewer system.)

Additional Materials Regarding the Woodville Water Quality Project:

- *Attachment #1:* February 12, 2013 Ratification of Board Actions Taken at the January 29, 2013 "Workshop on the Septic System Management Options Report by Lombardo Associates, Inc. and Update on the BMAP Process"*
- *Attachment #2:* March 12, 2013 Board of County Commissioners Agenda Item on the Approval of Additional Woodville Sewer Requirements for the City of Tallahassee 2030 Master Plan*

*Note: Due to the size of the attachments accompanying these agenda items, each attachment will be available to you upon request.

Project #10: Woodville Water Quality

Page 2

Project Criteria Detail:

Comprehensive Plan: This project complies with policy direction to concentrate new development in the Woodville Rural Community and utilize connection to sewer facilities designed to achieve Advanced Wastewater Treatment standards to help minimize impacts on groundwater quality. The Flood Study will ensure compliance with the policy regarding flood control level of service for the existing and new development.

Regional Mobility Plan: The Woodville Rural Community is a designated regional growth node in the Regional Mobility Plan. Wastewater infrastructure investments would allow for this growth.

Greenway Master Plan: N/A

Connectivity: N/A

Complements Blueprint 2000 Project(s): N/A

Water Quality Enhancements and Green Infrastructure: This project provides access to Advanced Wastewater Treatment. This higher level of treatment reduces potential nitrate loading to the groundwater by approximately a factor of ten. The study to identify storm volume-sensitive depressions will provide opportunities to implement low-impact development practices.

Stormwater/Sewer Capacity Improvements: This project addresses sewer target areas identified in the 2030 Master Sewer Plan. The Woodville Rural Community was identified as a target area for sewer extension in 1988 and again as part of the 2030 Master Sewer Plan. On April 26, 2011 the Board provided direction to add the Urban Fringe area west of Woodville as a target area. The Flood Study will ensure development is directed to appropriate locations in the Woodville Recharge Basin and protect existing volume-sensitive areas.

This project includes three components that will improve stormwater and sewer capacity as well as help reduce existing nitrate loading and manage future development in the area.

1. Woodville Recharge Basin Flood Study

The flood study will evaluate closed depression flooding throughout the Woodville Recharge Basin following high-volume storm events to ensure that future development occurs in appropriate locations. Leon County's experience in 1994 was that smaller closed depression areas were overwhelmed during tropical storms and began cascading to adjacent depressions. The product of this study will be a map indicating high water elevations and susceptible areas to be protected from development.

2. Woodville Rural Community Advanced Wastewater Treatment Service

The Woodville Rural Community begins three miles south of Capital Circle Southeast and includes approximately 2,600 acres. The proposed project will provide the rural community with access to sewer facilities designed to achieve Advanced Wastewater Treatment standards. This project would allow for higher density new development, redevelopment, and expanded commercial opportunities. Additionally, Advanced Wastewater Treatment reduces potential nitrate loading to the groundwater by approximately a factor of ten. Reducing the nitrate load helps to protect groundwater and achieve restoration goals for Wakulla Springs.

The Comprehensive Plan specifically recognizes the Woodville Rural Community on the Future Land Use Map and includes policy direction that “new development shall be concentrated in the urban service area plus in the Woodville Rural Community...” Policies for this area allow commercial development

Project #10: Woodville Water Quality

and residential development up to four units per acre, with an option to achieve up to eight units per acre through a Transfer of Development Units system that requires the protection of vulnerable lands outside of the designated rural community. The Comprehensive Plan also specifically identifies connection to sewer facilities designed to achieve Advanced Wastewater Treatment standards as the preferred method of wastewater treatment within the Woodville Rural Community to help minimize impacts on the groundwater.

3. Urban Fringe West of Woodville Advanced Wastewater Treatment Service

This Urban Fringe area expands west from Woodville Rural Community to include areas around Wakulla Springs Road and Crawfordville Highway. The 3,850 acre area was designated as a target sewer area by the Board of County Commissioners on April 26, 2011. Urban Fringe development is allowed at up to one unit per three acres and the Comprehensive Plan limits the use of central sewer to existing development or new conservation subdivisions. The Comprehensive Plan would need to be amended if there is a desire to allow central sewer to be used by future conventional subdivision in the Urban Fringe. The focus of this component of the project will be conversion of existing septic systems to Advanced Wastewater Treatment service.

Both the Woodville Rural Community and the adjacent Urban Fringe area are located inside the Primary Springs Protection Zone, an area designated in the Leon County Land Development Regulations based on the high potential for contaminants such as wastewater to reach the groundwater. A 2011 septic system management report by Lombardo Associates Inc., commissioned by Leon County, Wakulla County, and The City of Tallahassee, concluded that nearly all of the septic systems in this area of Leon County will need to be converted to Advanced Wastewater Treatment in order to achieve the regulatory goal for nitrates in Wakulla Springs. The report also identified Woodville and the Urban Fringe area west of Woodville as candidates for connection to City sewer or for development of a cluster treatment system than can achieve Advanced Wastewater Treatment standards.

The table below provides basic information on the size and costs for the two project areas and separates the capital costs from the landowner connection costs for both areas. The capital cost includes engineering and construction to provide landowners with access to sewer. The connection cost includes the individual landowner expenses to abandon a septic system, provide plumbing from the street to the house, and pay system charges. These individual landowner expenses have not been included in the project cost.

Project Area	Acres	Existing Septic Systems	Capital Costs for Sewer	Landowner Connection Costs (\$12,000/system)	Capital+ Connection Costs
Woodville	2,600	1,274	\$24.5 million	\$15.3 million	\$39.8 million
Urban Fringe	3,850	1,671	\$33.4 million	\$20 million	\$53.4 million
Totals	6,450	2,945	\$57.9 million	\$35.3 million	\$93.2 million

The County and City may choose to develop an incentive program and financing option to address the financial burden of individual landowner connection costs and allow for sufficient connections to ensure that maximum benefits are achieved from the public capital investment in the sewer system. Individual landowners will also be required to pay any monthly bills from the sewer provider (approximately \$65/month).

Transportation Capacity Improvements: N/A

Core Infrastructure: This project is considered a core infrastructure project due to the sewer access improvements and water quality enhancements as detailed above.

Leon County Board of County Commissioners

Cover Sheet for Agenda #2

February 12, 2013

To: Honorable Chairman and Members of the Board

From: Vincent S. Long, County Administrator 

Title: Ratification of Board Actions Taken at the January 29, 2013 “Workshop on the Septic System Management Options Report by Lombardo Associates, Inc. and Update on the BMAP Process”

County Administrator Review and Approval:	Vincent S. Long, County Administrator
Department/ Division Review and Approval:	Tony Park, P.E., Public Works and Community Development Director Wayne Tedder, Planning, Land Management, and Community Enhancement Director David McDevitt, Development Support and Environmental Management Director
Lead Staff/ Project Team:	Katherine G. Burke, P.E., Engineering Services Director Theresa Heiker, P.E., Stormwater Management Coordinator John Kraynak, P.E., Environmental Services Director Alex Mahon, Environmental Manager Brian Wiebler, Principal Planner, Comprehensive Planning Team Laura Youmans, Assistant County Attorney Kim Dressel, Senior Assistant to the County Administrator

Fiscal Impact:

This item does have a financial impact. Staff recommends providing \$50,000 in funding in the FY 2013/14 budget for the Leon County Health Department to complete an inventory of all septic systems within Leon County.

Staff Recommendation:

Option #1: Ratify the Board actions taken at the January 29, 2013 “Workshop on the Septic System Management Options Report by Lombardo Associates, Inc. and Update on the BMAP Process.”

Title: Ratification of Board Actions Taken at the January 29, 2013 “Workshop on the Septic System Management Options Report by Lombardo Associates, Inc. and Update on the BMAP Process”

February 12, 2013

Page 2

Report and Discussion

Background:

On January 29, 2013, the Board held a workshop to discuss a Phase 1 feasibility report, prepared by Lombardo Associates, Inc. (LAI), to identify wastewater treatment options to reduce nitrate load to Wakulla Springs from septic systems, where central sewer is not available, as well as associated management options. Related issues were also discussed during the workshop, including: the proposed Woodville Water Quality sales tax extension project, which would decrease the amount of nitrate reaching Wakulla Springs from septic systems; Florida Department of Environmental Protection’s Upper Wakulla River and Wakulla Springs Basin Management Action Plan process that is currently underway and, once finalized, will become binding upon the responsible parties; proposed amendments to the Comprehensive Plan and Code of Laws; the Wakulla Springs Alliance petition; and recommendations from the Science Advisory and Water Resources committees, as well as the Wakulla Springs Alliance.

The workshop item provided relevant background information regarding: the Cody Scarp; nitrate concentrations; the City’s Advanced Wastewater Treatment Project; the Leon County Aquifer Vulnerability Assessment (LAVA) and Primary Springs Protection Zone (PSPZ); actions the County has taken to limit the amount of nitrate reaching Wakulla Springs from the Leon County area; a previously-considered draft nitrogen-reducing systems ordinance; and HB 1263, which pre-empts Leon County’s ability to phase-in the replacement of failing traditional septic systems with nitrogen-reducing systems as part of an inspection program. A copy of LAI’s report and additional analysis regard the report were also included in the workshop item.

A portion of the discussion during the workshop centered upon central sewer not being the only method by which nitrogen-reducing wastewater treatment may be achieved; that decentralized wastewater treatment systems, such as cluster systems and other technologies, are viable options for certain areas; and the link between land use planning and wastewater treatment and disposal.

Title: Ratification of Board Actions Taken at the January 29, 2013 “Workshop on the Septic System Management Options Report by Lombardo Associates, Inc. and Update on the BMAP Process”

February 12, 2013

Page 3

Analysis:

The Board provided the following directions to staff during the workshop:

1. Direct staff to continue to pursue proposed sales tax extension project #10, Woodville Water Quality.
2. Direct staff to bring back a proposed amendment of the Code of Laws to establish an Advanced Wastewater Treatment nitrogen standard for new construction within the PSPZ, with means for managing those systems not on central sewer.
3. Direct staff to remain actively engaged in the BMAP process for the Upper Wakulla River and support further sampling to identify sources’ relative nitrate loads, particularly from Inflow north of the Cody Scarp and at the state line, the scope of which will be established as part of the BMAP process.
4. Direct staff to include \$50,000 in funding in the FY 2013/14 budget for the Leon County Health Department to complete an inventory of all septic systems within Leon County, in coordination with TLC-GIS.
5. Direct staff to take no action at this time to institute a county-wide level 4 or 5 RME or Wastewater/Nutrient Management Utility.
6. Direct staff to pursue an amendment of the Code of Laws to require a county-wide 24-inch separation, between the bottom of the drainfield and wettest season water table, for the repair of failing septic systems.
7. Direct staff to initiate a Comprehensive Plan amendment in the 2014-1 cycle which maintains the goal of spring protection, but removes the technical specificity by which this can be achieved with respect to wastewater treatment.

Options:

1. Ratify the Board actions taken at the January 29, 2013 “Workshop on the Septic System Management Options Report by Lombardo Associates, Inc. and Update on the BMAP Process.”
2. Do not ratify the Board actions taken at the January 29, 2013 “Workshop on the Septic System Management Options Report by Lombardo Associates, Inc. and Update on the BMAP Process.”
3. Board direction.

Recommendation:

Option #1.

Attachment:

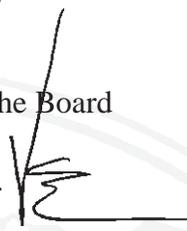
1. January 29, 2013 Workshop Item

Leon County Board of County Commissioners

Cover Sheet for Agenda #24

March 12, 2013

To: Honorable Chairman and Members of the Board

From: Vincent S. Long, County Administrator 

Title: Approval of Modification of Additional Woodville Sewer Requirements for City of Tallahassee 2030 Masterplan

County Administrator Review and Approval:	Vincent S. Long, County Administrator
Department/ Division Review:	Alan Rosenzweig, Deputy County Administrator Tony Park, P.E. Director of Public Works and Community Development
Lead Staff/ Project Team:	Kathy Burke, P.E. – Director of Engineering Services

Fiscal Impact:

This item has no direct fiscal impact to the county.

Staff Recommendation:

Option #1: Approve the modification provided to the City of Tallahassee to amend their 2030 Sewer Masterplan, deleting the requirement to include the additional area west of the Woodville Rural Community.

Title: Approval of Modification of Additional Woodville Sewer Requirements for City of Tallahassee 2030 Masterplan
March 12, 2013
Page 2

Report and Discussion

Background:

The City of Tallahassee (City) is required by the Interlocal Agreement with the County to provide periodic updates to its Sewer Masterplan (Attachment #1). The Masterplan serves as the guide for the City's annual submittal of their proposed five-year Capital Improvement Program (CIP). The 2005 Water Sewer Agreement (WSA) provides that the County has reserved the right to identify areas to be considered with a higher priority for sewer service. The County is required to construct the collection system within the target area, and the City agrees to provide the conveyance connecting the system to the city. Further, the City agrees to operate and maintain the system once it is built. For each system to be built, there would be a separate Implementation Agreement executed.

Attachment #2 provides an extensive background regarding the County's input to the City's Sewer Masterplan and the associated proposed sewer capital projects. The specific area being addressed in this agenda item relates to the April 12, 2011 workshop when the Board approved the 2030 Master Sewer Plan Phase 2 to include the addition of the area west of the Woodville Rural Community for future sewer (Attachment #3).

Analysis:

Given the Comprehensive Plan designation of the Woodville Rural Community as a development node, combined with the plan's transfer of densities to within this node, it is desired that economic development occur in this designated rural community node. In order for the Comprehensive Plan densities to be possible, central sewer is required. Central sewer within Woodville Rural Community node remains a viable alternative.

However, considering that the densities of the Urban Fringe (areas west of the Woodville Rural Community) may not support central sewer, it may not be prudent to proceed with a central sewer option for the Woodville Urban Fringe. Onsite septic tank technologies continue to improve and it is expected that a cost-effective onsite system that meets Advanced Wastewater Treatment (AWT) standards could be developed in the relatively near future.

The Board's priority/focus has been the development of the Woodville Rural Community node as a vibrant economic center. Central sewer is a key factor to allow the crucial density to occur. This area has been designated in the City's Sewer Masterplan for decades for connection to its system. The Woodville Urban Fringe area, due to its more rural character, has not historically been included in the Masterplan, and staff recommends that the City not be required to amend its Masterplan to include this more rural area.

The removal of the request to amend the Sewer Masterplan to include the Woodville Urban Fringe area is consistent with all of the Board's actions to date. The Sales Tax Project #10 had three components, flood study, Woodville Rural Node Sewer, Woodville Urban Fringe Sewer. The project description is consistent with the approach being recommended in this agenda item to remove the urban fringe area from the City's 2030 Sewer Masterplan: provide central sewer to the Rural Community node and allow other AWT alternatives to be considered for the urban fringe area.

Title: Approval of Modification of Additional Woodville Sewer Requirements for City of Tallahassee 2030 Masterplan
March 12, 2013
Page 3

Options:

1. Approve the modification provided to the City of Tallahassee to amend their 2030 Sewer Masterplan deleting the requirement to include the additional area west of the Woodville Rural Community.
2. Do not approve the modification provided to the City of Tallahassee to amend their 2030 Sewer Masterplan to delete the requirement to include the additional area west of the Woodville Rural Community.
3. Board direction.

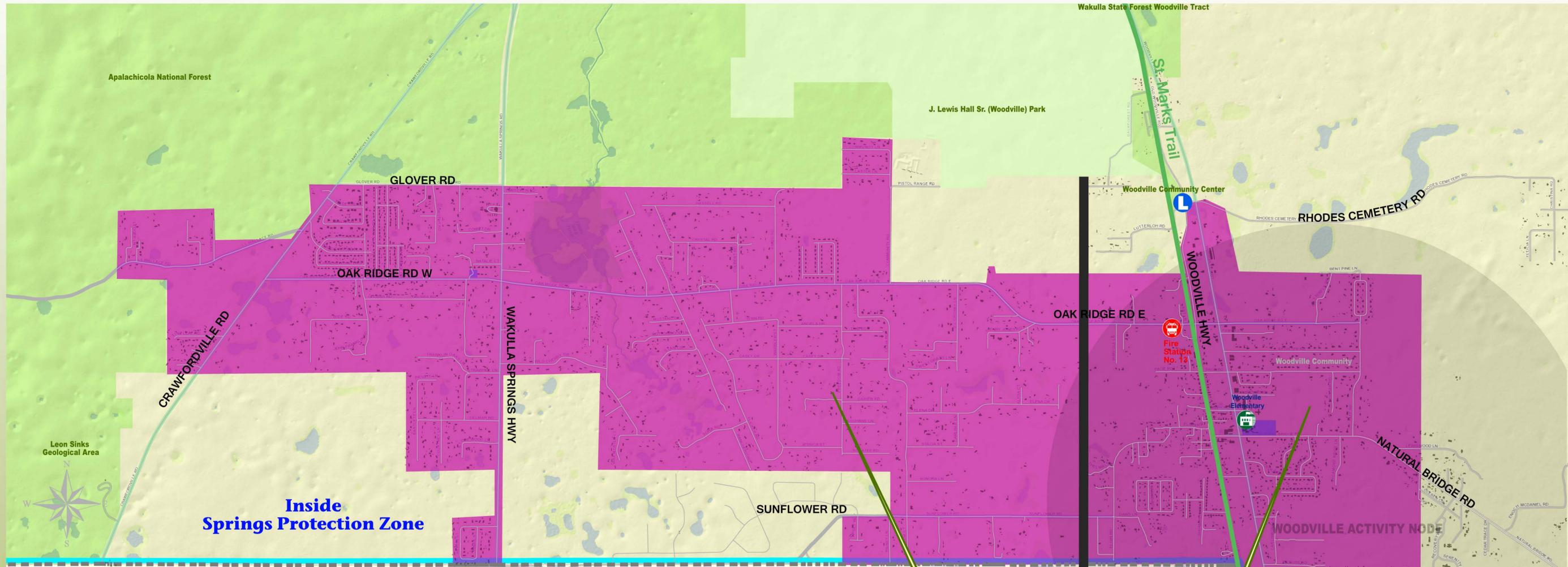
Recommendation:

Option #1.

Attachments:

1. Interlocal Agreement
2. Sewer Master Plan Historical Background
3. Woodville Rural Community and Woodville Urban Fringe Map

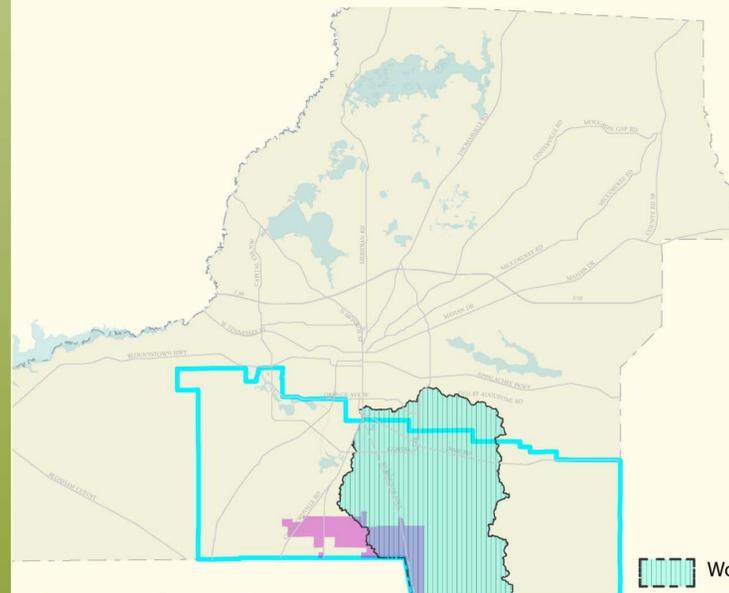
VSL/TP/KB/djw



Woodville Water Quality

Urban Fringe
Future Land Use

Woodville Rural
Community
Future Land Use



General Location

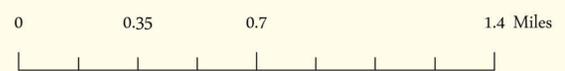
Woodville Recharge Basin

Legend

- Woodville Sewer
- St. Marks Trail
- Springs Protection
- Existing Sidewalks, Bike Routes
- Park Trails
- Parks
- Waterbodies

Woodville Water Quality Project Criteria:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input checked="" type="checkbox"/> Water Quality Enhancements and Green Infrastructure |
| <input checked="" type="checkbox"/> Regional Mobility | <input checked="" type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input type="checkbox"/> Greenway Master Plan | <input type="checkbox"/> Transportation Capacity Improvements |
| <input type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input type="checkbox"/> Complements BP2000 Project | |



This product has been compiled from the most accurate source data from Leon County and the City of Tallahassee. However, this product is for reference purposes only and is not to be construed as a legal document or survey instrument. Any reliance on the information contained herein is at the user's own risk. Leon County and the City of Tallahassee assume no responsibility for any use of the information contained herein or any loss resulting therefrom.

SALES TAX PROJECTS

Project Number: 41
Project Name: Oak Ridge Sewer Project
Total Project Cost: \$30.6 million

Total Project Cost Notes

This project cost is only for the sanitary sewer mainline construction only. Funding regarding landowner assistance for sewer hookups is discussed in Project #45.

Executive Project Summary

The Oak Ridge Sewer Project addresses the Lake Munson sewer target area identified in the City of Tallahassee 2030 Master Sewer Plan. The 3,173-acre Lake Munson sewer target area lies within the Wakulla Springs Primary Springs Protection Zone. The project centralizes sewer and reduces the possibility of high nitrogen and fecal coliform discharges to the natural systems, which can occur when septic tanks do not function properly, thereby improving the water quality of the area as well as the water quality of Wakulla Springs.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

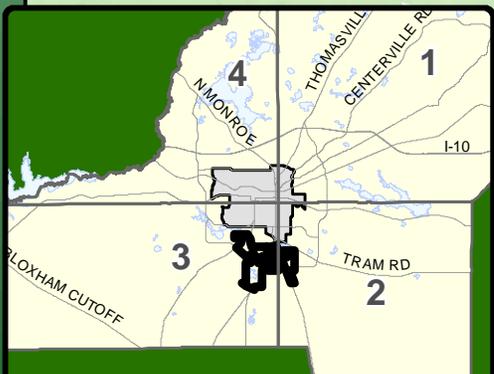
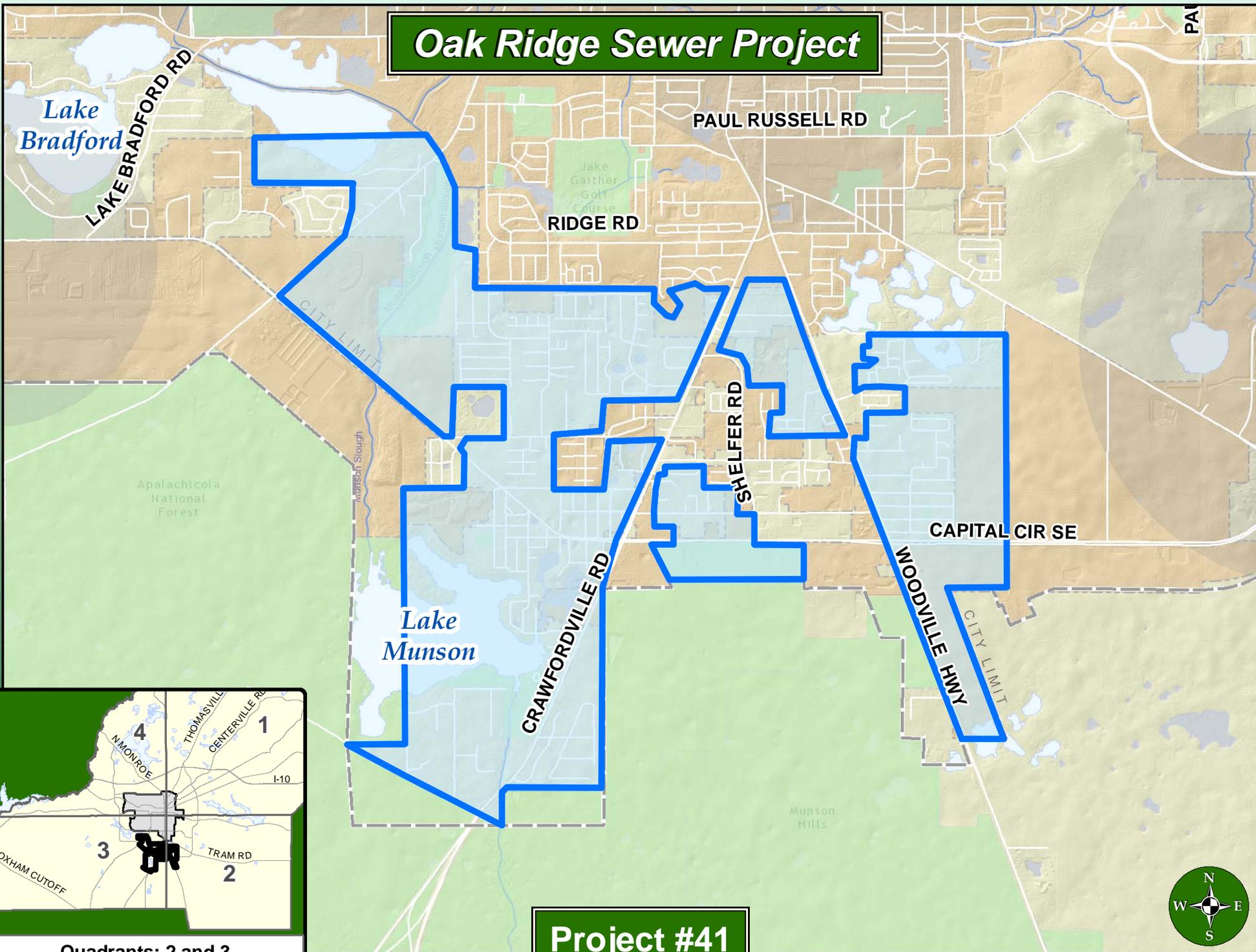
Detail Project Description

The project would allow provide access to sewer for over 2,500 landowners currently operating septic tanks in the Lake Munson sewer target area. The County and City may choose to develop an incentive program and financing option to address the individual landowner connection costs and allow for sufficient connections to ensure that maximum benefits are achieved from the public capital investment in the sewer system. This incentive program is described in Project 45.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
\$30.6 million	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Oak Ridge Sewer Project



Quadrants: 2 and 3

Project #41



Previous Project Information for:

Project #41

Oak Ridge Sewer Project

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Oak Ridge Sewer Project (*Previously Project #57*)

Proposed Sales Tax Projects by the Community

Project Name: Oak Ridge Sewer Project (Project #57)

Project Themes:

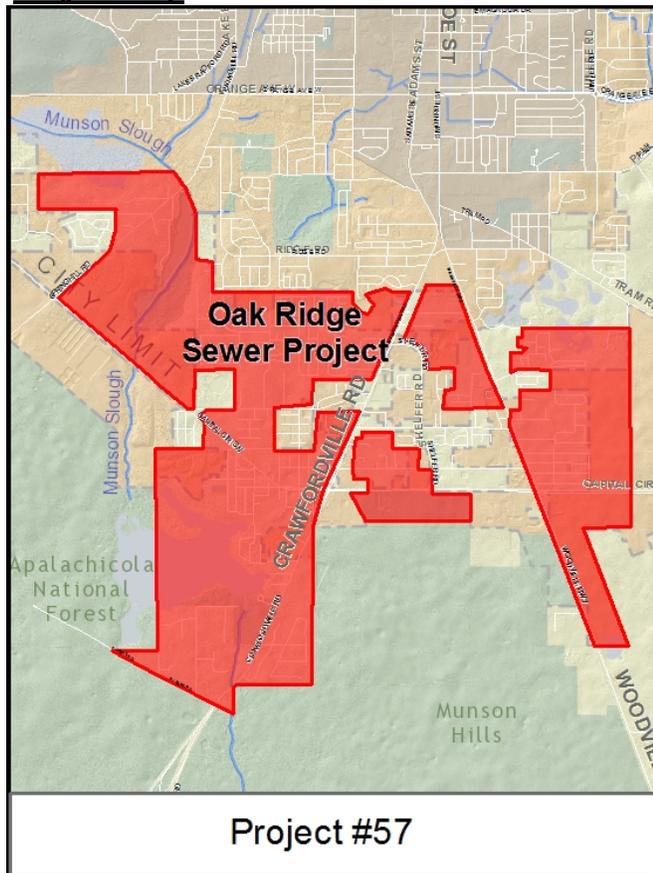
- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

The Oak Ridge Sewer Project addresses the Lake Munson sewer target area identified in the City of Tallahassee 2030 Master Sewer Plan. The 3,173-acre Lake Munson sewer target area lies within the Wakulla Springs Primary Springs Protection Zone. The project centralizes sewer and reduces the possibility of high nitrogen and fecal coliform discharges to the natural systems, which can occur when septic tanks do not function properly, thereby improving the water quality of the area as well as the water quality of Wakulla Springs.

The capital cost for engineering and construction to provide landowners access to sewer is estimated at \$30.6 million. The individual landowner connection expenses include system charges and the construction cost to abandon the existing septic system and provide plumbing from the street to the house. The County and City may choose to develop an incentive program and financing option to address the individual landowner connection costs and allow for sufficient connections to ensure that maximum benefits are achieved from the public capital investment in the sewer system.

Project Map:



SALES TAX PROJECTS

Project Number: 42
Project Name: Centerville Trace Water Resources
Total Project Cost: \$5 million

Total Project Cost Notes

This project cost is only for the sanitary sewer mainline construction only. Funding regarding landowner assistance for sewer hookups is discussed in Project #45.

Executive Project Summary

The Centerville Trace Water Resources project addresses a sewer target area identified in the 2030 Master Sewer Plan. The Centerville Trace subdivision is also a sewer target area identified in the Water and Sewer Agreement between the City and County. The project centralizes sewer and reduces the possibility of high nitrogen and fecal coliform discharges to the natural systems that can occur when septic tanks do not function properly, thereby improving the water quality of the area as well as the water quality of Dove Pond.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

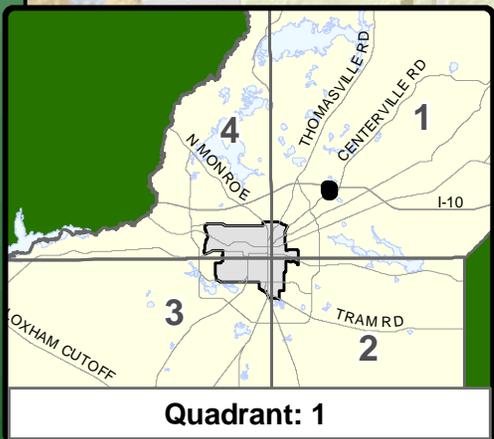
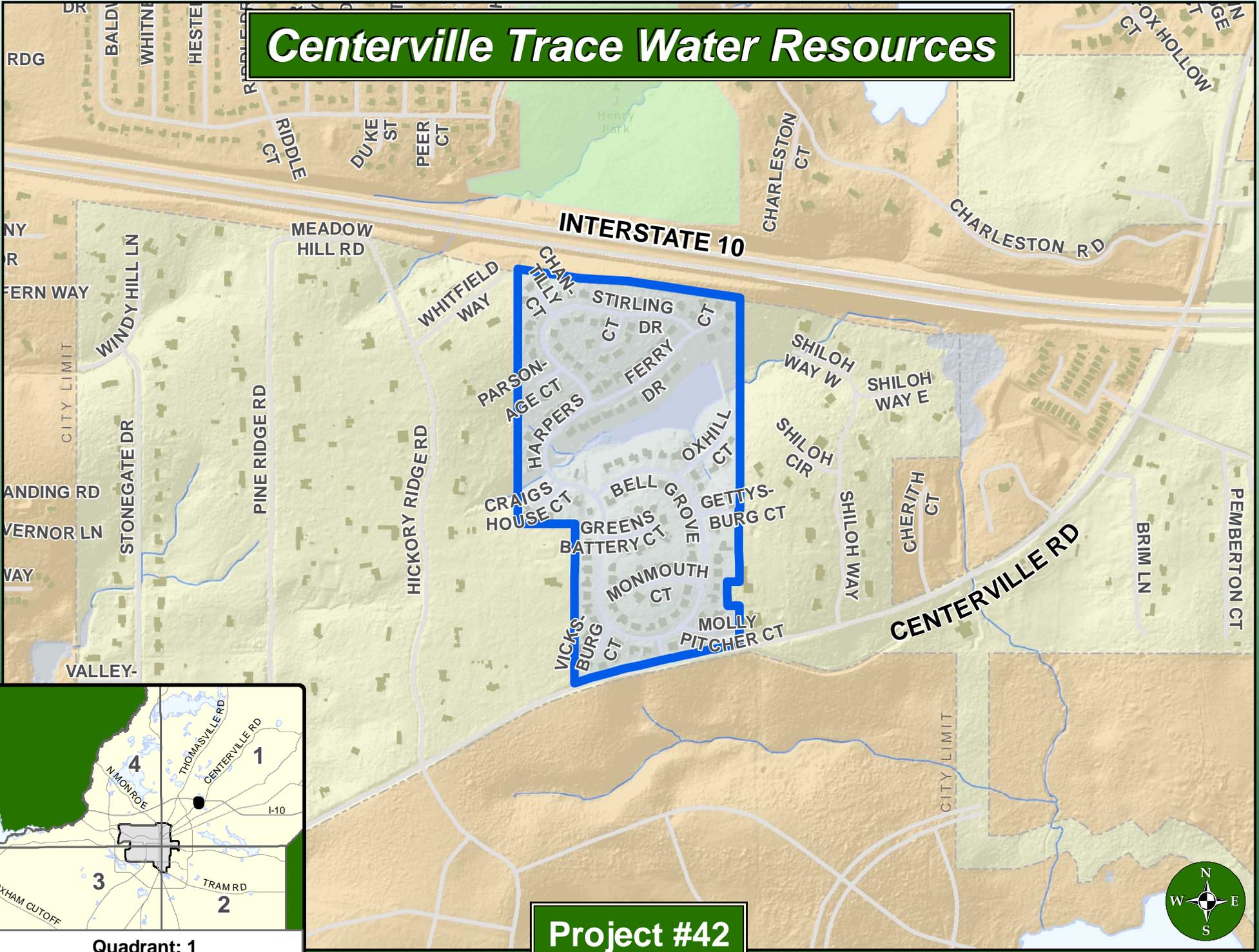
The 167-lot subdivision is located in northeast Tallahassee, with less than the 1/2-acre lot size required by current state standards. This project addresses a sewer target area identified in the Water and Sewer Agreement between the City and County. Sanitary sewer mainline construction will allow access to central sewer to reduce the possibility of high nitrogen and fecal coliform discharges to the natural systems that can occur when septic tanks do not function properly, thereby improving the water quality of the area as well as the water quality of Dove Pond.

The City and County may choose to develop an incentive program and financing option to address the financial burden and allow for sufficient connections to ensure that maximum benefits are achieved from the public capital investment in the sewer system. This incentive program is described in Project #45.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
\$5 million	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Centerville Trace Water Resources



Quadrant: 1

Project #42



Previous Project Information for:

Project #42

Centerville Trace Water Resources

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Centerville Trace Water Resources (*Previously Project #47*)

Proposed Projects for the Sales Tax Extension

Project #9: Centerville Trace Water Resources

Project Cost: \$5 million

Project Criteria:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input checked="" type="checkbox"/> Water Quality Enhancements and Green Infrastructure |
| <input type="checkbox"/> Regional Mobility | <input checked="" type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input type="checkbox"/> Greenway Master Plan | <input type="checkbox"/> Transportation Capacity Improvements |
| <input type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input type="checkbox"/> Complements BP2000 Project | |

Project Description:

The Centerville Trace Water Resources project addresses a sewer target area identified in the 2030 Master Sewer Plan. The Centerville Trace subdivision is also a sewer target area identified in the Water and Sewer Agreement between the City and County. The 167-lot subdivision is located in northeast Tallahassee, with less than the 1/2-acre lot size required by current state standards. The project centralizes sewer and reduces the possibility of high nitrogen and fecal coliform discharges to the natural systems that can occur when septic tanks do not function properly, thereby improving the water quality of the area as well as the water quality of Dove Pond. (Note: This estimate does not include the individual landowner sewer connection costs. The City and County may choose to develop an incentive program and financing option to address the financial burden and allow for sufficient connections to ensure that maximum benefits are achieved from the public capital investment in the sewer system.)

Project Criteria Detail:

Comprehensive Plan: This project supports with the both the Utility and Conservation elements of the Comprehensive Plan. The areas that the project complies with are listed below.

- Utilities Element- Sanitary Sewer Goal 1: Provide efficient wastewater treatment that meets the demands of the community while maintaining public health and environmental standards.
- Conservation Element Goal1: Preserve, protect and conserve the ecological value and diversity of natural resources in Tallahassee and Leon County. (Reduction of nitrogen protects the natural systems)

Regional Mobility Plan: N/A

Greenway Master Plan: N/A

Connectivity: N/A

Complements Blueprint 2000 Project(s): N/A

Water Quality Enhancements and Green Infrastructure: This project provides access to Advanced Wastewater Treatment sewer system and reduces the possibility of high nitrogen and fecal coliform discharges to natural systems that can occur when septic systems do not function properly. This improvement protects the water quality of Dove Pond.

Stormwater/Sewer Capacity Improvements: This project addresses a sewer target area identified in the Water and Sewer Agreement between the City and County. The table below provides basic information on the size and cost for the project and separates the capital cost from the landowner connection cost.

Project #9: Centerville Trace Sanitary Sewer Project

Page 2

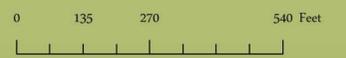
The capital cost listed below includes engineering and construction to provide landowners with access to sewer. The connection cost includes the individual landowner expenses to abandon a septic system, provide plumbing from the street to the house, and pay system charges. These individual landowner expenses have not been included in the project cost.

Project Area	Acres	Existing Septic Systems	Capital Costs for Sewer	Landowner Connection Costs (\$12,000/system)	Capital + Connection Costs
Centerville Trace	64	167	\$5.0 million	\$2.0 million	\$7.0 million

The County and City may choose to develop an incentive program and financing option to address the financial burden of individual landowner connection costs and allow for sufficient connections to ensure that maximum benefits are achieved from the public capital investment in the sewer system. Individual landowners will also be required to pay any monthly bills from the sewer provider (approximately \$65/month).

Transportation Capacity Improvements: N/A

Core Infrastructure: The Centerville Trace Sanitary Sewer project is considered a core infrastructure project due to the stormwater/sewer capacity improvements and water quality enhancements as detailed above.



Centerville Trace Water Resources Project



- Legend**
- Centerville Trace Sewer Project
 - Parks

- Centerville Trace Sanitary Sewer Project Criteria:**
- Comprehensive Plan
 - Regional Mobility
 - Greenway Master Plan
 - Connectivity
 - Complements BP2000 Project
 - Water Quality Enhancements and Green Infrastructure
 - Stormwater/Sewer Capacity Improvements
 - Transportation Capacity Improvements
 - Core Infrastructure



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SALES TAX PROJECTS

Project Number: 43A

Project Name: Alternatives to Central Sewer in Harbinwood Estates: Decentralized Cluster System

Total Project Cost: \$14.1 million

Total Project Cost Notes

Estimated costs include property acquisition and piping, pumps etc for a cluster septic system with appropriately designed drainfields

Executive Project Summary

This project is for the construction of a decentralized cluster system of new drainfields within the Harbinwood Community to enhance the treatment of the domestic waste.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

Harbinwood is a 400 lot subdivision consisting of less than 1/2 acres lots. Construction of a decentralized cluster system meeting the appropriate treatment standards is estimated to require the acquisition of approximately 35 lots (homes) to provide sufficient land area for the enhanced drainfields with a reasonable loading rate for the clay soils. Mounded systems are likely given the high water table. Retrofitting to route clusters of homes to a common drainfield would require effluent pumps, piping etc.

Provision of functioning drainfields would improve water quality of Lake Jackson with the reduction of fecal coliform and nitrogen loading to the surface water of the lake. This alternative does not provide AWT treatment as there are currently no State approved septic systems that are certified to meet AWT standards.

The estimated cost of this cluster system project for property acquisition, design, permitting and construction is \$14.1 million.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
\$14.1 million	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

SALES TAX PROJECTS

Project Number: 43B
Project Name: Alternatives to Central Sewer in Harbinwood Estates: Advanced Wastewater Treatment Plant
Total Project Cost: \$29.3 million

Total Project Cost Notes

The project cost includes land acquisition design permitting construction of a centralized sewer collection system, Advanced Wastewater Treatment Plant (AWT) plant with a irrigation effluent disposal.

Executive Project Summary

This project would remove approx. 400 substandard septic tanks with a neighborhood centralized gravity collection system routed to a neighborhood AWT plant capable of meeting all current AWT standards including treating the domestic effluent for nitrogen to 3mg/l.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

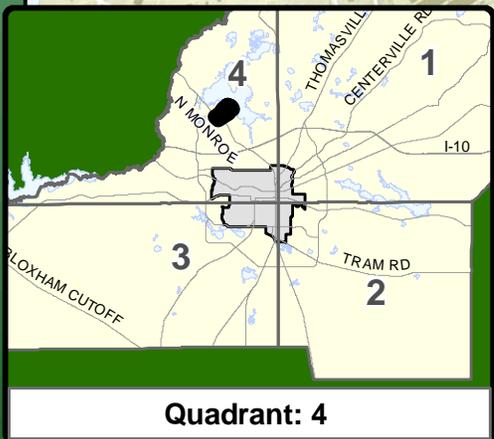
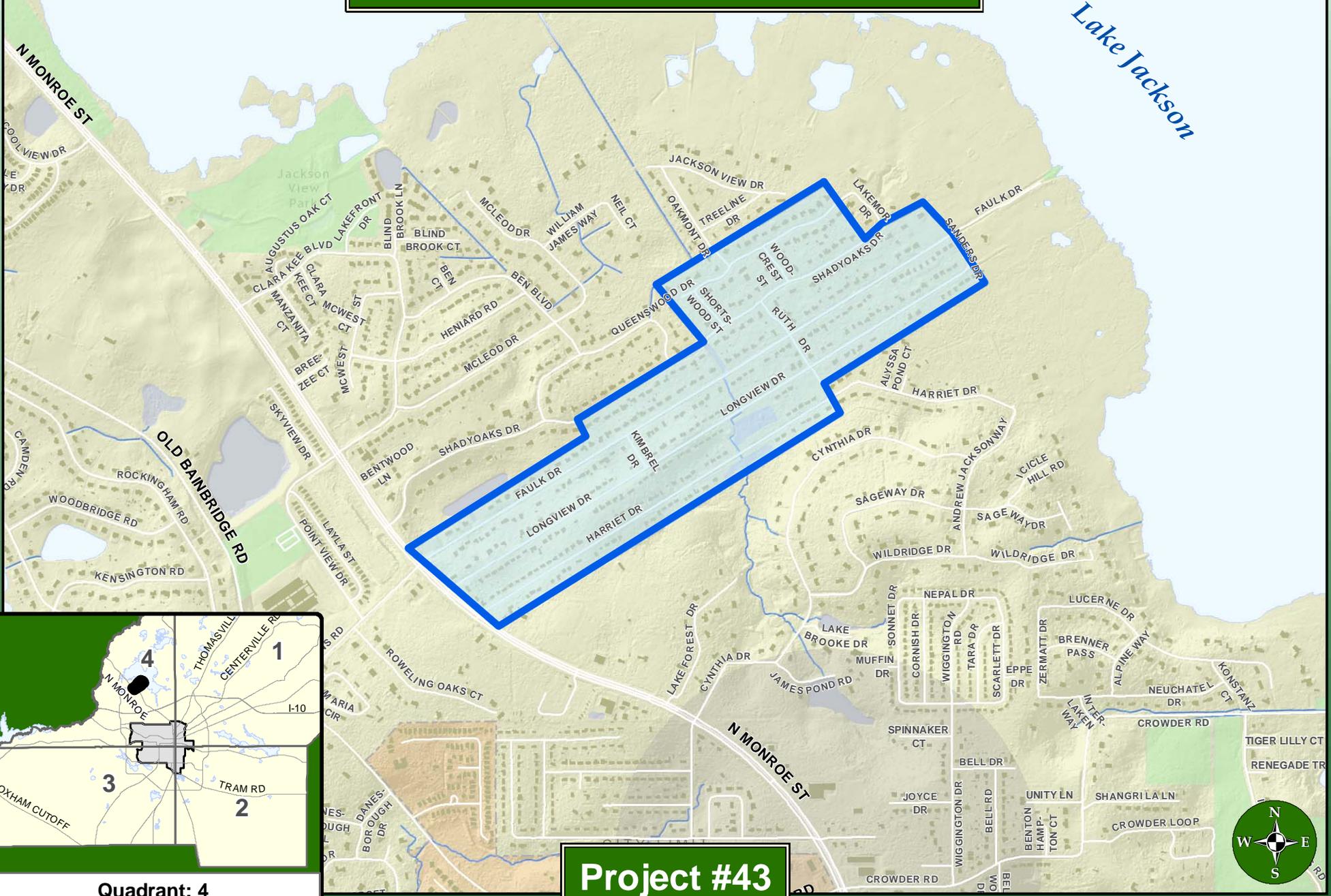
Project consists on land acquisition for both the drainfield and treatment plant which is estimated at 37 lots. Construction of a centralized collection system, a advanced wastewater treatment plant with odor control and capable of producing effluent with a nitrogen concentration of 3mg/l or less, a drip irrigation system disposal system.

Project would remove either by treatment or physical removal 400 non functioning septic systems and improve the water quality of Lake Jackson with the decrease in nitrogen loading.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
\$29.3 million	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Alternatives to Central Sewer in Harbinwood Estates



Quadrant: 4

Project #43



Previous Project Information for:

Project #43
Alternatives to Central Sewer in Harbinwood
Estates:
A - Decentralized Cluster System
B - Advanced Wastewater Treatment Plan

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Harbinwood Estates: Alternatives to Central Sewer (*Previously Project #44*)

Proposed Sales Tax Projects by the Community

Project Name: Harbinwood Estates: Alternatives to Central Sewer (Project #44)

Project Themes:

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

This project involves developing alternative methods of domestic wastewater treatment and disposal for the built-out neighborhood of Harbinwood Estates. Implementation of cluster disposal systems or package treatment and disposal facilities requires modification to the Tallahassee/Leon County Comprehensive Plan as directed by the Board of County Commissioners.

Harbinwood is located in close proximity to the City of Tallahassee's sewer collection system. This neighborhood has sufficient density for consideration to be connected to the city's system and is included in the City Master Sewer Plan. While, developing this project, three other alternatives were considered: sewer cluster systems, package plants, and connection to Talquin Tower Road sewer plant. However, these alternatives are not recommended by staff for several reasons which are discussed below.

Cluster system: Retrofitting an existing neighborhood for a cluster system would create significant adverse neighborhood impacts. Due to low soils with low percolation rates in this neighborhood, it is estimated that approximately 35 lots would need to be acquired to provide sufficient drainfield area for improved treatment. This would displace a large number of residents and the projected cost is higher than connection to the city's system.

Package Plant: Constructing a sewer package plant in Harbinwood proposes similar issues to the cluster system. However, this alternative would require more land to construct the plant. In addition, the sewer package plant could not be located near Lake Jackson.

Connection to the Talquin Tower Road Plan: Constructing a large force main several miles long would be necessary to connect to the existing Talquin plant on Tower Road. The capacity at this plant would need to be greatly increased in order to service Harbinwood. In addition, Talquin's sprayfield at this plant would need to be expanded in an adjacent vacant subdivision which would need to be acquired.

Project Map:



SALES TAX PROJECTS

Project Number: 44

Project Name: Lake Jackson Preservation and Mobility Enhancements

Total Project Cost: \$30.4 million

Total Project Cost Notes

The project cost includes acquisition of eastern shore property with trails and habitat restoration. Also, this project cost reflects the sanitary sewer mainline construction only. Funding regarding landowner assistance for sewer hookups is discussed in Project #45.

Executive Project Summary

The Lake Jackson Enhancement and Preservation project seeks to reduce nutrient loading, improve lake water quality, and preserve a treasured natural amenity in the unincorporated county designated as an 'Outstanding Florida Waterbody' while implementing a portion of the Lake Jackson Greenways. This project provides additional bike and pedestrian interconnections and links to existing trails to County and State parks, including Lake Jackson. The project restores the eastern shore ecosystem of the lake through the acquisition of 275 acres of vacant property. Finally, this project provides sanitary sewer access within the Harbinwood subdivision to address nearly 400 septic system discharges.

Themes

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input checked="" type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

A critical component of the project is constructing central sanitary sewer to the Harbinwood subdivision. The Harbinwood subdivision is a sewer target area identified in the Water and Sewer Agreement between the City and County. This area has been identified as a major contributor to high nutrient concentrations in Lake Jackson and is part of an area identified as a target sewer project. The County and City may choose to develop a landowner connection incentive program and financing option to address the financial burden and allow for sufficient connections to ensure that maximum benefits are achieved from the public capital investment in the sewer system.

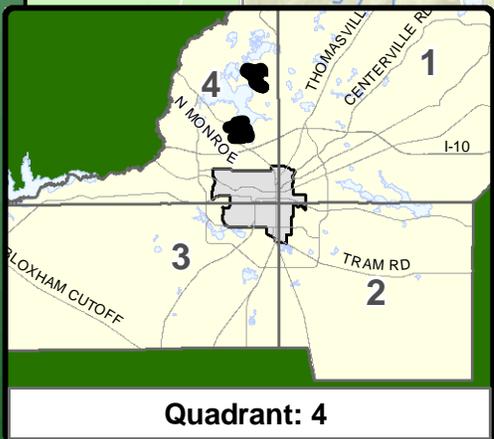
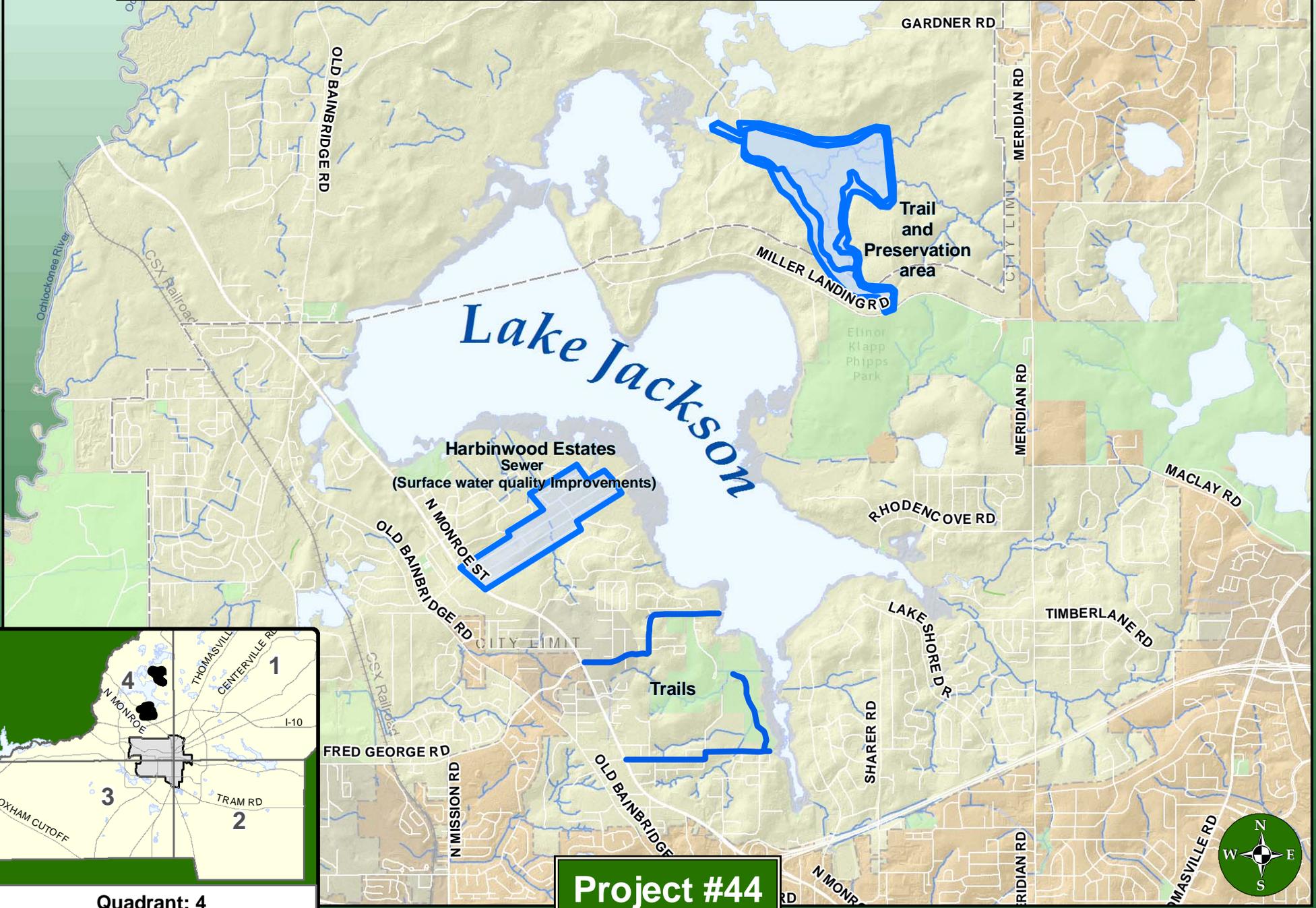
The project originally identified eastern shore property for acquisition and restoration; however, the landowner has subsequently indicated the property is not available for purchase. Approximately \$15.9 million of the total project cost is associated with this component.

The balance of the project involves improvement of the Crowder Road and US 27 North intersection, and construction of sidewalks along Crowder Road and Fuller Road to provide neighborhood access to the regional amenities. The sidewalks along Crowder Road and Talpeco/Fuller Roads connect neighborhoods to the Lake Jackson Mounds State Park and the County's Okeehoopkee Prairie Park.

Cost By Themes

\$1 million	Regional Mobility/Transportation
	Sense of Community
\$12.1 million	Environmental/Water Quality
\$1.4 million	Connectivity
	Economic Vitality
\$15.9 million	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Lake Jackson Preservation and Mobility Enhancements



Quadrant: 4

Project #44



Previous Project Information for:

Project #44

Lake Jackson Preservation and Mobility Enhancements

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Lake Jackson Preservation and Mobility Enhancements
(Previously Project #51)

Proposed Projects for the Sales Tax Extension

Project #2: Lake Jackson Preservation and Mobility Enhancements

Estimated Project Cost: \$30 million

Project Criteria:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input checked="" type="checkbox"/> Green Infrastructure & Water Quality Enhancements |
| <input checked="" type="checkbox"/> Regional Mobility | <input checked="" type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input checked="" type="checkbox"/> Greenway Master Plan | <input type="checkbox"/> Transportation Capacity Improvements |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input type="checkbox"/> Complements BP2000 Project | |

Project Description:

The Lake Jackson Enhancement and Preservation project seeks to reduce nutrient loading, improve lake water quality, and preserve a treasured natural amenity in the unincorporated county designated as an ‘Outstanding Florida Waterbody’ while implementing a portion of the Lake Jackson Greenways. This project provides additional bike and pedestrian interconnections and links to existing trails to County and State parks, including Lake Jackson. The project restores the eastern shore ecosystem of the lake through the acquisition of 275 acres of vacant property. Finally, this project provides sanitary sewer access within the Harbinwood subdivision to address nearly 400 septic system discharges. The Harbinwood subdivision is a sewer target area identified in the Water and Sewer Agreement between the City and County. This area has been identified as a major contributor to high nutrient concentrations in Lake Jackson and is part of an area identified as a target sewer project. (Note: This estimate does not include the individual landowner sewer connection costs. The County and City may choose to develop an incentive program and financing option to address the financial burden and allow for sufficient connections to ensure that maximum benefits are achieved from the public capital investment in the sewer system.)

The project proposed improvements include the following:

- Provides sanitary sewer access within the Harbinwood subdivision to address nearly 400 septic system discharges to reduce nutrient loading;
- Improves lake water quality by preserving the ecosystem and allowing restoration of disturbed ravines on the eastern shore in order to enhance water quality treatment;
- Construction of almost one-half mile of trails connecting Okeeheepkee Prairie Park and Lake Jackson Mounds Archaeological State Park;
- Construction of over two miles of sidewalks along Talpeco, Fuller, and Crowder Road;
- Promotes recreational activity by adding 275 acres on the eastern shore of Lake Jackson.

Project Criteria Detail:

Comprehensive Plan: This project supports the Mobility, Conservation, Parks and Recreation, and Stormwater Management elements of the Comprehensive Plan. The areas that the project complies with are listed below.

- ***Mobility Element*** Policy 1.2.3: Establish and maintain a safe and effective system of bicycle lanes, sidewalks, and shared-use paths in conjunction with existing and planned roadways and the Greenways Master Plan. Where design criteria allow and safe operation will occur, separate bicycle and pedestrian traffic from vehicular traffic. Provide adequate and secure bicycle parking facilities at major destinations.
- ***Conservation Element:***
 - Goal1: Preserve, protect and conserve the ecological value and diversity of natural resources in Tallahassee and Leon County.
 - Policy 2.3.3: Require connection to central sewer systems whenever sewer is available or becomes available especially in the Lake Jackson Special Development Zone.

Project #2: Lake Jackson Enhancement and Preservation

Page 2

- Goal 3: Protect, enhance, and restore natural resources, wildlife habitat, and natural vegetative communities to maintain a diversity of native flora and fauna to assure the maintenance of a viable population of native species.
- Utilities Sanitary Sewer Goal 1: Provide efficient wastewater treatment that meets the demands of the community while maintaining public health and environmental standards.
- *Parks and Recreation Element:*
 - Goal 1: Provide, manage and fund adequate recreational and cultural facilities to encourage a physically and mentally healthy lifestyle for all citizens of Tallahassee-Leon County
 - Policy 1.5.3: The County shall help fund and develop a Greenways Trail System that provides the public opportunities to access a safe and convenient trail system in the unincorporated County. This system should incorporate public lands, right of way, easements on private lands, and open space designated lands to achieve this goal. The trail system will connect to the maximum extent possible existing and future residential areas to employment, education, and activity centers.

Regional Mobility Plan: This project provides additional bike and pedestrian interconnections and linkages to existing trails as well as Lake Jackson.

Greenway Master Plan: This project addresses components of the Lake Jackson North and Lake Jackson South Greenways.

Connectivity: This project will provide connectivity through the acquisition of the above property by creating recreational connectivity for the north arm of Lake Jackson by linking to the Northwest Florida Water Management District property (Elinor Klapp-Phipps Park). The sidewalks along Crowder Road and Talpeco/Fuller Roads connect neighborhoods to the Lake Jackson Mounds State Park and the County's Okeehoopkee Prairie Park.

Complements Blueprint 2000 Project(s): N/A

Green Infrastructure and Water Quality Enhancements: This project will reduce nutrient loading, improve lake water quality, and preserve the surrounding ecosystem on the eastern shore. The project includes acquisition of approximately 275 acres of vacant property on the eastern shore that will enhance water quality treatment and restore the disturbed ravine system. Ecosystem restoration will include naturalized treatment systems and other low-intensity development principles to reduce pollutant loads from historic development east of Meridian Road.

Stormwater/Sewer Capacity Improvements: This project consists of providing sanitary sewer access within the Harbinwood subdivision to address nearly 400 septic system discharges to reduce nutrient loading and improve lake and stream water quality. This higher density subdivision on the western shore of the lake was developed with septic systems on less than the state standard of 1/2-acre lot size. The area has been identified as a major contributor to high nutrient concentrations in Lake Jackson and is part of an area identified as a target sewer project.

The capital cost listed below includes engineering and construction to provide landowners with access to sewer. The connection cost includes the individual landowner expenses to abandon a septic system, provide plumbing from the street to the house, and pay system charges. These individual landowner expenses have not been included in the project cost.

Project Area	Acres	Existing Septic Systems	Capital Costs for Sewer	Landowner Connection Costs (\$12,000/system)	Capital + Connection Costs
Harbinwood	151	400	\$12.1 million	\$4.8 million	\$16.9 million

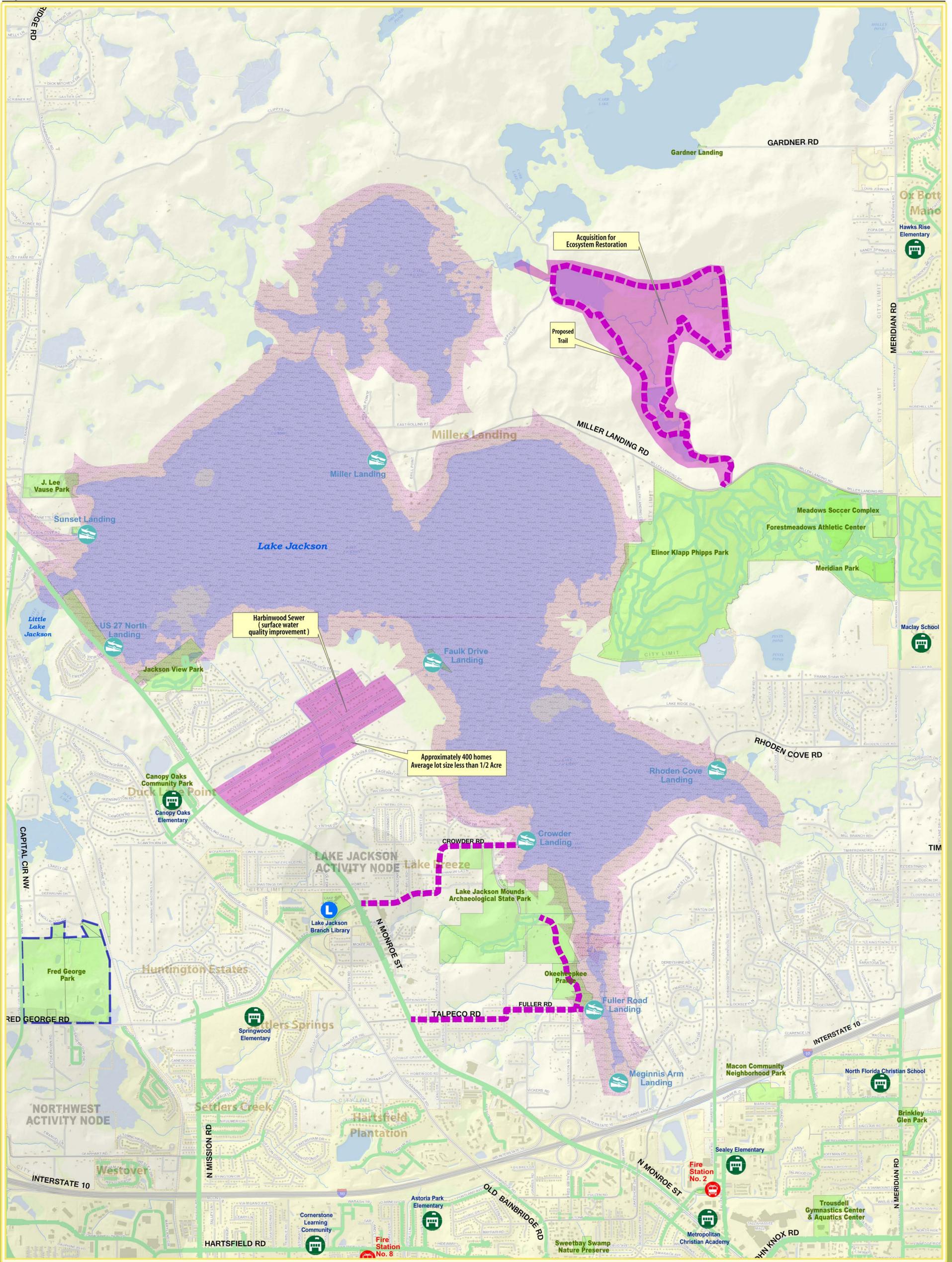
Project #2: Lake Jackson Enhancement and Preservation

Page 3

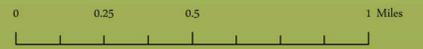
The County and City may choose to develop an incentive program and financing option to address the financial burden of individual landowner connection costs and allow for sufficient connections to ensure that maximum benefits are achieved from the public capital investment in the sewer system. Individual landowners will also be required to pay any monthly bills from the sewer provider (approximately \$65/month).

Transportation Capacity Improvements: N/A

Core Infrastructure: This project is considered a core infrastructure project due to the stormwater/sewer capacity improvements, green infrastructure, and water quality enhancements as detailed above.



Lake Jackson Preservation and Mobility Enhancements



Legend

- Existing Sidewalks, Bike Routes
- Park Trails
- Proposed Sidewalk/Trail Improvements
- Water Quality Benefits Area
- Lake Jackson Ecosystem Enhancements & Preservation
- Blueprint 2000 Projects
- Parks

Lake Jackson Preservation and Mobility Enhancements Project Criteria:

- Comprehensive Plan
- Regional Mobility
- Greenway Master Plan
- Connectivity
- Complements BP2000 Project
- Water Quality Enhancements & Green Infrastructure
- Stormwater/Sewer Capacity Improvements
- Transportation Capacity Improvements
- Core Infrastructure



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SALES TAX PROJECTS

Project Number: 45
Project Name: Sewer Hookups Incentive Program
Total Project Cost: \$33.1 million

Total Project Cost Notes

The attached table estimates that this program could cost up to \$33.1 million. The Committee may choose to allocate only a certain amount of funds for the sewer hookup incentive program.

Executive Project Summary

This project is for providing incentives to landowners for sewer hookups. The Committee may choose to develop an incentive program and financing option to address the financial burden and allow for sufficient connections to ensure that maximum benefits are achieved from the public capital investment in the sewer system.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

The attached table outlines possible scenarios for funding an incentive project for sewer connections.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
\$33.1 million	Environmental/Water Quality
	Connectivity
	Economic Vitality
	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Sales Tax Septic Tank Abatement Discussion

November 29, 2012

Project Area	Septic Tanks		(A)	(B)	(C)	(D)**	(E)
	With Sewer Available	Without Sewer Available	Capital Costs for Sewer (million dollars)	Connection Costs at \$12,000 per Connection (million dollars)	(A+B) Total System + Connection Cost (million dollars)	Connection Costs at \$7,000 per Connection (million dollars)	(A+D) Total System + Connection Cost (million dollars)
Centerville Trace ^a	0	167	\$5.0	\$2.0	\$7.0	\$1.2	\$6.2
Harbinwood ^a	0	400	\$12.1	\$4.8	\$16.9	\$2.8	\$14.9
Woodville ^a	0	1274	\$24.5	\$15.3	\$39.8	\$8.9	\$33.4
Woodville (Urban Fringe) ^a	<u>0</u>	<u>1,671</u>	<u>\$33.4</u>	<u>\$20.1</u>	<u>\$53.5</u>	<u>\$11.7</u>	<u>\$45.1</u>
Co. subtotal		3,512	\$75.0	\$42.2	\$117.2	\$24.6	\$99.6
City Septic Tanks ^b	<u>1044</u>	<u>174</u>	<u>\$5.2</u>	<u>\$14.6</u>	<u>\$19.8</u>	<u>\$8.5</u>	<u>\$13.7</u>
Total =	1044	3,686	\$80.2	\$56.8	\$137.0	\$33.1	\$113.3

a: From Leon County Sales Tax Workshop Agenda Materials

b: From City Underground Utilities to serve 174 OSTDs

Proposal for Including an Incentive to Connect:

- Use sales tax revenue to provide grant for connecting within 12 months of availability.
 - Would be available to all County residents, including those within the City.
 - City and County should consider jointly adopting a policy of enforcing state law requiring connection within 12 months.
- ** Reduced connection charges based on economies of scale and cost averaging for a large number of connections.**

Previous Project Information for:

Project #45

Sewer Hookups Incentive Program

Note: Based on direction from the Committee at the March 28, 2013 Sales Tax Committee meeting, below is a list of previous project(s) that were merged or modified to create a holistic project for the Committee's consideration.

Project Name: Sales Tax Septic Tank Abatement Discussion (*Previously Project #39*)

Sales Tax Septic Tank Abatement Discussion
November 29, 2012

Project Area	Septic Tanks		(A)	(B)	(C)	(D)**	(E)
	With Sewer Available	Without Sewer Available	Capital Costs for Sewer (million dollars)	Connection Costs at \$12,000 per Connection (million dollars)	(A+B) Total System + Connection Cost (million dollars)	Connection Costs at \$7,000 per Connection (million dollars)	(A+D) Total System + Connection Cost (million dollars)
Centerville Trace ^a	0	167	\$5.0	\$2.0	\$7.0	\$1.2	\$6.2
Harbinwood ^a	0	400	\$12.1	\$4.8	\$16.9	\$2.8	\$14.9
Woodville ^a	0	1274	\$24.5	\$15.3	\$39.8	\$8.9	\$33.4
Woodville (Urban Fringe) ^a	<u>0</u>	<u>1,671</u>	<u>\$33.4</u>	<u>\$20.1</u>	<u>\$53.5</u>	<u>\$11.7</u>	<u>\$45.1</u>
Co. subtotal		3,512	\$75.0	\$42.2	\$117.2	\$24.6	\$99.6
City Septic Tanks ^b	<u>1044</u>	<u>174</u>	<u>\$5.2</u>	<u>\$14.6</u>	<u>\$19.8</u>	<u>\$8.5</u>	<u>\$13.7</u>
Total =	1044	3,686	\$80.2	\$56.8	\$137.0	\$33.1	\$113.3

a: From Leon County Sales Tax Workshop Agenda Materials

b: From City Underground Utilities to serve 174 OSTDs

Proposal for Including an Incentive to Connect:

- Use sales tax revenue to provide grant for connecting within 12 months of availability.
 - Would be available to all County residents, including those within the City.
 - City and County should consider jointly adopting a policy of enforcing state law requiring connection within 12 months.
- ** Reduced connection charges based on economies of scale and cost averaging for a large number of connections.**

SALES TAX PROJECTS

Project Number: 46A

Project Name: Operating Costs for Parks Built with Blueprint 2000 Funds: County Portion

Total Project Cost: \$16,500,000

Total Project Cost Notes

Total project cost reflects the current annual operation and maintenance cost of both the County projects plus any one-time start up amounts over a 15 year period. The annual operating cost for the County portion of these parks is \$1.1 million.

Executive Project Summary

As a result of the holistic infrastructure planning and sensitive land purchase program, Blueprint 2000 has purchased and participated in partnerships to acquire properties that are intended for park space in Leon County and the City of Tallahassee. These park spaces, and their associated operation/maintenance obligations, are turned over to the City and County at project completion. This project seeks funding for the operation of Parks built with Blueprint 2000 funds. This project description details the operating costs for the parks maintained by Leon County.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

Florida Statutes 212.055 (2) allows for a portion of the sales tax proceeds to be used for the operation and maintenance of parks and recreation programs or facilities which have been constructed with the proceeds of the sales tax throughout the duration of the sales tax levy. The statute states:

“a county having a population greater than 75,000 in which the taxable value of real property is less than 60 percent of the just value of real property for ad valorem tax purposes for the tax year in which an infrastructure surtax referendum is placed before the voters, and the municipalities within such a county, may use the proceeds and interest of the surtax for operation and maintenance of parks and recreation programs and facilities established with the proceeds of the surtax throughout the duration of the surtax levy or while interest earnings accruing from the proceeds of the surtax are available for such use, whichever period is longer.”

For the most recent year, the County's just value was \$23.54 billion and the taxable value was \$13.38 billion or 56.87%. Therefore, the County qualifies for sales tax proceeds to be used for the operation and maintenance of parks and recreation programs and facilities that were constructed with sales tax funds.

In Leon County, there are seven facilities open to the public which utilized sales tax funds for all or a portion of the costs to purchase or enhance the property including, Apalachee Regional Park, Jackson View, Lake Henrietta, Martha Wellman Park, Miccosukee Community Center, Woodville Community Center, and Woodville Community Park. Together, these facilities have an annual operating cost of \$365,400 for only the sales tax funded portion. There are seven facilities that are not yet open to the public but were purchased or will utilize sales tax funds. These parks include Broadmoore Pond, Fred George Greenway, NE Community Park, Nusbickel Property, Okeeheepkee Prairie Park, Park Place PUD and the St. Marks Greenway. Together, these facilities have an estimated annual operating cost of \$700,600. The combined current and future annual operating cost of the park facilities in Leon County is estimated to be 1.1 million dollars.

The City of Tallahassee will maintain and operate Cascades Park and Debbie Lightsey Nature Trail. The annual operating and maintenance of Cascades Park is \$652,250 plus a one time one-time, non-recurring start-up budget of \$205,600 for equipment.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
\$16,500,000	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

SALES TAX PROJECTS

Project Number: 46B

Project Name: Operating Costs for Parks Built with Blueprint 2000 Funds: City Portion

Total Project Cost: \$10,514,300

Total Project Cost Notes

Total project cost reflects the current annual operation and maintenance cost of the City parks plus any one-time start up amounts over a 15 year period. The annual operating and maintenance cost of these parks is \$687,200.

Executive Project Summary

As a result of the holistic infrastructure planning and sensitive land purchase program, Blueprint 2000 has purchased and participated in partnerships to acquire properties that are intended for park space in Leon County and the City of Tallahassee. These park spaces, and their associated operation/maintenance obligations, are turned over to the City and County at project completion. This project seeks funding for the operation and maintenance of parks built with Blueprint 2000 funds. This project description details the operating costs for the parks maintained by The City of Tallahassee.

Themes

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input checked="" type="checkbox"/> Environmental/Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Detail Project Description

Florida Statutes 212.055 (2) allows for a portion of the sales tax proceeds to be used for the operation and maintenance of parks and recreation programs or facilities which have been constructed with the proceeds of the sales tax throughout the duration of the sales tax levy. The statute states:

“a county having a population greater than 75,000 in which the taxable value of real property is less than 60 percent of the just value of real property for ad valorem tax purposes for the tax year in which an infrastructure surtax referendum is placed before the voters, and the municipalities within such a county, may use the proceeds and interest of the surtax for operation and maintenance of parks and recreation programs and facilities established with the proceeds of the surtax throughout the duration of the surtax levy or while interest earnings accruing from the proceeds of the surtax are available for such use, whichever period is longer.”

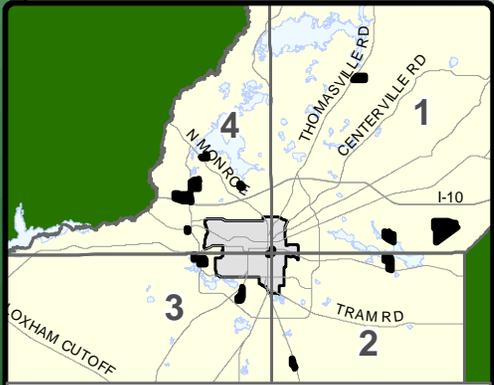
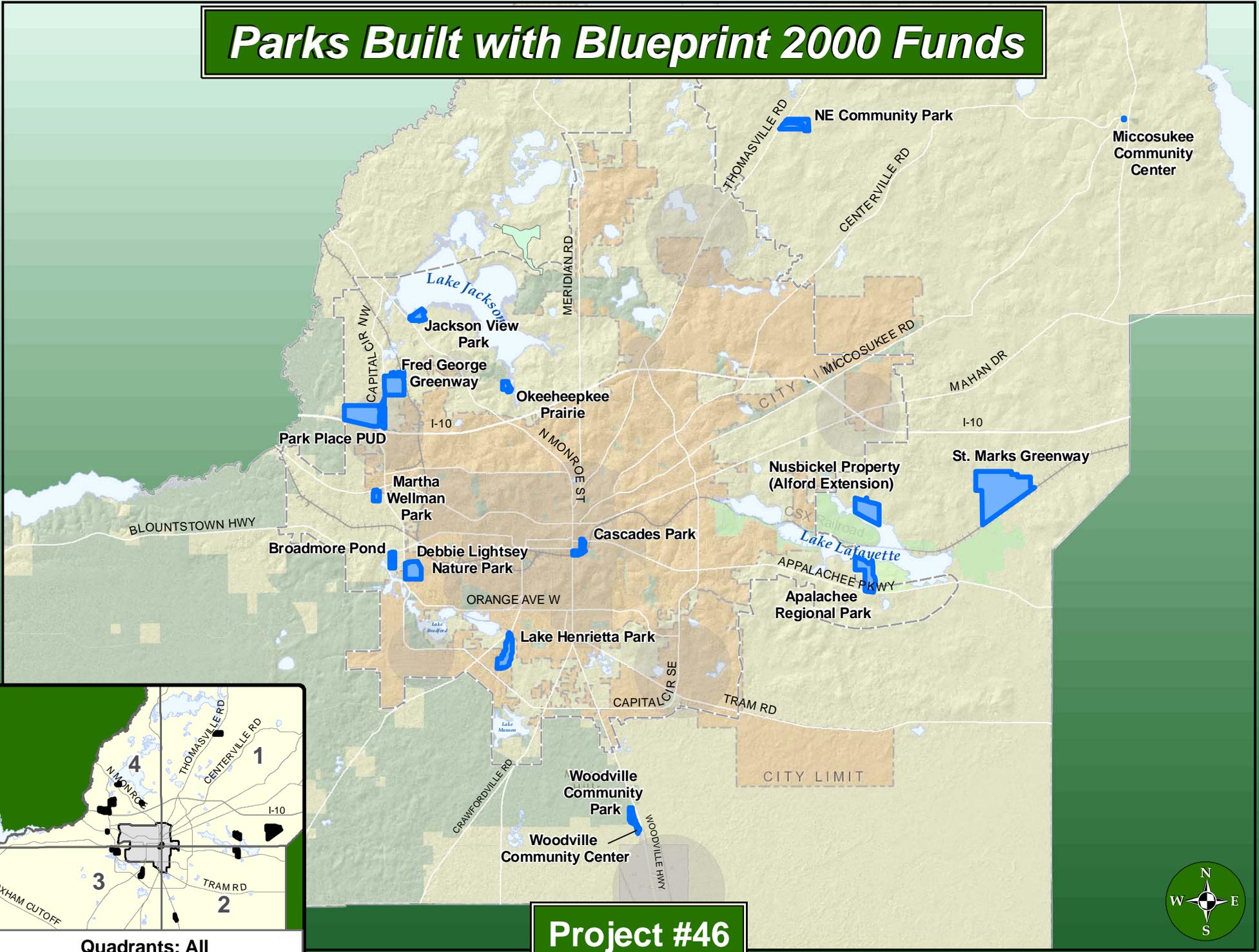
For the most recent year, the County’s just value was \$23.54 billion and the taxable value was \$13.38 billion or 56.87%. Therefore, the County qualifies for sales tax proceeds to be used for the operation and maintenance of parks and recreation programs and facilities that were constructed with sales tax funds.

In the City of Tallahassee, there are two facilities that are not yet open to the public but were purchased with sales tax funds. These parks include Cascades Park and the Debbie Lightsey Nature Trail. The City of Tallahassee will maintain and operate Cascades Park and Debbie Lightsey Nature Trail. The annual operating and maintenance of Cascades Park is \$687,200 plus a one time one-time, non-recurring start-up budget of \$205,600 for equipment.

Cost By Themes

	Regional Mobility/Transportation
	Sense of Community
	Environmental/Water Quality
	Connectivity
	Economic Vitality
\$10,514,300	Ecotourism/Parks
	Vertical Infrastructure
	Gateways

Parks Built with Blueprint 2000 Funds



Quadrants: All

Project #46



Proposed Sales Tax Projects by the Community

Project Name: Operating Costs for Parks Built with Blueprint 2000 Funds (Project #73)

Project Themes:

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

Florida Statutes 212.055 (2) allows for a portion of the sales tax proceeds to be used for the operation and maintenance of parks and recreation programs or facilities which have been constructed with the proceeds of the sales tax throughout the duration of the sales tax levy. The statute states:

“a county having a population greater than 75,000 in which the taxable value of real property is less than 60 percent of the just value of real property for ad valorem tax purposes for the tax year in which an infrastructure surtax referendum is placed before the voters, and the municipalities within such a county, may use the proceeds and interest of the surtax for operation and maintenance of parks and recreation programs and facilities established with the proceeds of the surtax throughout the duration of the surtax levy or while interest earnings accruing from the proceeds of the surtax are available for such use, whichever period is longer.”

For the most recent year, the County’s just value was \$23.54 billion and the taxable value was \$13.38 billion or 56.87%. Therefore, the County qualifies for sales tax proceeds to be used for the operation and maintenance of parks and recreation programs and facilities that were constructed with sales tax funds.

Project Map:

N/A

Appendix A

Projects Removed by Committee on March 28, 2013

Canopy Roads Legacy Projects (<i>Previously Project #14</i>)	Page 1
Inspiring the Creative Class: “America’s First Christmas” DeSoto Winter Encampment (<i>Previously Project #22</i>)	Page 3
Meadow Ridge Drive Park (<i>Previously Project #29</i>)	Page 5
Parallel Road to Tennessee Street (<i>Previously Project #30</i>)	Page 7
Moving People: Build the Regional Transfer System (<i>Previously Project #32</i>)	Page 9
Underground Utilities (<i>Previously Project #38</i>)	Page 11
County Facilities Infrastructure Improvements (<i>Previously Project #48</i>)	Page 13
County Road Resurfacing (<i>Previously Project #48</i>)	Page 15
Northeast Park (<i>Previously Project #55</i>)	Page 19
Private Dirt Road Paving Program (<i>Previously Project #60</i>)	Page 23
Sports Complex (<i>Previously Project #65</i>)	Page 25
Goodwood Land Purchase for Community Parks and Gardens (<i>Previously Project #72</i>)	Page 27

Proposed Sales Tax Projects by the Community

Project Name: Canopy Roads Legacy Projects (Project #14)

Project Themes:

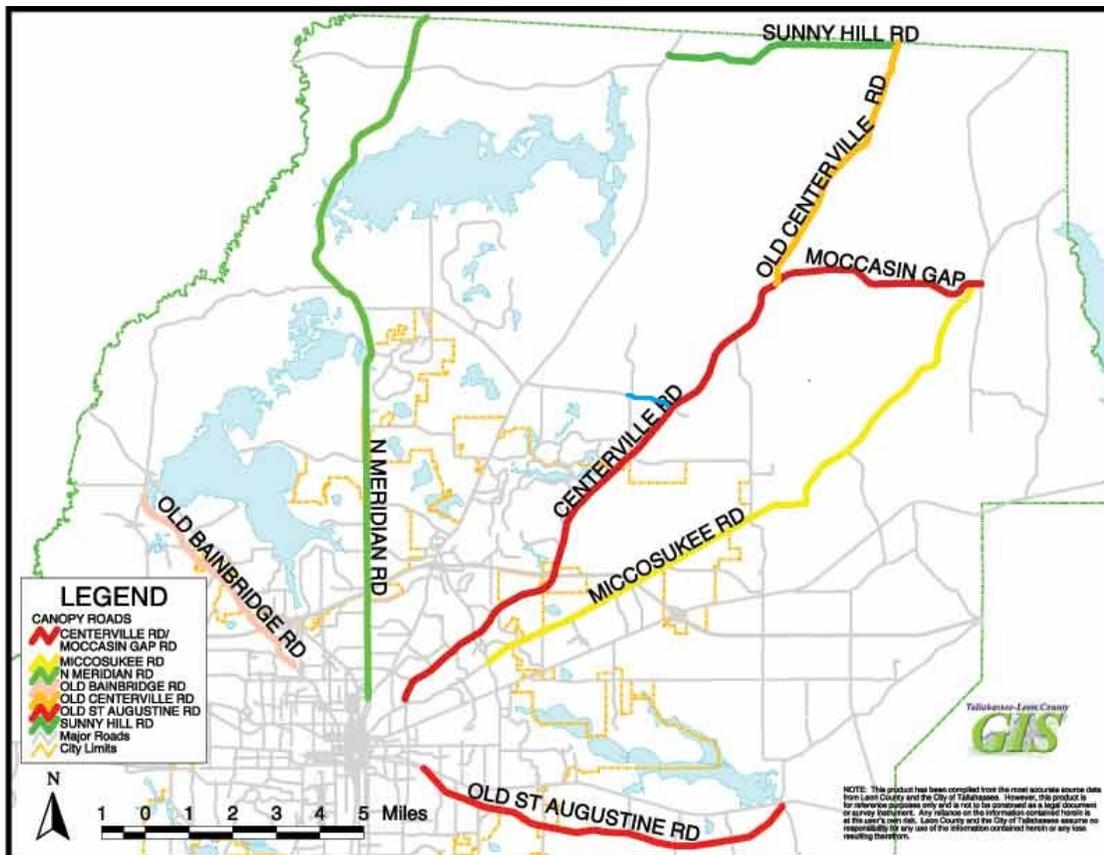
- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Project Description:

This proposed project is for an additional investment in the Canopy Road system in order to protect the historic and scenic roadways. Since 1990, the Comprehensive Plan has called for planning and implementation to maintain and improve each canopy road according to its unique attributes and incorporate appropriate safety provisions (Objective 3.4 [C] and accompanying policies).

The proposed Blueprint project could include funding the removal of identified tree hazards, as well as a unified maintenance and replanting program, neither of which currently exists. It could also include a marketing initiative to highlight the Canopy Roads' cultural and economic value to the community.

Project Map:



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Program
4g

Inspiring the Creative Class: "America's First Christmas" DeSoto Winter Encampment

Estimated Cost:
\$250,000

WHAT IS IT?

With the development of the America's First Christmas site, Tallahassee can become a nationally promoted tourist destination for those wishing to commemorate the location of the first Christmas in the Americas. The City of Tallahassee, partnering with other stakeholders, will be working with Senator Montford and the Leon County Commission to encourage the appropriation of state funds for the development of the site, the potential purchase of surrounding property, and creating sufficient ingress/egress to the site.

As the site itself is part of a state-owned park, the City and County are best suited for developing the surrounding transportation infrastructure such as additional sidewalks, widening of adjacent City/County roadways, and the overall improvement and beautification of the surrounding area. This Sales Tax proposal pertains to Goodbody Lane, a northeast to southwest road which is the preferred access point to the America's First Christmas site. The estimated cost includes an entrance feature, sidewalks, landscaping, and crosswalks.

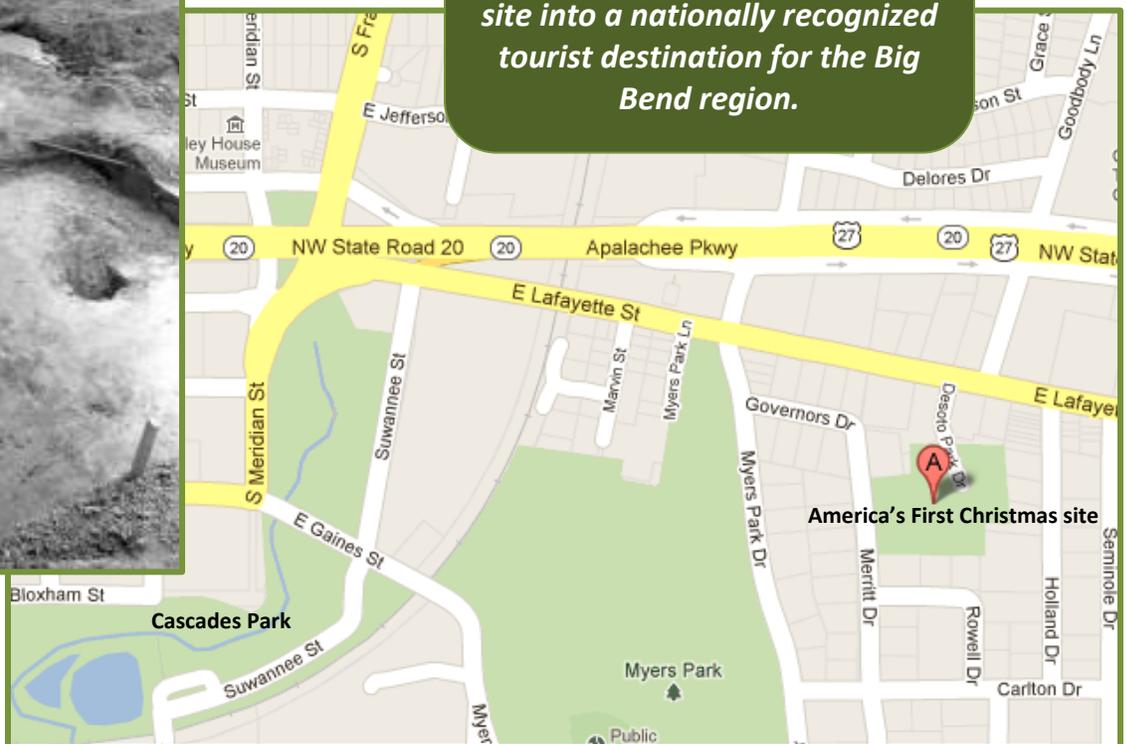
Evaluation Criteria:

- Provides Regional Impact / Geographic Equity ✓
- Enhances Mobility ✓
- Supports Recreation and Quality of Life ✓

- Benefits the Environment
- Supports Economic Development / Revitalization ✓
- On an Existing Master Plan or Blueprint List
- Leverages Other Funds ✓



Above, Calvin Jones excavating at the DeSoto Site, circa 1987 (photo courtesy of the Florida Memory Project)



The City, partnering with the State and County, can develop the America's First Christmas site into a nationally recognized tourist destination for the Big Bend region.

WHY IS IT IMPORTANT?

Provides Regional Impact/Geographical Equity – Investment in this incredibly important historical resource would lead to increased national and international recognition of Tallahassee and Leon County.

Enhances Mobility – The impact of additional vehicular traffic around the site could be mitigated by enhancing sidewalks and roadways on Lafayette Street and Goodbody Lane. Additionally, businesses and residents in the area would benefit from improved infrastructural improvements along these roadways.

Supports Recreation and Quality of Life – Citizens of Tallahassee and Leon County will be able to visit and enjoy an historic site of national significance. The America's First Christmas site represents a truly amazing find and has the potential to become landmark attraction for the Big Bend region.

Supports Economic Development/Revitalization – Development of the America's First Christmas site would create a nationally recognized tourist destination with the potential to attract large numbers of tourists with varied interests. As such, the site could become a significant economic engine for the City of Tallahassee due to increased tourist demand for hotels, restaurants and other amenities. And with the site's proximity to Cascades Park (about ¼ mile), tourists coming to Tallahassee for the America's First Christmas attraction will likely visit Cascades Park and the businesses along Gaines Street as well.

Leverages Other Funds – A commitment by the City of Tallahassee and Leon County towards this project will support efforts of the Leon County legislative delegation and the Florida Department of State in acquiring additional funding from the State. Leon County is also currently upgrading stormwater, sidewalks, and landscaping along Lafayette Street between the site and Cascade Park.



At left, volunteers assisting with excavation of DeSoto Site, circa 1987 (photo courtesy of the Florida Memory Project)

At right, the Gov. John W. Martin House is a historic home located on the state-owned property of Hernando De Soto's winter campsite. The house, located at 1001 Governor's Drive, was added to the U.S. National Register of Historic Places on January 6, 1986. The house may serve a role in the commemoration of the America's First Christmas site.



Proposed Sales Tax Projects by the Community

Project Name: Meadow Ridge Drive Park (Project #29)

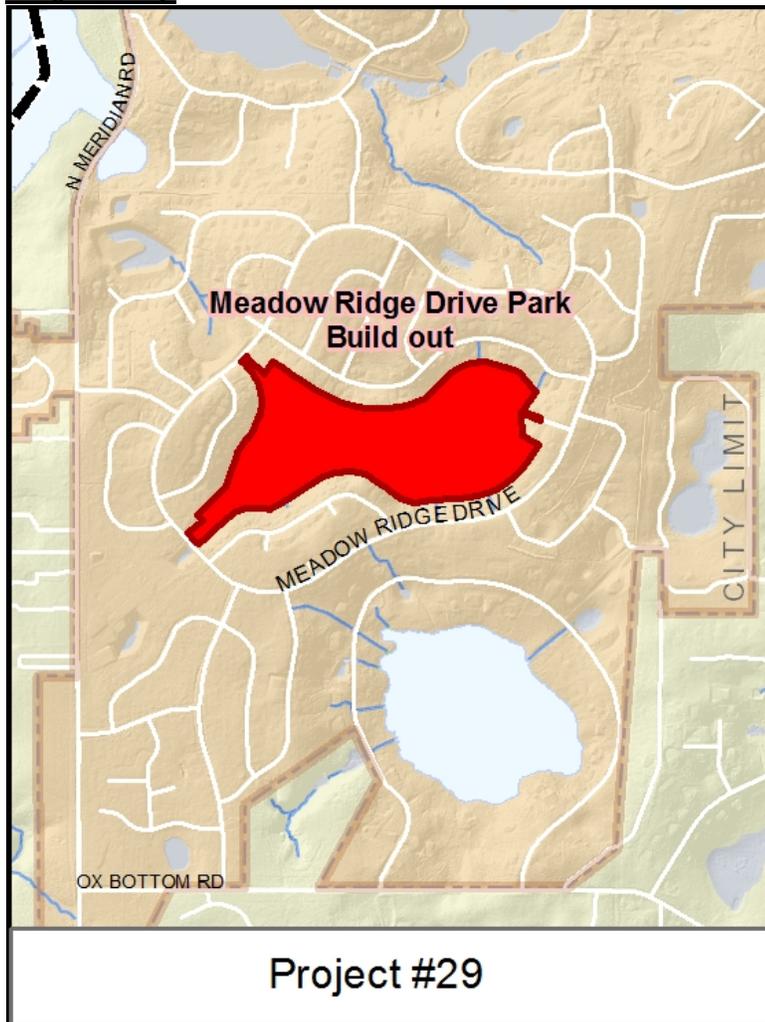
Project Themes:

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

This project would involve creating a playground on City owned property in the Ox Bottom subdivision. This would provide a park in walking distance to the Meadow Ridge neighborhood, whereas the neighborhood residents would otherwise have to drive their children to another park.

Project Map:



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Proposed Sales Tax Projects by the Community

Project Name: Parallel Road to Tennessee Street (Project #30)

Project Themes:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

This project proposes to develop a new roadway which parallels Tennessee Street. Said roadway shall serve to relieve some of the Tennessee Street traffic demand. Preliminary analysis by City staff finds the following:

- Tennessee Street (US 90), is owned and operated by the Florida Department of Transportation (FDOT) and that the section between Monroe Street to Ocala Road lies within the City’s Multi-modal Transportation District (MMTD). The primary goal of the MMTD is to create a safe and desirable environment for pedestrians, cyclists, and transit users. This is accomplished by discouraging traditional transportation capacity improvements and instead encouraging improvements that expand the available modal choices.
- The MMTD extends north of Tennessee Street to Tharpe Street (under the County’s jurisdiction) and South to Orange Avenue (under the FDOT’s jurisdiction). Therefore, any project proposed to create a parallel road to Tennessee Street between Tharpe Street and Orange Avenue would fall within the MMTD and must support the modal needs of the district. The MMTD is currently densely developed and includes institutional developments which would likely be traversed with the new roadway.

It is staff’s recommendation that if this project is to advance, its focus should shift to provide facilities which support the modal alternatives rather than traditional roadways.

Project Map:



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Program
2d

Moving People: Build the Regional Transfer Station

Estimated Cost:
\$5 million

WHAT IS IT?

The new decentralized system supports regional travel from the surrounding counties. Regional routes, such as from Wakulla County, would connect to the route structure at the edge of the service area and have access to the entire system. A regional transfer center is proposed for the SouthWood Town Center, and would include bus bays to support routes going throughout the system, restrooms, ticket purchase facilities, and leasable space. The estimated cost of this facility is \$5,000,000.

Evaluation Criteria:

Provides Regional Impact / Geographic Equity	✓	Benefits the Environment	✓
Enhances Mobility	✓	Supports Economic Development / Revitalization	✓
Supports Recreation and Quality of Life	✓	On an Existing Master Plan or Blueprint List	✓
		Leverages Other Funds	✓



This rendering shows the proposed Regional Transfer Station at Southwood. Land for a transfer station was dedicated at the time Southwood was approved.

WHY IS IT IMPORTANT?

Benefits the Environment – The facility would expand transit options for surrounding counties providing an alternative to daily automobile commutes. Each center could support walkable mixed-use centers with retail, housing, and office space.

Provides Regional Impact / Geographic Equity – Limited options are currently available for transit outside the City of Tallahassee. The transfer station would encourage service expansion into the growing communities in surrounding counties.

Enhances Mobility – More options would be available for people with disabilities, the elderly, the economically disadvantaged, students, and those who desire to reduce automobile dependency.

Supports Recreation and Quality of Life – Tallahassee residents could use transit to access recreational facilities in surrounding counties, and County residents would have access to city destinations.

Supports Economic Development / Revitalization – A regional transfer station could attract development in and around each station.

On an Existing Master Plan or Blueprint List – The Regional Transfer Station is included in StarMetro’s current Transit Development Plan as well as in the Regional Mobility Plan for the Capital Area.

Leverages Other Funds – Land is dedicated in the Southwood DRI for transit facility. Under the current federal transportation bill (MAP-21), additional funding could be leveraged under the Federal Transit Administration (FTA) Section 5339 Bus and Bus Facility program for this project provided a local match is identified.



In addition to restrooms and covered walkways, the Regional Transfer Station would include a branch library and potentially small retail space.

Proposed Sales Tax Projects by the Community

Project Name: Underground Utilities (Project #38)

Project Themes:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Gateways |

Project Description:

Undergrounding utilities along certain key corridors would provide several types of benefits, both utilitarian and aesthetic. First, it could result in fewer down lines during strong storm events, resulting in fewer power outages and less maintenance time, both resulting in cost savings. Aesthetically, it could enhance Gateway corridors throughout Tallahassee and Leon County by removing visual clutter from the streets and sidewalks, enhancing the sense of place of certain areas. Finally, undergrounding in certain corridors would allow for safer sidewalk access because utility poles could be removed where they currently share limited right-of-way and impinge pedestrian movement.

Project Map:

N/A

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Proposed Projects for the Tax Extension

Project #15: County Facilities Infrastructure Improvements

Estimated Project Cost: \$42 million

Project Criteria:

- | | |
|---|--|
| <input type="checkbox"/> Comprehensive Plan | <input type="checkbox"/> Green Infrastructure & Water Quality Enhancements |
| <input type="checkbox"/> Regional Mobility | <input type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input type="checkbox"/> Greenway Master Plan | <input type="checkbox"/> Transportation Capacity Improvements |
| <input type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input type="checkbox"/> Complements BP2000 Project | |

Project Description:

This project is for county facilities infrastructure improvements with a lifespan of five years or more in accordance with Florida Statutes. During the project identification process, staff identified over \$75 million of facilities improvements over the next 20 years to county buildings such as the Main Library, branch libraries, Public Works and Facilities offices, community centers, the Public Safety Complex, and other county facilities. It is important to note that County facilities and infrastructure have numerous additional needs that will need to be funded through the annual budget process. The needs included in this proposed project are court and public safety buildings which are vital core infrastructure facilities. These improvements include construction and/or maintenance performed on the exterior, mechanical, and electrical. Although not included in these preliminary estimates, future population growth will most likely require additional courtrooms and inmate pods. The County Facilities Infrastructure Improvements project meets the core infrastructure criterion and is a critical component of the long term capital improvement program.

The County Facilities Infrastructure Improvement project proposed improvements include the following:

- **Leon County Courthouse and Annex:** According to Florida Statue 29.008, Leon County is required to fund the cost of communications services, construction and/or lease, maintenance, utilities, and security of facilities for the circuit and county courts, public defenders' offices, state attorney's offices, guardian ad litem offices, and the offices of clerks of the circuit and county courts performing court related functions.

Over the next 20 years, the Leon County Courthouse and Annex will need construction and/or maintenance performed on the exterior, mechanical and electrical for a total of \$23.8 million. Although not included in these preliminary estimates, future population growth will most likely require additional courtrooms. Any improvements to the Courthouse and the Annex will be made in accordance with the Leon County Office of Resource Stewardship initiatives. This construction and maintenance is required under Florida law and is an allowable expense for the local governance infrastructure surtax as stated in FS 212.055(2)(d)(1)(c):

"For the purposes of this paragraph, the term "infrastructure" means ... c. Any expenditure for the construction, lease, or maintenance of, or provision of utilities or security for, facilities, as defined in s. 29.008."

- **Leon County Jail:** According to Florida Statue 30.49, the Leon County Sheriff must submit a proposed budget for all proposed expenditures excluding the cost of construction, repair or capital improvements of county buildings. Due to the fact that Leon County jail is a county building, the County is required to maintain the facility including funding the cost of construction and/or repair.

Project #15: County Facilities Infrastructure Improvements

Page 2

Over the next 20 years, the Leon County Jail will need construction and/or maintenance performed on the exterior, mechanical, and electrical for a total of \$18.2 million. Although not included in these preliminary estimates, future population growth will most likely require additional inmate pods. Any improvements to the Courthouse and the Annex will be made in accordance with the Leon County Office of Resource Stewardship initiatives. This improvement is an allowable expense for the local governance infrastructure surtax as stated in FS 212.055(2)(d)(1)(a):

“For the purposes of this paragraph, the term "infrastructure" means: a. Any fixed capital expenditure or fixed capital outlay associated with the construction, reconstruction, or improvement of public facilities that have a life expectancy of 5 or more years and any related land acquisition, land improvement, design, and engineering costs.”

Project Criteria Detail:

Comprehensive Plan: N/A

Regional Mobility Plan: N/A

Greenway Master Plan: N/A

Connectivity: N/A

Complements Blueprint 2000 Project(s): N/A

Green Infrastructure and Water Quality Enhancements: N/A

Stormwater/Sewer Capacity Improvements: N/A

Transportation Capacity Improvements: N/A

Core Infrastructure: The Leon County Courthouse, Courthouse Annex, and Jail are facilities that are critical to the core function of county government. The improvements made to these facilities will be made in accordance with the Leon County Office of Resource Stewardship initiatives. As stated previously, County facilities and infrastructure has numerous needs that will be funded through the annual budget process. The needs included in this proposed project are critical buildings that must be maintained according to Florida Statute.

Proposed Project for the Sales Tax Extension

Project #14: County Road Resurfacing

Estimated Project Cost: \$156.6 million

Project Criteria:

- | | |
|--|--|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input type="checkbox"/> Water Quality Enhancements and Green Infrastructure |
| <input checked="" type="checkbox"/> Regional Mobility | <input type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input type="checkbox"/> Greenway Master Plan | <input type="checkbox"/> Transportation Capacity Improvements |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input type="checkbox"/> Compliments BP2000 Project | |

Project Description:

This project is for the County's on-going road resurfacing program. Best practices indicate that the County should resurface arterial roads every 8 years (33.9 miles per year), local roads every 12 years (23.3 per year), and public access roads as needed.

The current gas tax revenues do not provide any funds towards the County's resurfacing program or intersection/safety improvements. The road resurfacing program is supported through the balance of funds remaining from the prior infrastructure sales tax. This allocation occurred in FY2008 when the County recommitted existing resources away from new projects towards the maintenance of existing infrastructure (such as resurfacing). Based on current projections, these funds will be depleted during FY2014. Beginning in FY2014, staff continues to recommend utilizing sales tax resources for the resurfacing and intersection/safety improvement program through the County's 10% share of the existing sales tax.

Project Criteria Detail:

Comprehensive Plan: This project supports the both the Capital Improvement elements of the Comprehensive Plan. The areas that the project complies with are listed below.

- **Capital Improvement Element Policy 2a:** Priorities of Capital Improvements within a Type of Public Facility. Capital improvements within a type of public facility are to be evaluated on the following criteria and considered in the order of priority listed below. The local government shall establish the final priority of all capital improvements using the following criteria as general guidelines. Any revenue source that cannot be used for a high priority facility will be used beginning with the highest priority for which the revenue can legally be expended. Repair, remodeling, renovation, or replacement of obsolete or worn out facilities that contribute to achieving or maintaining standards for levels of service adopted in this Comprehensive Plan.

Regional Mobility Plan: This project rehabilitates the existing infrastructure network supports neighborhood and regional roadway investments by the preservation of the current roadway network.

Greenway Master Plan: N/A

Connectivity: This project will maintain county roadways and support connectivity through improved rideability of the roadways.

Compliments Blueprint 2000 Project(s): N/A

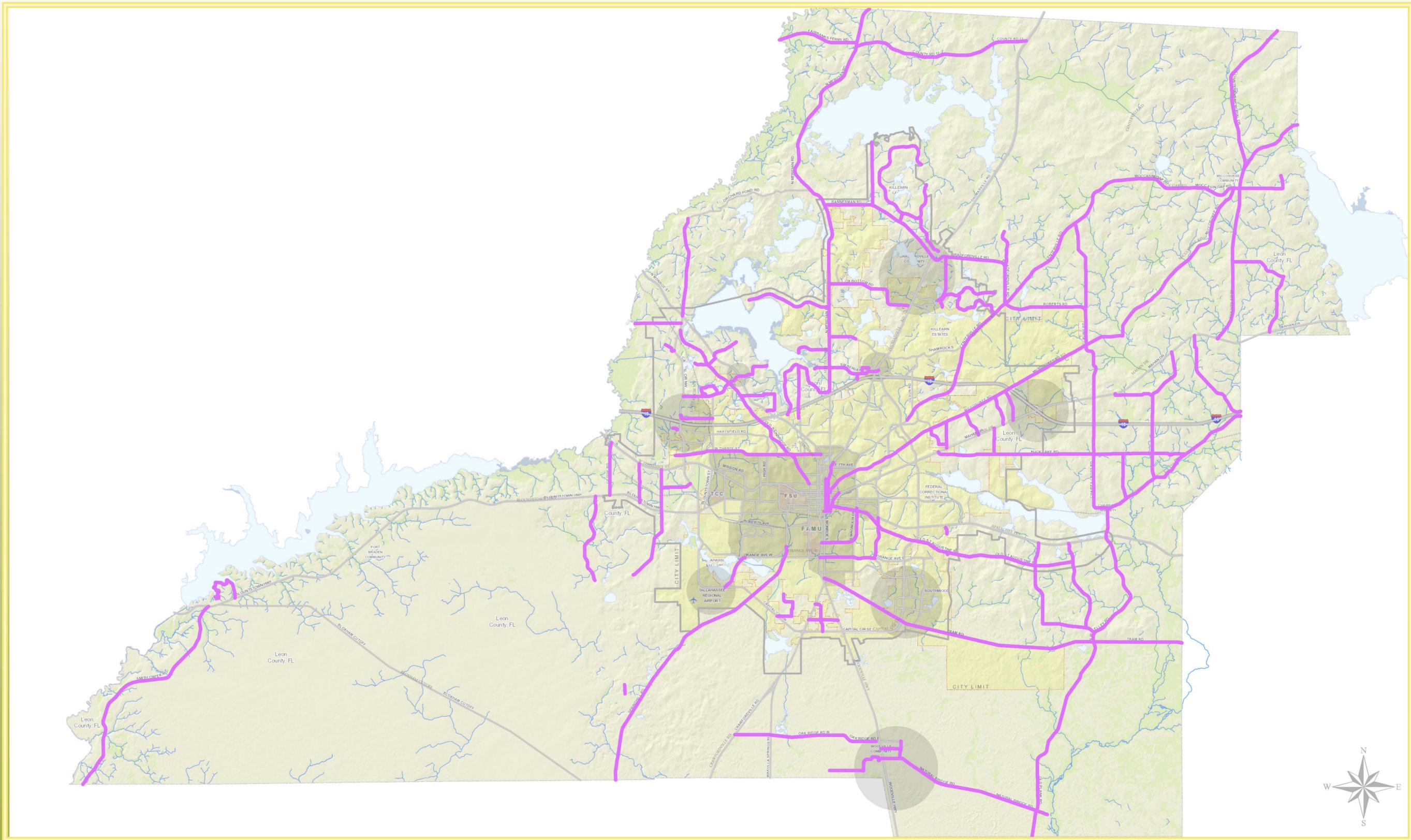
Water Quality Enhancements and Green Infrastructure: N/A

Stormwater/Sewer Capacity Improvements: N/A

Transportation Capacity Improvements: N/A

Core Infrastructure: This project is considered core infrastructure projects which enhance the safety and efficient operations of the roadways for all vehicles.

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County Road Resurfacing

Legend

- County Road Resurfacing
- Mobility District/Activity Nodes
- Urban Service Area

County Road Resurfacing Project Criteria:

- | | |
|--|--|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input type="checkbox"/> Water Quality Enhancements and Green Infrastructure |
| <input checked="" type="checkbox"/> Regional Mobility | <input type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input checked="" type="checkbox"/> Greenway Master Plan | <input type="checkbox"/> Transportation Capacity Improvements |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input type="checkbox"/> Compliments BP2000 Project | |



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Proposed Project for the Sales Tax Extension

Project #11: Northeast Park

Estimated Project Cost: \$13.13 million

Project Criteria:

- | | |
|--|--|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input type="checkbox"/> Water Quality Enhancements and Green Infrastructure |
| <input type="checkbox"/> Regional Mobility | <input type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input checked="" type="checkbox"/> Greenway Master Plan | <input type="checkbox"/> Transportation Capacity Improvements |
| <input type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input type="checkbox"/> Complements BP2000 Project | |

Project Description:

The Northeast Park is 100-acre parcel located at the corner of Proctor Road and Thomasville Road. Currently, the northeast area of the County is the only geographically area that is not served by a community park. During the May 22, 2012, the Board of County Commission purchased the park land. While specific elements of the project will be determined by the community's needs and input, it is anticipated that the activity-based facilities will be limited to approximately 55 acres within the tract and will include the following:

- Little League fields
- Multi-purpose fields
- Junior/Senior Major fields
- Concession/restroom facilities
- Adequate parking, lighting, irrigation, scoreboards, bleachers, sidewalks
- A maintenance building
- Playground
- Natural trails and trail head
- Necessary stormwater facilities

The remaining approximate 45 acres of the park will be set aside as a passive park and will include trails for hiking and biking.

This project also includes the realignment and paving of Proctor Road within the confines of the park property. Additionally, a north-bound turn lane would need to be added on Thomasville Road to facilitate traffic flow into the park.

Project Criteria Detail:

Comprehensive Plan: This project supports the Goal, Objectives and Policies within the Parks and Recreation element of the Comprehensive Plan. In particular, two are listed below:

- Parks and Recreation Element Goal 1: Provide, manage and fund adequate recreational and cultural facilities to encourage a physically and mentally healthy lifestyle for all citizens of Tallahassee-Leon County.
- Parks and Recreation Policy 1.1.4: In the acquisition and development of future park facilities, priority shall be given to the preservation of natural vegetation and land features, consistent with the functional purpose of the individual park. As a minimum, a system-wide average of 50% of the natural features will remain undeveloped except for inclusion of passive recreational facilities, such as trail systems, ancillary parking, and picnicking facilities. This system-wide policy is based on the recognition that some types of facilities (such as golf course and ball fields), due to their design requirements and functions, will require greater disturbance of natural features than would other types of park facilities. This standard shall be based on all lands included in the level of service system provided in Policy 1.1.1 [R] above, not on the land holdings of an individual government.
- Parks and Recreation Element Policy 1.1.5: Design and construction of all future activity based parks and any future redesign of existing activity based parks will include equipped children's playground areas whenever feasible.

Project #11: Northeast Park

Page 2

Regional Mobility Plan: N/A

Greenway Master Plan: This project will also provide for passive recreation over the remaining approximate 40 acres including trails for hiking and biking.

Connectivity: N/A

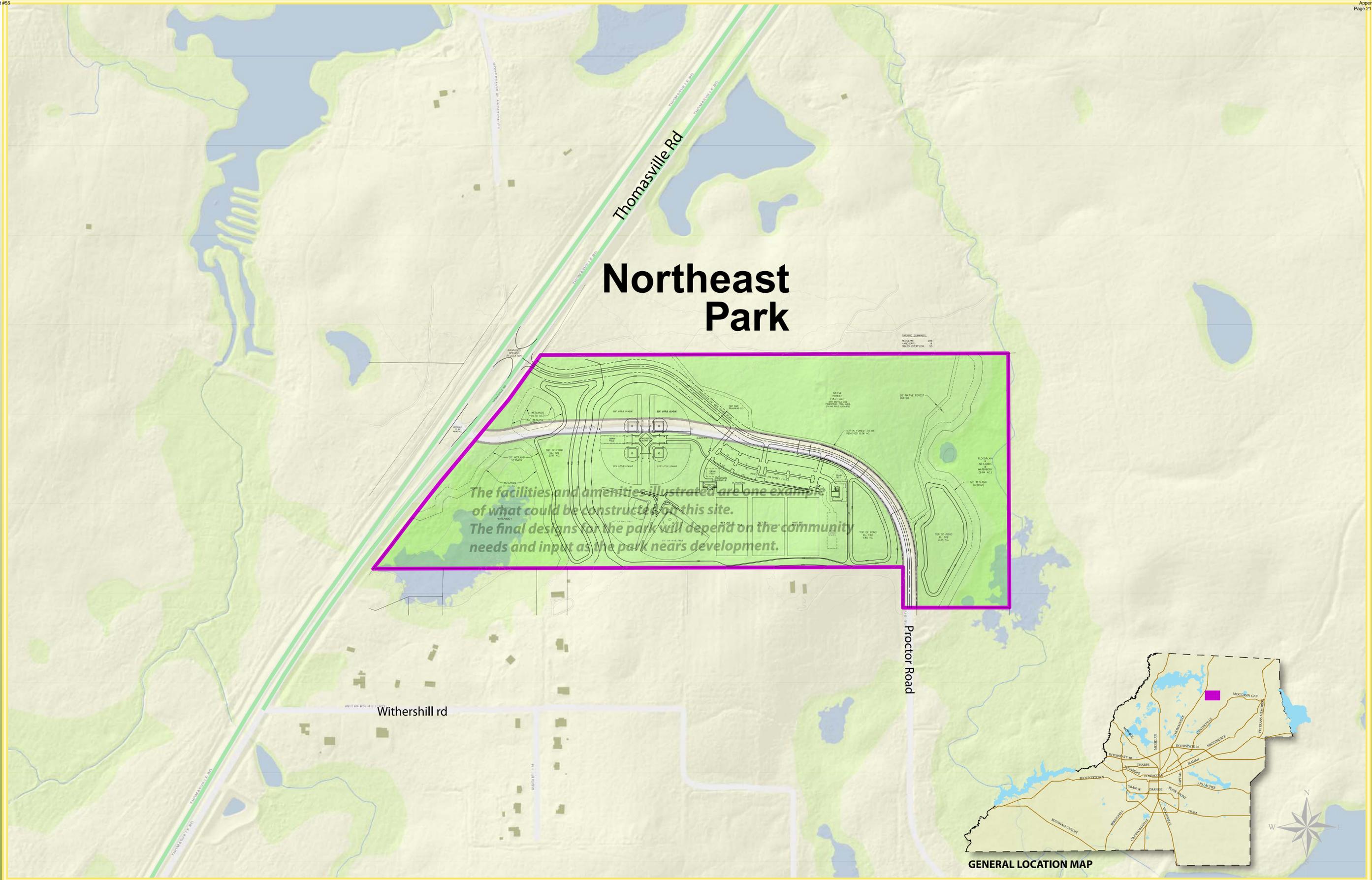
Complements Blueprint 2000 Project(s): N/A

Water Quality Enhancements and Green Infrastructure: N/A

Stormwater/Sewer Capacity Improvements: N/A

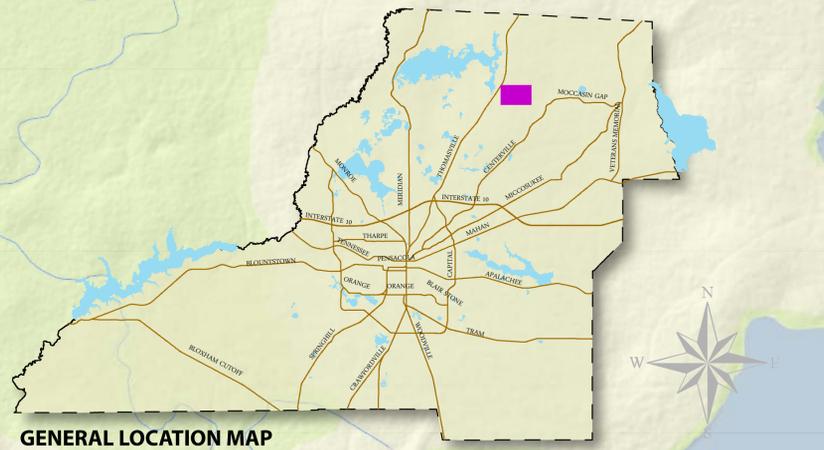
Transportation Capacity Improvements: N/A

Core Infrastructure: This project fills a significant gap in recreational facilities that currently exists in the northeast section of the County. The facilities and amenities illustrated on the map are one example of what could be constructed on Northeast Park site. The final designs for the park will depend on the community needs and input as the park nears development. Alternative illustrations are available upon request.



Northeast Park

The facilities and amenities illustrated are one example of what could be constructed on this site. The final designs for the park will depend on the community needs and input as the park nears development.



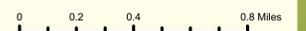
Northeast Park

Legend

- Proposed Project
- Existing Sidewalks, Bike Routes
- Parks
- Major Lakes

Northeast Park Project Criteria

- | | |
|--|--|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input type="checkbox"/> Water Quality Enhancements and Green Infrastructure |
| <input checked="" type="checkbox"/> Regional Mobility | <input type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input checked="" type="checkbox"/> Greenway Master Plan | <input type="checkbox"/> Transportation Capacity Improvements |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input type="checkbox"/> Complements BP2000 Project | |



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Proposed Project for the Sales Tax Extension

Project #16: Private Dirt Road Paving Program

Estimated Project Cost: \$52 million

Project Criteria:

- | | |
|--|---|
| <input checked="" type="checkbox"/> Comprehensive Plan | <input checked="" type="checkbox"/> Green Infrastructure & Water Quality Enhancements |
| <input checked="" type="checkbox"/> Regional Mobility | <input checked="" type="checkbox"/> Stormwater/Sewer Capacity Improvements |
| <input type="checkbox"/> Greenway Master Plan | <input checked="" type="checkbox"/> Transportation Capacity Improvements |
| <input checked="" type="checkbox"/> Connectivity | <input checked="" type="checkbox"/> Core Infrastructure |
| <input type="checkbox"/> Complements BP2000 Project | |

Project Description:

This project is for the paving of private dirt roads in the incorporated area of Leon County. Currently, for the majority of development activity, developers are required to build roads to County standards regardless if the road is public or private. There are exceptions for large rural tracts based on intensity and density. Staff has identified 195 miles of unpaved dirt roads. Specific criteria would need to be developed to prioritize roads, such as a minimum number of lots accessing the road.

Under the sales tax extension, the County could develop a program that provides funding towards the paving of the private roads if it met certain criteria, which may include, but not be limited to: 100% of the ROW being donated and 2/3's of the property owners to sign a petition requesting the project. Paving of existing dirt roads will require additional resurfacing dollars to preserve the investment in the new infrastructure.

Project Criteria Detail:

Comprehensive Plan: This project supports the both the Mobility and Conservation elements of the Comprehensive Plan. The areas that the project complies with are listed below.

- Mobility Element Policy 1.3.4 Design, construct, and maintain transportation facilities to prevent flooding, minimize pollution, and maintain natural stormwater quantity, timing, rate, and direction of flow characteristics consistent with the adopted Stormwater Level of Service Standard.
- Conservation Element Goal 1: Preserve, protect and conserve the ecological value and diversity of natural resources in Tallahassee and Leon County.

Regional Mobility Plan: Paved roadways enhance transportation mode opportunities.

Greenway Master Plan: N/A

Connectivity: Paving of dirt roads provides the increased opportunity for pedestrians and bicycles and enhances a varied sector of the population to utilize and access the overall transportation network. Further paving the roadways increases access to residential neighborhoods during inclement weather and provides an efficient means for police, fire and EMS to respond without threat of being adversely impeded by poor roadway conditions.

Complements Blueprint 2000 Project(s): N/A

Green Infrastructure and Water Quality Enhancements: Paved roadways with proper stormwater treatment improves water quality with the reduction of soil erosion to adjacent natural systems.

Stormwater/Sewer Capacity Improvements: N/A

Transportation Capacity Improvements: Paved roadways improve the efficiency of vehicular movements by providing an all weather driving surface.

Core Infrastructure: Dirt road improvements are considered core infrastructure projects which enhance the safety and efficient operations of the roadways and provide improved access for law enforcement and emergency vehicles.

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Proposed Sales Tax Projects by the Community

Project Name: Sports Complex (Project #65)

Project Themes:

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input checked="" type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

This project proposes an indoor Sports Complex which would support 12 basketball courts and 16 volleyball courts. Additionally, the facility would serve indoor soccer, indoor lacrosse, wrestling, cheerleading and martial arts.

The Florida Sports Foundation 2011 report assessed the County's current inventory of competitive sports facilities and identified the need for sports venues that would generate an economic impact through the development of sports tourism. Leon County's Sports Complex economic feasibility analysis was completed in 2012 by Real Estate InSync (REI). The analysis noted that there is only one other indoor facility within a 200-mile radius, and determined that an indoor facility would have an immediate net operating profit. REI projected that the indoor field house would cost \$1.4 million in the first year of operation and generate \$1.7 million in operating revenue. Almost two-thirds of the revenue (\$1.1 million) for the field house would be generated by local residents, as opposed to visitors, through activities such as fitness club memberships and facility rentals. The current revenue model may be counterintuitive to the initial purpose of the development of a competitive sports complex, which was to develop a facility supported primarily through tourism activities. The REI's study identified the top five sites for an indoor field house. The Downtown/O'Connell site owned by the Tallahassee Community Redevelopment Agency, previously identified as a potential location for a convention center and hotel, is the top ranked site.

During the July 10, 2012 Board of County Commissioners meeting, the Board considered the findings on the REI study. The projected capital cost for the indoor field house is between \$27 million and \$36 million with an annual operating cost of approximately \$1.4 million. Given the breadth of this project and the limitations of the County's finances, the Board decided to take no further action in pursuing the Sports Complex.

Project Map:

N/A

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Proposed Sales Tax Projects by the Community

Project Name: Goodwood Land Purchase for Community Parks and Gardens (Project #72)

Project Themes:

- | | |
|---|---|
| <input type="checkbox"/> Regional Mobility/Transportation | <input checked="" type="checkbox"/> Economic Vitality |
| <input checked="" type="checkbox"/> Sense of Community | <input checked="" type="checkbox"/> Ecotourism/Parks |
| <input type="checkbox"/> Water Quality | <input type="checkbox"/> Vertical Infrastructure |
| <input type="checkbox"/> Connectivity | <input type="checkbox"/> Gateways |

Project Description:

This project is for the purchase of land within the Goodwood Plantation property. The purchase of this property would create additional park land within the urban core. Goodwood is recommending the purchase of three parcels of land between \$200,000 and \$400,000 per acre. The proceeds from the sale of the land would go towards the retirement of Goodwood’s existing debt with Capital City Bank.

- Parcel #1: One acre of land located at the southwest corner of the Goodwood grounds and the northeast corner of Miccosukee Road and Medical Drive. Due to the location of the parcel to Tallahassee Memorial Hospital, it is recommended that this area be used as a children’s park. As part of a public/private partnership, TMH has agreed to maintain this playground site.
- Parcel #2: This parcel is 1.2 acres and located on the southeast corner of the Goodwood ground along Miccosukee Road. It is recommended that this land be used as an extension of the children’s park, possibly for older children or as a passive area.
- Parcel #3: This parcel is 0.7 acres and located at the northeast corner of the Goodwood grounds along Surgeon’s Drive. It is recommended that this parcel be used for a community garden.

This project complements the City of Tallahassee’s proposed Project #4f: Goodwood Playground and Community Garden.

Project Map:

